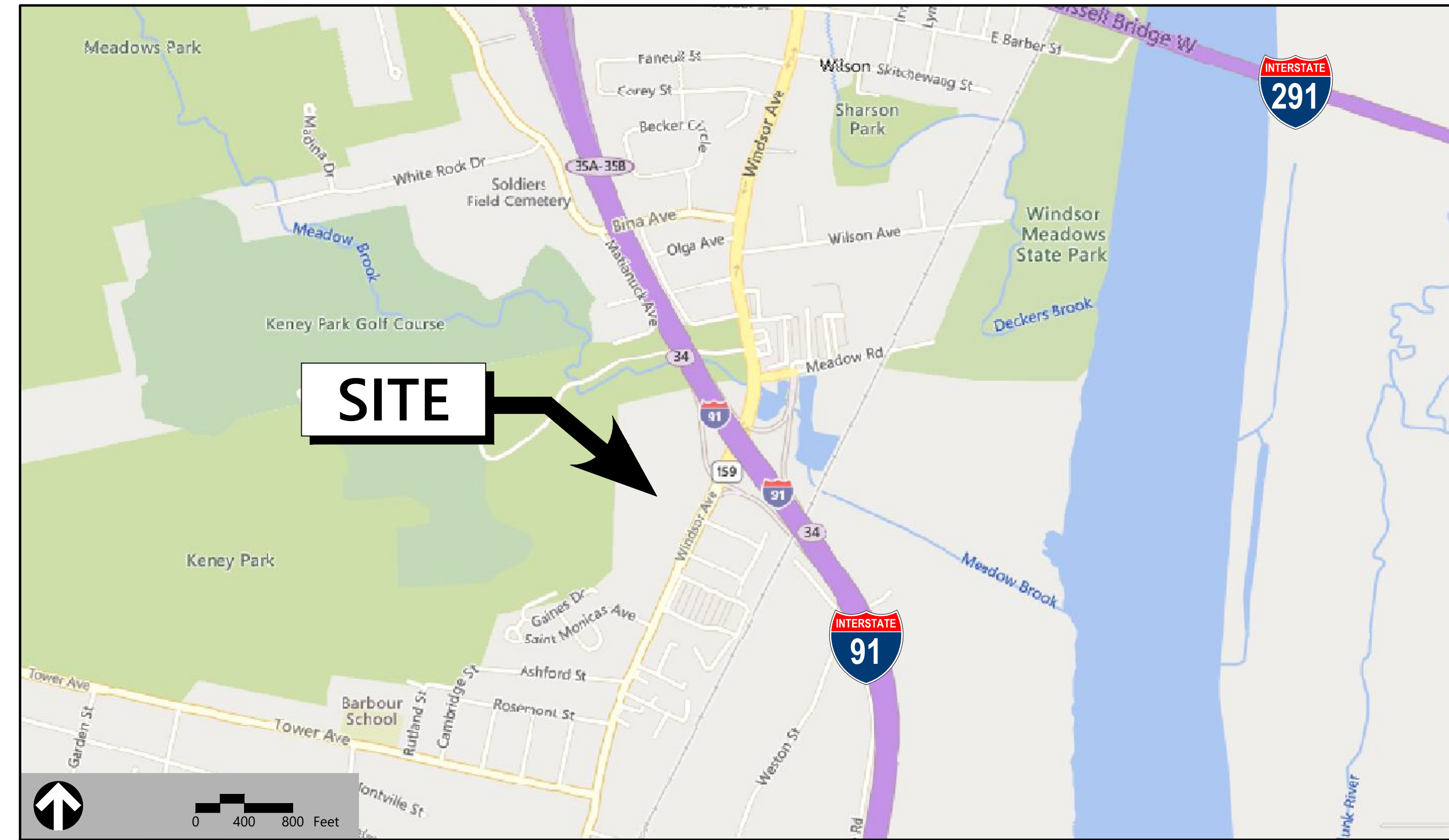


# Site Plans

Issued for	Site Plan Approval
Date Issued	July 28, 2023
Latest Issue	July 28, 2023

## Wilson Center Mixed-Use Development

29 Windsor Ave  
Windsor, CT 06095



### Owner

Dijon, LLC  
1500 Main Street  
Suite 255  
Springfield, MA 01103

### Applicant

Dijon, LLC  
1500 Main Street  
Suite 255  
Springfield, MA 01103

**Accessor's Map: 81**  
**Block: 9**  
**Lot: 12**

Sheet Index		
No.	Drawing Title	Latest Issue
C-1	Legend, Abbreviations, and General Notes	July 28, 2023
C-2	Demolition Plan	July 28, 2023
C-3	Overall Site Plan	July 28, 2023
C-4	Layout and Materials Plan	July 28, 2023
C-5	Grading and Drainage Plan	July 28, 2023
C-6	Utility Plan	July 28, 2023
C-7	Erosion and Sediment Control Plan	July 28, 2023
C-8	Details	July 28, 2023
C-9	Details	July 28, 2023
C-10	Details	July 28, 2023
C-11	Details	July 28, 2023
C-12	Details	July 28, 2023
C-13	Fire Truck Route	July 28, 2023

Reference Drawings		
No.	Drawing Title	Latest Issue
VB101	ALTA/NSPS Land Title Survey	December 6, 2017
VB102	ALTA/NSPS Land Title Survey	December 6, 2017
SL-2	Site Lighting Photometric Calculation	June 5, 2023
L-401	Planting Plan	July 28, 2023
L-402	Planting Details	July 28, 2023
A-1	Proposed Convenience Store	July 6, 2023
A-1	Proposed Car Wash	July 11, 2023





One Federal Street  
Building 103-3N  
Springfield, MA 01105  
413.747.7113

Legend

Table with 4 columns: Exist., Prop., Description, and another set of Exist./Prop. columns. It lists various site features like Property Line, Easement, Parking Setback, Baseline, Zoning Line, Wetland Line, FLOODPLAIN, and various types of roads and curbs.

Abbreviations

Table with 2 columns: General and Abbreviations. It lists terms like ABANDON, ACCESSIBLE CURB RAMP, ADJUST, APPROXIMATE, BITUMINOUS, BOTTOM OF CURB SLOPE, BROKEN WHITE LANE LINE, CONCRETE, DOUBLE YELLOW CENTER LINE, ELEVATION, EXISTING, FOUNDATION, etc.

Notes

- CONTRACTOR SHALL NOTIFY 'CALL BEFORE YOU DIG' (1-800-922-4455) AT LEAST 72 HOURS BEFORE EXCAVATING.
CONTRACTOR SHALL BE RESPONSIBLE FOR SITE SECURITY AND JOB SAFETY. CONSTRUCTION ACTIVITIES SHALL BE IN ACCORDANCE WITH OSHA STANDARDS AND LOCAL REQUIREMENTS.
ACCESSIBLE ROUTES, PARKING SPACES, RAMPS, SIDEWALKS AND WALKWAYS SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE FEDERAL AMERICANS WITH DISABILITIES ACT AND WITH STATE AND LOCAL LAWS AND REGULATIONS (WHICHEVER ARE MORE STRINGENT).

Layout and Materials

- DIMENSIONS ARE FROM THE FACE OF CURB, FACE OF BUILDING, FACE OF WALL, AND CENTER LINE OF PAVEMENT MARKINGS, UNLESS OTHERWISE NOTED.
CURB RADI ARE 3 FEET UNLESS OTHERWISE NOTED.
CURBING SHALL BE PRECAST CONCRETE CURB (PCC) WITHIN THE SITE UNLESS OTHERWISE INDICATED ON THE PLANS.

Demolition

- CONTRACTOR SHALL REMOVE AND DISPOSE OF EXISTING MANMADE SURFACE FEATURES WITHIN THE LIMIT OF WORK INCLUDING BUILDINGS, STRUCTURES, PAVEMENTS, SLABS, CURBING, FENCES, UTILITY POLES, SIGNS, ETC. UNLESS INDICATED OTHERWISE ON THE DRAWINGS.
EXISTING UTILITIES SHALL BE TERMINATED, UNLESS OTHERWISE NOTED, IN CONFORMANCE WITH LOCAL, STATE AND INDIVIDUAL UTILITY COMPANY STANDARD SPECIFICATIONS AND DETAILS.

Erosion Control

- PRIOR TO STARTING ANY OTHER WORK ON THE SITE, THE CONTRACTOR SHALL NOTIFY APPROPRIATE AGENCIES AND SHALL INSTALL EROSION CONTROL MEASURES AS SHOWN ON THE PLANS AND AS IDENTIFIED IN FEDERAL, STATE, AND LOCAL APPROVAL DOCUMENTS PERTAINING TO THIS PROJECT.
CONTRACTOR SHALL INSPECT AND MAINTAIN EROSION CONTROL MEASURES ON A WEEKLY BASIS (MINIMUM) OR AS REQUIRED PER THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP).

Existing Conditions Information

- ALTA/NSPS LAND TITLE SURVEY WAS PREPARED BY LANGAN DATED DECEMBER 6, 2017. A PORTION OF THE OFF-SITE INFORMATION IS NOT SURVEYED.
A GEOTECHNICAL REPORT WAS PREPARED BY WHITESTONE ASSOCIATES, INC., DATED OCTOBER 17, 2017.

Document Use

- THESE PLANS AND CORRESPONDING CADD DOCUMENTS ARE INSTRUMENTS OF PROFESSIONAL SERVICE, AND SHALL NOT BE USED, IN WHOLE OR IN PART, FOR ANY PURPOSE OTHER THAN FOR WHICH IT WAS CREATED WITHOUT THE EXPRESSED, WRITTEN CONSENT OF VHB.
CONTRACTOR SHALL NOT RELY SOLELY ON ELECTRONIC VERSIONS OF PLANS, SPECIFICATIONS, AND DATA FILES THAT ARE OBTAINED FROM THE DESIGNERS, BUT SHALL VERIFY LOCATION OF PROJECT FEATURES IN ACCORDANCE WITH THE PAPER COPIES OF THE PLANS AND SPECIFICATIONS THAT ARE SUPPLIED AS PART OF THE CONTRACT DOCUMENTS.

Wilson Center Mixed-Use Development

29 Windsor Avenue  
Windsor, Connecticut

Table with 4 columns: No., Revision, Date, Apprd. It contains several rows of revision information.

Designed by: Checked by:
Issued for: Date:

Site Plan Approval July 28, 2023

Not For Construction

Legend, Abbreviations and General Notes

Professional seal for John J. Furman, Professional Engineer, License No. 1642. Includes drawing number C-1 and sheet number 1 of 13.



One Federal Street  
Building 103-3N  
Springfield, MA 01105  
413.747.7113



ZONE: B2  
MAP 81 BLOCK 9 LOT 10  
NOW OR FORMERLY  
STATE OF CONNECTICUT  
VOL 969 PAGE 258  
#9 WINDSOR AVENUE

ZONE: B2  
MAP 81 BLOCK 9 LOT 11  
NOW OR FORMERLY  
ADVANCED GAS COMPANY INC  
VOL 1179 PAGE 529  
#17 WINDSOR AVENUE



0 20 40 80 Feet

### Wilson Center Mixed-Use Development 29 Windsor Avenue Windsor, Connecticut

No.	Revision	Date	Appr.

Designed by: \_\_\_\_\_ Checked by: \_\_\_\_\_  
Issued for: \_\_\_\_\_ Date: \_\_\_\_\_  
**Site Plan Approval** July 28, 2023

Not For Construction  
Drawing Title  
**Demolition Plan**  
Drawing Number



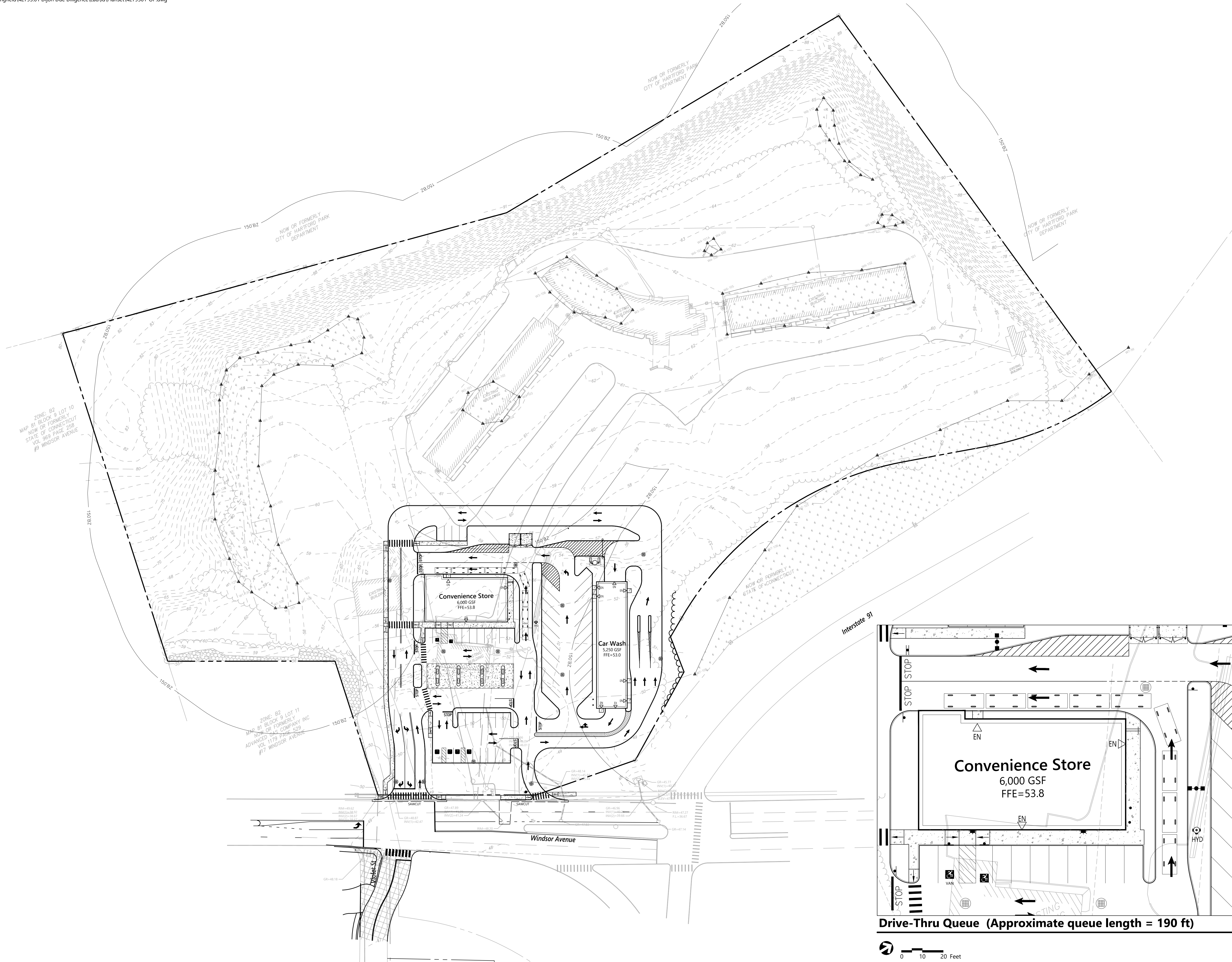
# C-2

Sheet 2 of 13

Project Number  
42795.01

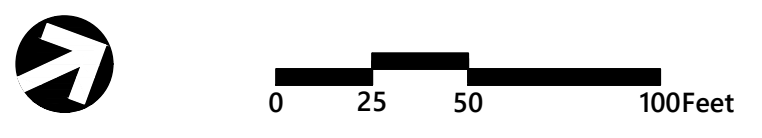


One Federal Street  
Building 103-3N  
Springfield, MA 01105  
413.747.7113



ZONE: B2  
MAP 81 BLOCK 9 LOT 10  
NOW OR FORMERLY  
STATE OF CONNECTICUT  
VOL 969 PAGE 258  
#9 WINDSOR AVENUE

ZONE: B2  
MAP 81 BLOCK 9 LOT 11  
NOW OR FORMERLY  
ADVANCED TAC COMPANY INC  
VOL 1179 PAGE 229  
#17 WINDSOR AVENUE



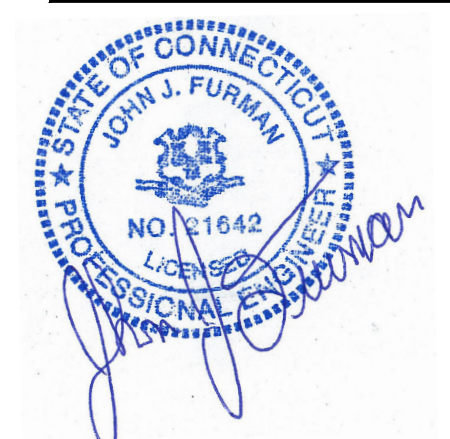
### Wilson Center Mixed-Use Development

29 Windsor Avenue  
Windsor, Connecticut

No.	Revision	Date	Appd.

Designed by: \_\_\_\_\_ Checked by: \_\_\_\_\_  
 Issued for: \_\_\_\_\_ Date: \_\_\_\_\_  
**Site Plan Approval** July 28, 2023

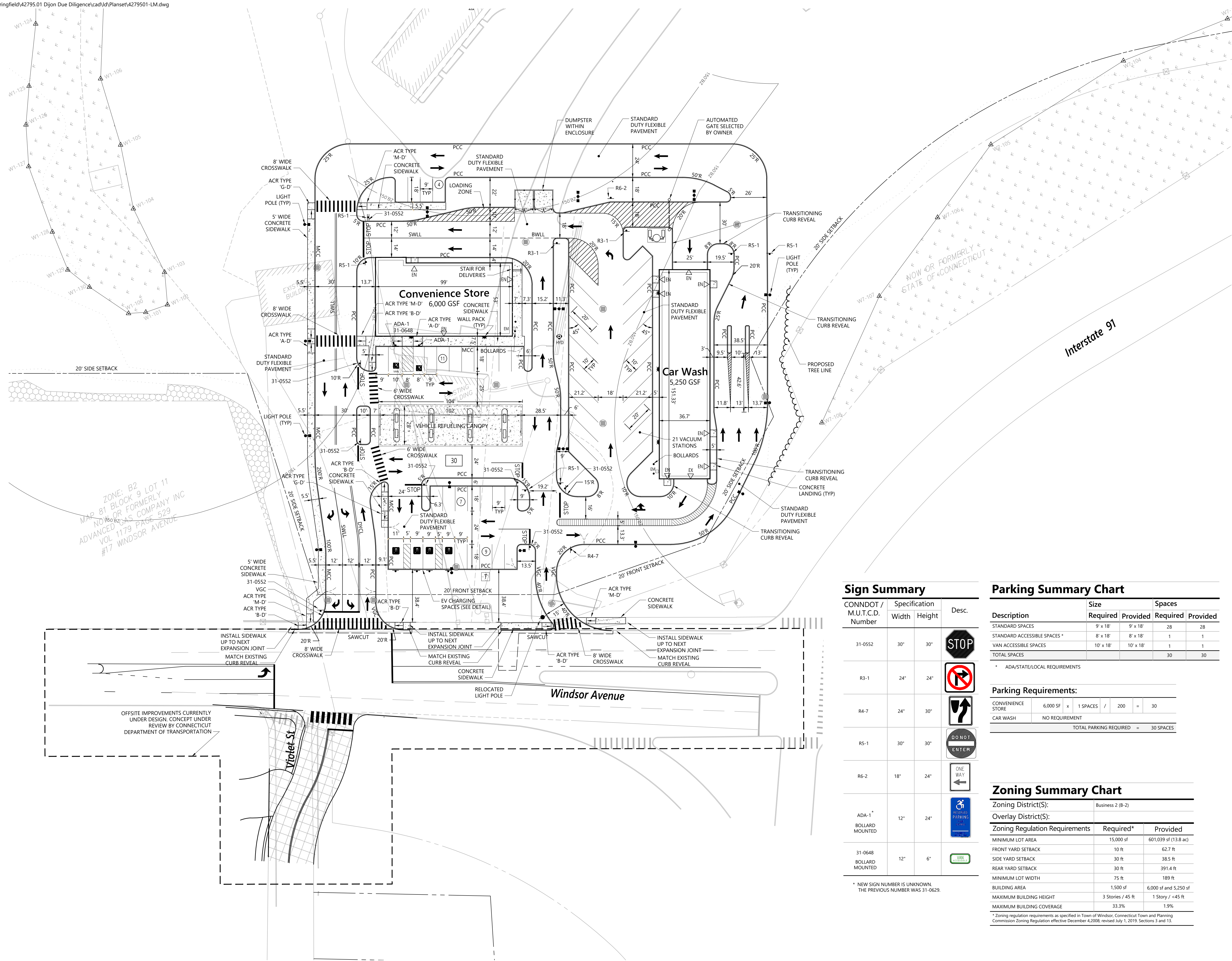
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 Drawing Title  
**Overall Site Plan**  
 Drawing Number



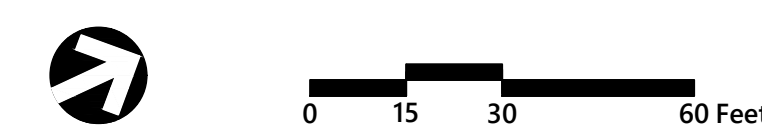
## C-3

Sheet 3 of 13

Project Number  
42795.01



ZONE: B2  
MAP 81 BLOCK 9 LOT 11  
NOW OR FORMERLY  
ADVANCED GAS COMPANY INC  
VOL 1179 PAGE 529  
#17 WINDSOR AVENUE



**Sign Summary**

CONNDOT / M.U.T.C.D. Number	Specification		Desc.
	Width	Height	
31-0552	30"	30"	
R3-1	24"	24"	
R4-7	24"	30"	
R5-1	30"	30"	
R6-2	18"	24"	
ADA-1 BOLLARD MOUNTED	12"	24"	
31-0648 BOLLARD MOUNTED	12"	6"	

\* NEW SIGN NUMBER IS UNKNOWN, THE PREVIOUS NUMBER WAS 31-0629.

**Parking Summary Chart**

Description	Size		Spaces	
	Required	Provided	Required	Provided
STANDARD SPACES	9' x 18'	9' x 18'	28	28
STANDARD ACCESSIBLE SPACES *	8' x 18'	8' x 18'	1	1
VAN ACCESSIBLE SPACES	10' x 18'	10' x 18'	1	1
TOTAL SPACES			30	30

\* ADA/STATE/LOCAL REQUIREMENTS

**Parking Requirements:**

CONVENIENCE STORE	6,000 SF	x	1 SPACES	/	200	=	30
CAR WASH	NO REQUIREMENT						
TOTAL PARKING REQUIRED = 30 SPACES							

**Zoning Summary Chart**

Zoning District(S):	Business 2 (B-2)	
Overlay District(S):		
Zoning Regulation Requirements	Required*	Provided
MINIMUM LOT AREA	15,000 sf	601,039 sf (13.8 ac)
FRONT YARD SETBACK	10 ft	62.7 ft
SIDE YARD SETBACK	30 ft	38.5 ft
REAR YARD SETBACK	30 ft	391.4 ft
MINIMUM LOT WIDTH	75 ft	189 ft
BUILDING AREA	1,500 sf	6,000 sf and 5,250 sf
MAXIMUM BUILDING HEIGHT	3 Stories / 45 ft	1 Story / <45 ft
MAXIMUM BUILDING COVERAGE	33.3%	1.9%

\* Zoning regulation requirements as specified in Town of Windsor, Connecticut Town and Planning Commission Zoning Regulation effective December 4, 2008, revised July 1, 2019, Sections 3 and 13.

**Wilson Center Mixed-Use Development**

29 Windsor Avenue  
Windsor, Connecticut

No.	Revision	Date	App'd.

Designed by: \_\_\_\_\_ Checked by: \_\_\_\_\_  
Issued for: \_\_\_\_\_ Date: \_\_\_\_\_  
**Site Plan Approval** July 28, 2023

Not For Construction

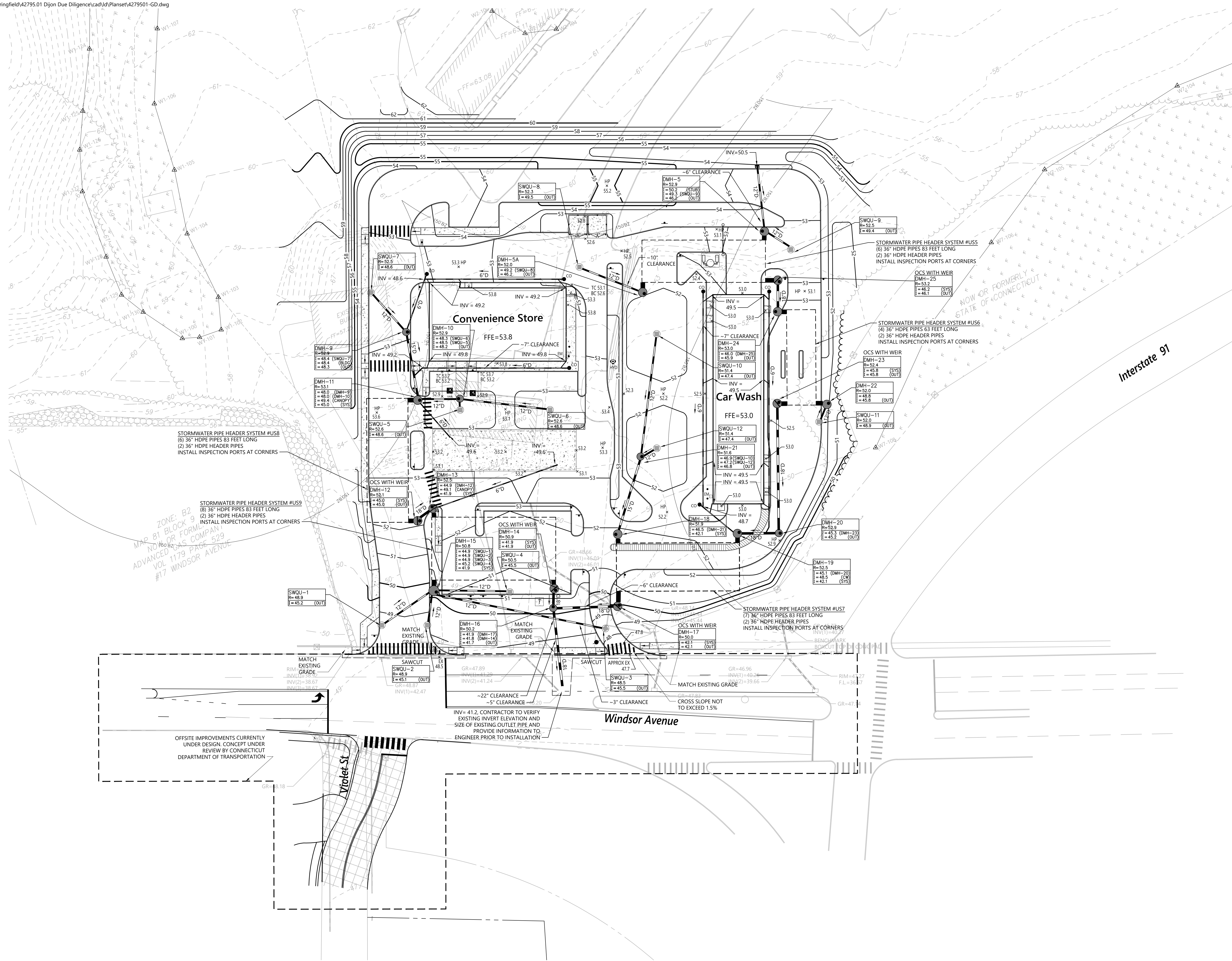
**Layout and Materials Plan**

Drawing Number: \_\_\_\_\_

**C-4**

Sheet **4** of **13**

Project Number: **42795.01**



MAP ZONE: B2  
MAP B1 BLOCK 9  
NO. 82-345 COMPANY  
ADVANCED PLANS  
VOL 1179 PAGE 529  
#17 WINDSOR AVENUE

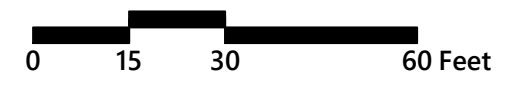
OFFSITE IMPROVEMENTS CURRENTLY UNDER DESIGN. CONCEPT UNDER REVIEW BY CONNECTICUT DEPARTMENT OF TRANSPORTATION

INV = 41.2. CONTRACTOR TO VERIFY EXISTING INVERT ELEVATION AND SIZE OF EXISTING OUTLET PIPE AND PROVIDE INFORMATION TO ENGINEER PRIOR TO INSTALLATION

CROSS SLOPE NOT TO EXCEED 1.5%

NOTES:

- 1) ALL CATCH BASINS ON SITE ARE TO BE CDS1515-3-C WATER QUALITY UNITS.
- 2) ALL ROOF DRAIN DOWNSPOUT CONNECTIONS SHALL BE 4" PVC. COORDINATE WITH ARCHITECTURAL AND MEP PLANS FOR FINAL LOCATIONS OF DOWNSPOUTS.



### Wilson Center Mixed-Use Development

29 Windsor Avenue  
Windsor, Connecticut

No.	Revision	Date	App'd.

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**Site Plan Approval** July 28, 2023

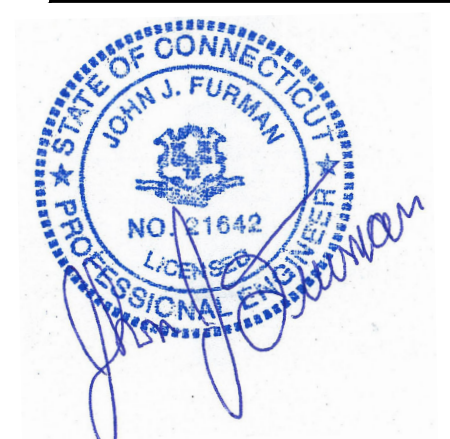
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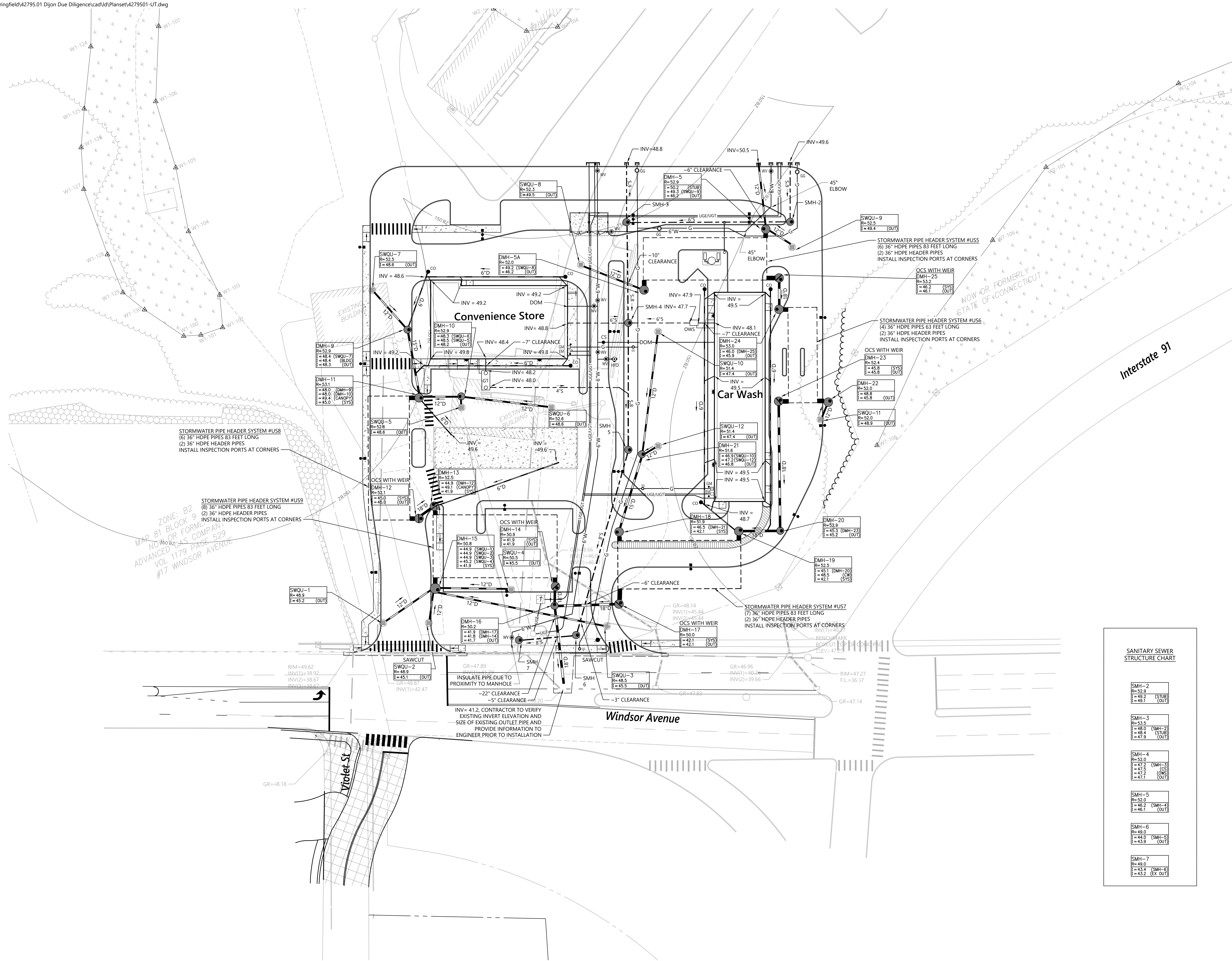
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**Grading and Drainage Plan**

Drawing Number  
**C-5**

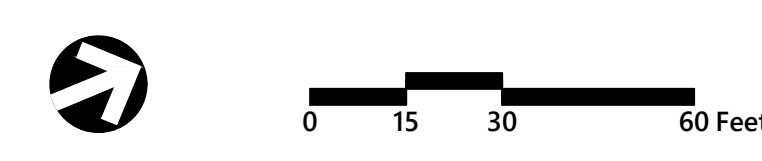
Sheet **5** of **13**

Project Number  
**42795.01**





MAP ZONE: B2  
MAP 81 BLOCK 9  
NO. 80 BZ OR FORM  
ADVANCED GAS COMPANY  
VOL 1179 PAGE 529  
#17 WINDSOR AVENUE



### Wilson Center Mixed-Use Development

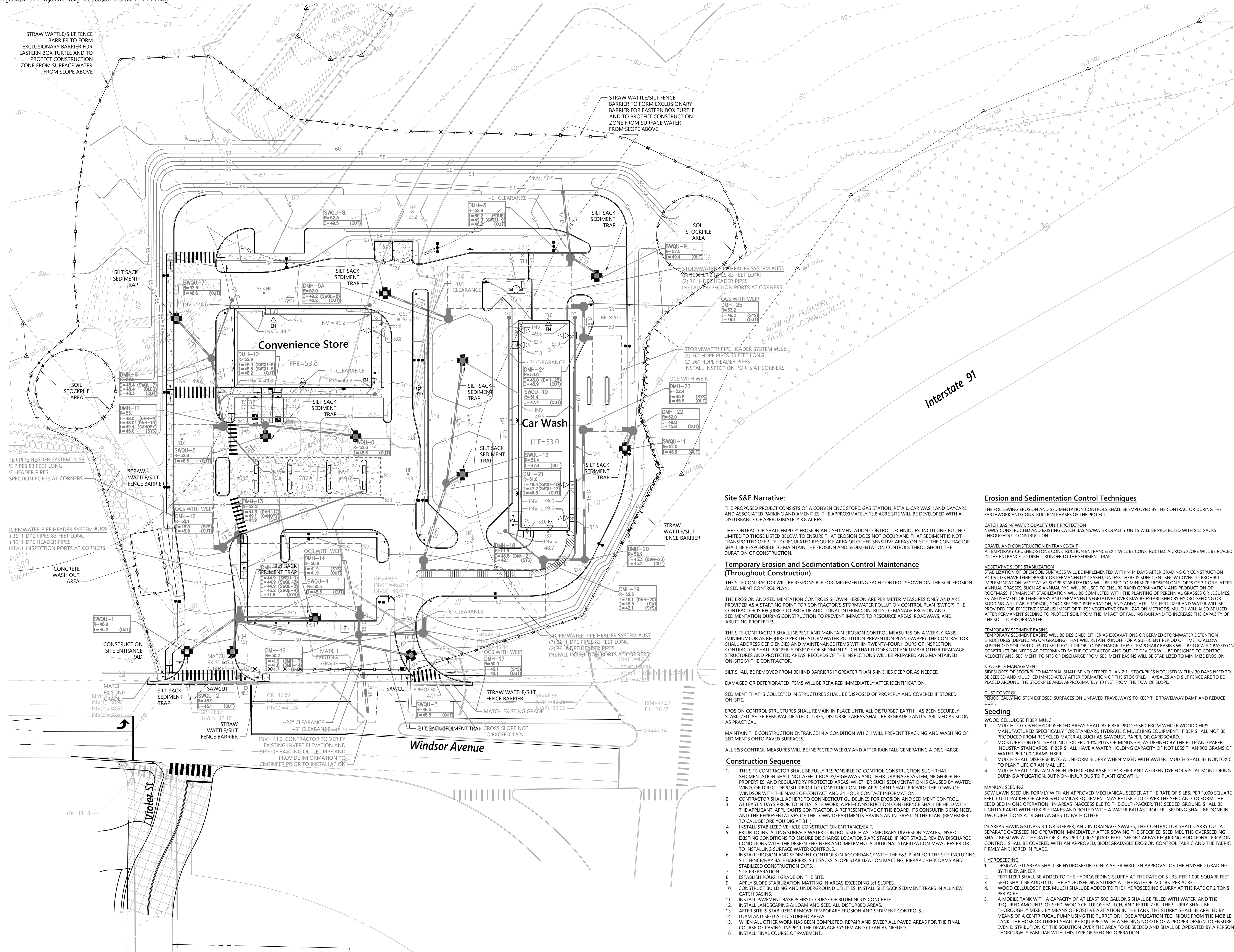
29 Windsor Avenue  
Windsor, Connecticut

No.	Revision	Date	App'd.

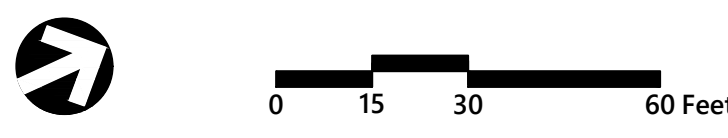
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Issued for: \_\_\_\_\_ Date: \_\_\_\_\_  
**Site Plan Approval** July 28, 2023

Not For Construction  
Drawing Title  
**Utility Plan**  
Drawing Number

**C-6**  
Sheet of  
6 13  
Project Number  
42795.01



NOTES: 1) CONTRACTOR TO PERFORM DAILY MONITORING OF SILT FENCE BARRIER TO ENSURE EASTERN BOX TURTLE DOES NOT ENTER WORKING ZONE. RELOCATE ANY TURTLES AWAY FROM SILT FENCE BARRIER FURTHER INTO UNDISTURBED SITE. SEE WINDSOR INLAND WETLAND PERMIT 22-143 ISSUED FOR PROJECT.



Wilson Center Mixed-Use Development 29 Windsor Avenue Windsor, Connecticut

Table with columns: No., Revision, Date, App'd. Includes a signature line for Site Plan Approval dated July 28, 2023.

Not For Construction Erosion and Sediment Control Plan

Professional seal for John J. Furman, Professional Engineer, License No. 1642. Includes drawing number C-7 and sheet number 7 of 13.

Site S&E Narrative: THE PROPOSED PROJECT CONSISTS OF A CONVENIENCE STORE, GAS STATION, RETAIL, CAR WASH AND DAYCARE AND ASSOCIATED PARKING AND AMENITIES. THE APPROXIMATELY 13.8 ACRE SITE WILL BE DEVELOPED WITH A DISTURBANCE OF APPROXIMATELY 3.8 ACRES.

THE CONTRACTOR SHALL EMPLOY EROSION AND SEDIMENTATION CONTROL TECHNIQUES, INCLUDING BUT NOT LIMITED TO THOSE LISTED BELOW, TO ENSURE THAT EROSION DOES NOT OCCUR AND THAT SEDIMENT IS NOT TRANSPORTED OFF-SITE TO REGULATED RESOURCE AREA OR OTHER SENSITIVE AREAS ON-SITE. THE CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN THE EROSION AND SEDIMENTATION CONTROLS THROUGHOUT THE DURATION OF CONSTRUCTION.

Temporary Erosion and Sedimentation Control Maintenance (Throughout Construction) THE SITE CONTRACTOR WILL BE RESPONSIBLE FOR IMPLEMENTING EACH CONTROL SHOWN ON THE SOIL EROSION & SEDIMENT CONTROL PLAN.

THE EROSION AND SEDIMENTATION CONTROLS SHOWN HEREON ARE PERIMETER MEASURES ONLY AND ARE PROVIDED AS A STARTING POINT FOR CONTRACTOR'S STORMWATER POLLUTION CONTROL PLAN (SWPP). THE CONTRACTOR IS REQUIRED TO PROVIDE ADDITIONAL INTERIM CONTROLS TO MANAGE EROSION AND SEDIMENTATION DURING CONSTRUCTION TO PREVENT IMPACTS TO RESOURCE AREAS, ROADWAYS, AND ADJUTING PROPERTIES.

THE SITE CONTRACTOR SHALL INSPECT AND MAINTAIN EROSION CONTROL MEASURES ON A WEEKLY BASIS (MINIMUM) OR AS REQUIRED PER THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP). THE CONTRACTOR SHALL ADDRESS DEFICIENCIES AND MAINTENANCE ITEMS WITHIN TWENTY-FOUR HOURS OF INSPECTION. CONTRACTOR SHALL PROPERLY DISPOSE OF SEDIMENT SUCH THAT IT DOES NOT ENCLUMBER OTHER DRAINAGE STRUCTURES AND PROTECTED AREAS. RECORDS OF THE INSPECTIONS WILL BE PREPARED AND MAINTAINED ON-SITE BY THE CONTRACTOR.

SILT SHALL BE REMOVED FROM BEHIND BARRIERS IF GREATER THAN 6-INCHES DEEP OR AS NEEDED. DAMAGED OR DETERIORATED ITEMS WILL BE REPAIRED IMMEDIATELY AFTER IDENTIFICATION.

SEDIMENT THAT IS COLLECTED IN STRUCTURES SHALL BE DISPOSED OF PROPERLY AND COVERED IF STORED ON-SITE. EROSION CONTROL STRUCTURES SHALL REMAIN IN PLACE UNTIL ALL DISTURBED EARTH HAS BEEN SECURELY STABILIZED. AFTER REMOVAL OF STRUCTURES, DISTURBED AREAS SHALL BE REGRADED AND STABILIZED AS SOON AS PRACTICAL.

MAINTAIN THE CONSTRUCTION ENTRANCE IN A CONDITION WHICH WILL PREVENT TRACKING AND WASHING OF SEDIMENTS ONTO PAVED SURFACES. ALL ERS CONTROL MEASURES WILL BE INSPECTED WEEKLY AND AFTER RAINFALL GENERATING A DISCHARGE.

- Construction Sequence 1. THE SITE CONTRACTOR SHALL BE FULLY RESPONSIBLE TO CONTROL CONSTRUCTION SUCH THAT SEDIMENTATION SHALL NOT AFFECT ROADS/HIGHWAYS AND THEIR DRAINAGE SYSTEM, NEIGHBORING PROPERTIES, AND REGULATORY PROTECTED AREAS. 2. CONTRACTOR SHALL ADHERE TO CONNECTICUT GUIDELINES FOR EROSION AND SEDIMENT CONTROL AT LEAST 5 DAYS PRIOR TO INITIAL SITE WORK. 3. PRIOR TO CONSTRUCTION, THE APPLICANT SHALL PROVIDE THE TOWN OF WINDSOR WITH THE NAME OF CONTACT AND 24 HOUR CONTACT INFORMATION. 4. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES AND STRUCTURES. 5. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES AND STRUCTURES. 6. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES AND STRUCTURES. 7. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES AND STRUCTURES. 8. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES AND STRUCTURES. 9. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES AND STRUCTURES. 10. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES AND STRUCTURES. 11. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES AND STRUCTURES. 12. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES AND STRUCTURES. 13. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES AND STRUCTURES. 14. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES AND STRUCTURES. 15. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES AND STRUCTURES. 16. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES AND STRUCTURES.

Erosion and Sedimentation Control Techniques THE FOLLOWING EROSION AND SEDIMENTATION CONTROLS SHALL BE EMPLOYED BY THE CONTRACTOR DURING THE EXISTING AND CONSTRUCTION PHASES OF THE PROJECT.

CATCH BASIN/WATER QUALITY UNIT PROTECTION NEWLY CONSTRUCTED AND EXISTING CATCH BASINS/WATER QUALITY UNITS WILL BE PROTECTED WITH SILT SACKS THROUGHOUT CONSTRUCTION.

GRAVEL AND CONSTRUCTION ENTRANCE/EXIT A TEMPORARY CRUSHED-STONE CONSTRUCTION ENTRANCE/EXIT WILL BE CONSTRUCTED. A CROSS SLOPE WILL BE PLACED IN THE ENTRANCE TO DIRECT RUNOFF TO THE SEDIMENT TRAP.

VEGETATIVE SLOPE STABILIZATION STABILIZATION OF OPEN SOIL SURFACES WILL BE IMPLEMENTED WITHIN 14 DAYS AFTER GRADING OR CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED. UNLESS THERE IS SUFFICIENT SNOW COVER TO PROHIBIT IMPLEMENTATION, VEGETATIVE SLOPE STABILIZATION WILL BE USED TO MINIMIZE EROSION ON SLOPES OF 3:1 OR FLATTER. ANNUAL GRASSES, SUCH AS ANNUAL RYE, WILL BE USED TO ENSURE RAPID GERMINATION AND PRODUCTION OF ROOTMASS. PERMANENT STABILIZATION WILL BE COMPLETED WITH THE PLANTING OF PERENNIAL GRASSES OR LEGUMES. ESTABLISHMENT OF TEMPORARY AND PERMANENT VEGETATIVE COVER MAY BE ESTABLISHED BY HYDRO-SEEDING OR SCODING, A SUITABLE TOPSOIL, GOOD SEEDING PREPARATION, AND ADEQUATE LINE, FERTILIZER AND WATER WILL BE PROVIDED FOR EFFECTIVE ESTABLISHMENT OF THESE VEGETATIVE STABILIZATION METHODS. MULCH WILL ALSO BE USED AFTER PERMANENT SEEDING TO PROTECT SOIL FROM THE IMPACT OF FALLING RAIN AND TO INCREASE THE CAPACITY OF THE SOIL TO ABSORB WATER.

TEMPORARY SEDIMENT BASINS TEMPORARY SEDIMENT BASINS WILL BE DESIGNED EITHER AS EXCAVATIONS OR BERMED STORMWATER DETENTION STRUCTURES (DEPENDING ON GRADING) THAT WILL RETAIN RUNOFF FOR A SUFFICIENT PERIOD OF TIME TO ALLOW SUSPENDED SOIL PARTICLES TO SETTLE OUT PRIOR TO DISCHARGE. THESE TEMPORARY BASINS WILL BE LOCATED BASED ON CONSTRUCTION NEEDS AS DETERMINED BY THE CONTRACTOR AND OUTLET DEVICES WILL BE DESIGNED TO CONTROL VELOCITY AND SEDIMENT POINTS OF DISCHARGE FROM SEDIMENT BASINS WILL BE STABILIZED TO MINIMIZE EROSION.

STOCKPILE MANAGEMENT SIDESLOPES OF STOCKPILED MATERIAL SHALL BE NO STEEPER THAN 2:1. STOCKPILES NOT USED WITHIN 30 DAYS NEED TO BE SEEDED AND MULCHED IMMEDIATELY AFTER FORMATION OF THE STOCKPILE. HAYBALES AND SILT FENCE ARE TO BE PLACED AROUND THE STOCKPILE AREA APPROXIMATELY 10 FEET FROM THE TOW OF SLOPE.

DUST CONTROL PERIODICALLY MOISTEN EXPOSED SURFACES ON UNPAVED TRAVELWAYS TO KEEP THE TRAVELWAY DAMP AND REDUCE DUST.

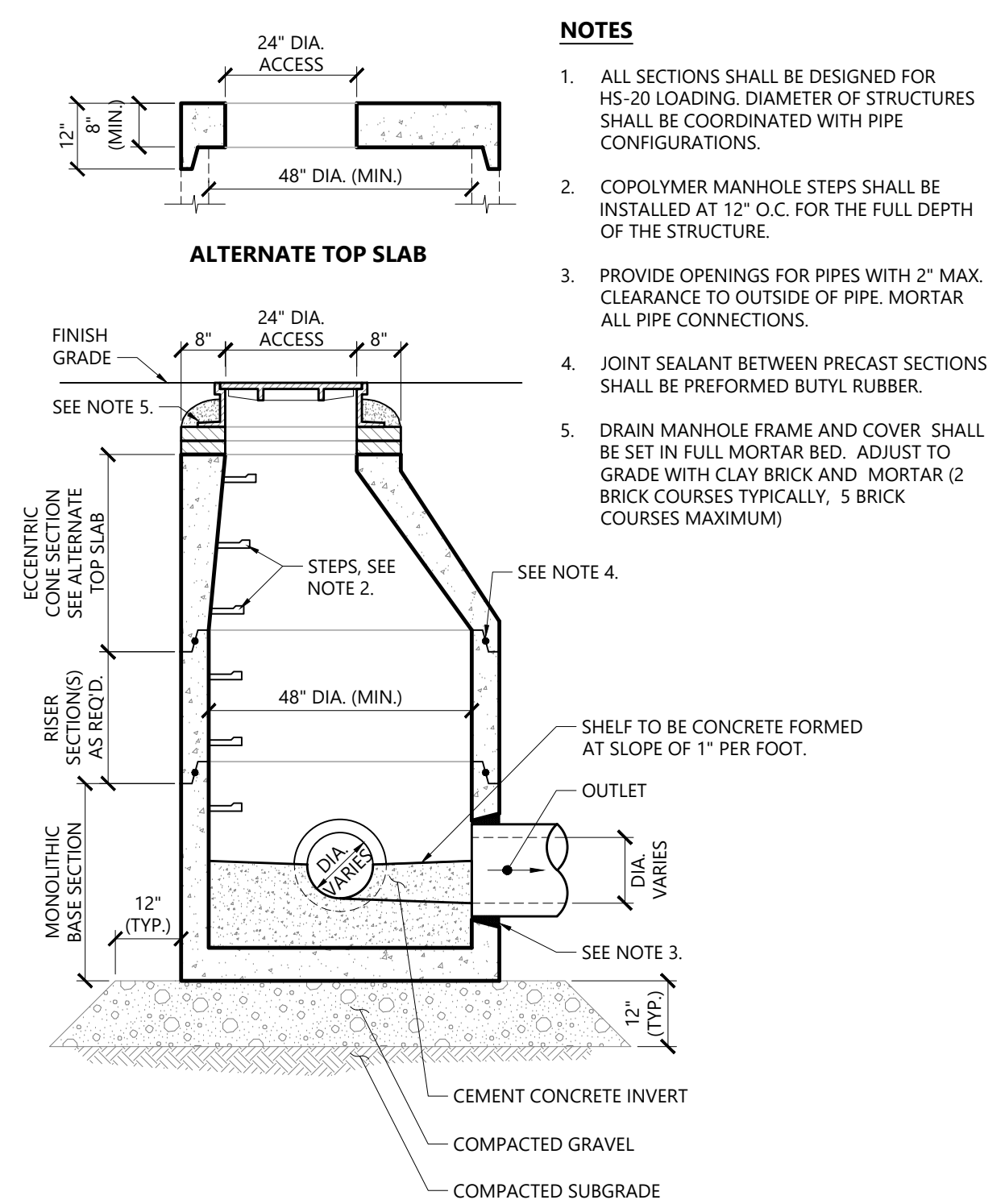
Seeding WOOD CELLULOSE FIBER MULCH MULCH TO COVER HYDROSEEDING AREAS SHALL BE FIBER-PROCESSED FROM WHOLE WOOD CHIPS MANUFACTURED SPECIFICALLY FOR STANDARD HYDRAULIC MULCHING EQUIPMENT. FIBER SHALL NOT BE PRODUCED FROM RECYCLED MATERIAL SUCH AS SAWDUST, PAPER, OR CARBOABARD. 2. MULCH SHALL BE APPLIED TO THE HYDROSEEDING SLURRY AT THE RATE OF 220 LBS. PER ACRE. 3. MULCH SHALL HAVE A WATER HOLDING CAPACITY OF NOT LESS THAN 900 GRAMS OF WATER PER 100 GRAMS FIBER. 4. MULCH SHALL DISPENSE INTO A UNIFORM SLURRY WHEN MIXED WITH WATER. MULCH SHALL BE NONTOXIC TO PLANT LIFE OR ANIMAL LIFE. 5. MULCH SHALL CONTAIN A NON-PETROLEUM BASED TACKIFIER AND A GREEN DYE FOR VISUAL MONITORING DURING APPLICATION, BUT NON-INJURIOUS TO PLANT GROWTH.

MANUAL SEEDING SOW LAWN SEED UNIFORMLY WITH AN APPROVED MECHANICAL SEEDER AT THE RATE OF 5 LBS. PER 1,000 SQUARE FEET. CULTI-PACKER OR APPROVED SIMILAR EQUIPMENT MAY BE USED TO COVER THE SEED AND TO FORM THE SEED BED IN ONE OPERATION. IN AREAS INACCESSIBLE TO THE CULTI-PACKER, THE SEEDING GROUND SHALL BE LIGHTLY RAKED WITH FLEXIBLE RAKES AND ROLLED WITH A WATER BALLAST ROLLER. SEEDING SHALL BE DONE IN TWO DIRECTIONS AT RIGHT ANGLES TO EACH OTHER.

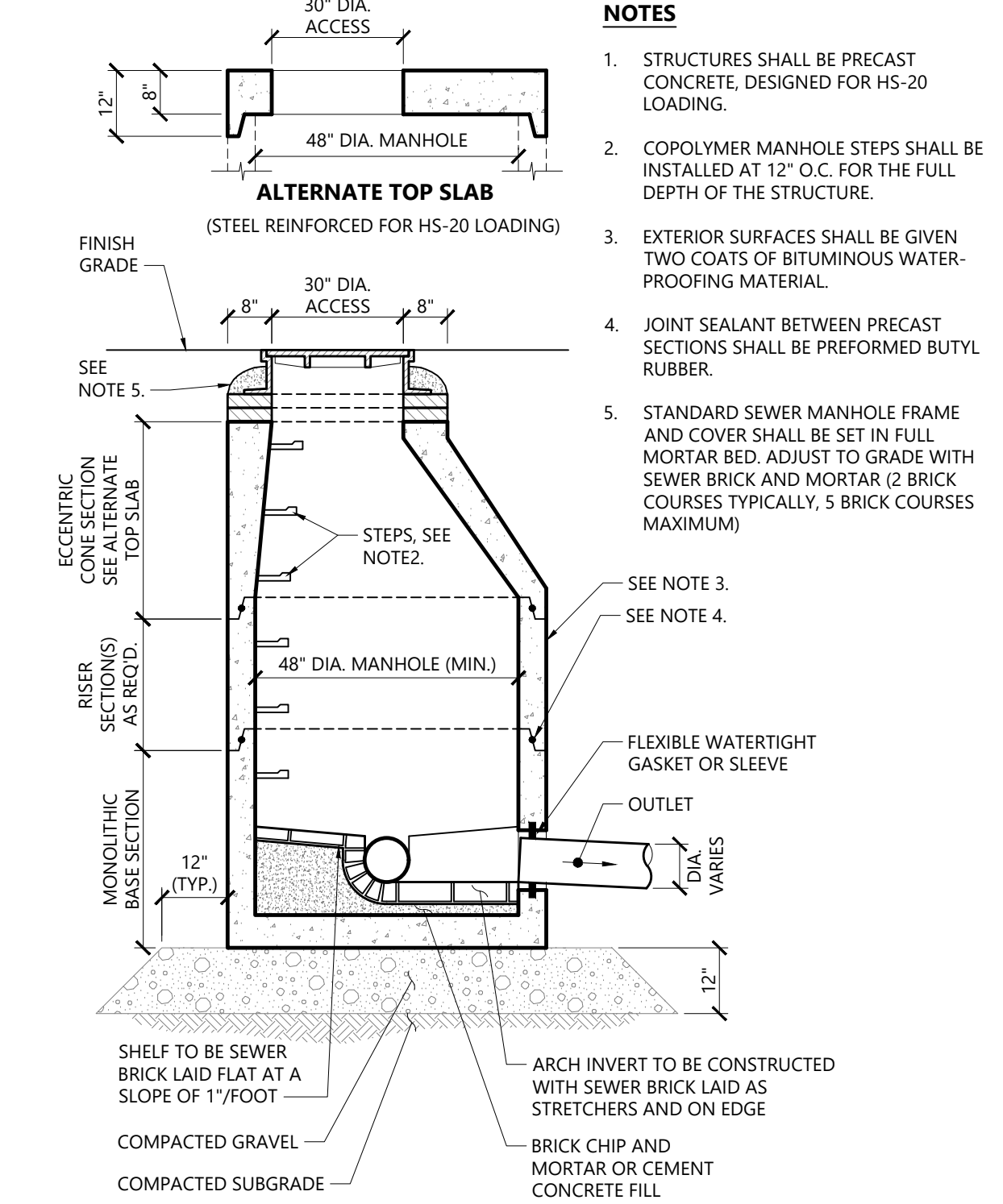
IN AREAS HAVING SLOPES 3:1 OR STEEPER, AND IN DRAINAGE SWALES, THE CONTRACTOR SHALL CARRY OUT A SEPARATE OVERSEEDING OPERATION IMMEDIATELY AFTER SOWING THE SPECIFIED SEED MIX. THE OVERSEEDING SHALL BE SOWN AT THE RATE OF 3 LBS. PER 1,000 SQUARE FEET. SEEDING AREAS REQUIRING ADDITIONAL EROSION CONTROL SHALL BE COVERED WITH AN APPROVED, BIODEGRADABLE EROSION CONTROL FABRIC AND THE FABRIC FIRMLY ANCHORED IN PLACE.

HYDROSEEDING DESIGNATED AREAS SHALL BE HYDROSEEDING ONLY AFTER WRITTEN APPROVAL OF THE FINISHED GRADING BY THE ENGINEER. 2. FERTILIZER SHALL BE ADDED TO THE HYDROSEEDING SLURRY AT THE RATE OF 5 LBS. PER 1,000 SQUARE FEET. 3. SEED SHALL BE ADDED TO THE HYDROSEEDING SLURRY AT THE RATE OF 220 LBS. PER ACRE. 4. WOOD CELLULOSE FIBER MULCH SHALL BE ADDED TO THE HYDROSEEDING SLURRY AT THE RATE OF 2 TONS PER ACRE. 5. A MOBILE TANK WITH A CAPACITY OF AT LEAST 500 GALLONS SHALL BE FILLED WITH WATER AND THE REQUIRED AMOUNTS OF SEED, WOOD CELLULOSE MULCH, AND FERTILIZER. THE SLURRY SHALL BE THOROUGHLY MIXED BY MEANS OF POSITIVE AGITATION IN THE TANK. THE SLURRY SHALL BE APPLIED BY MEANS OF A CENTRIFUGAL PUMP USING THE TURRET OR HOSE APPLICATION TECHNIQUE FROM THE MOBILE TANK. THE HOSE OR TURRET SHALL BE EQUIPPED WITH A SEEDING NOZZLE OF A PROPER DESIGN TO ENSURE EVEN DISTRIBUTION OF THE SOLUTION OVER THE AREA TO BE SEEDING AND SHALL BE OPERATED BY A PERSON THOROUGHLY FAMILIAR WITH THIS TYPE OF SEEDING OPERATION.





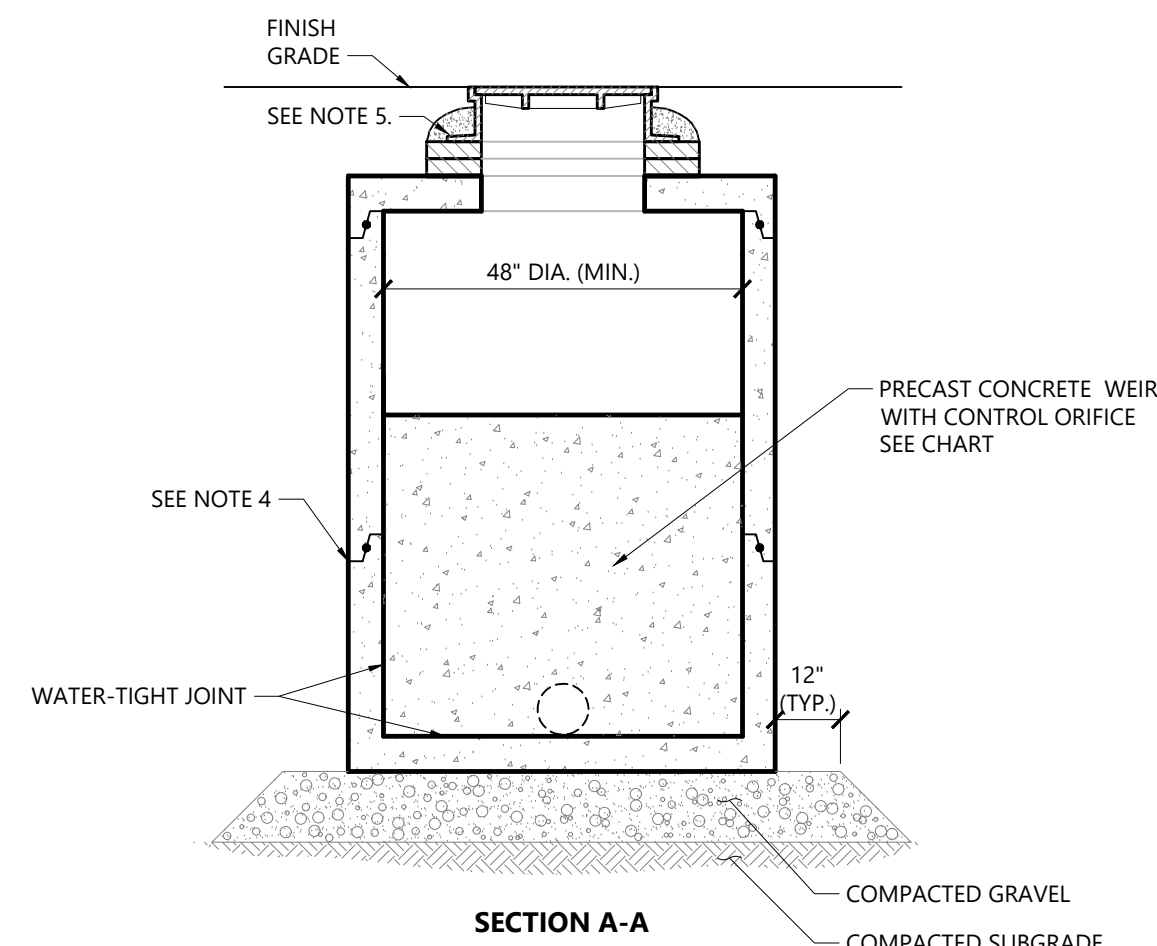
**Drain Manhole (DMH)** 1/16  
N.T.S. Source: VHB LD\_115



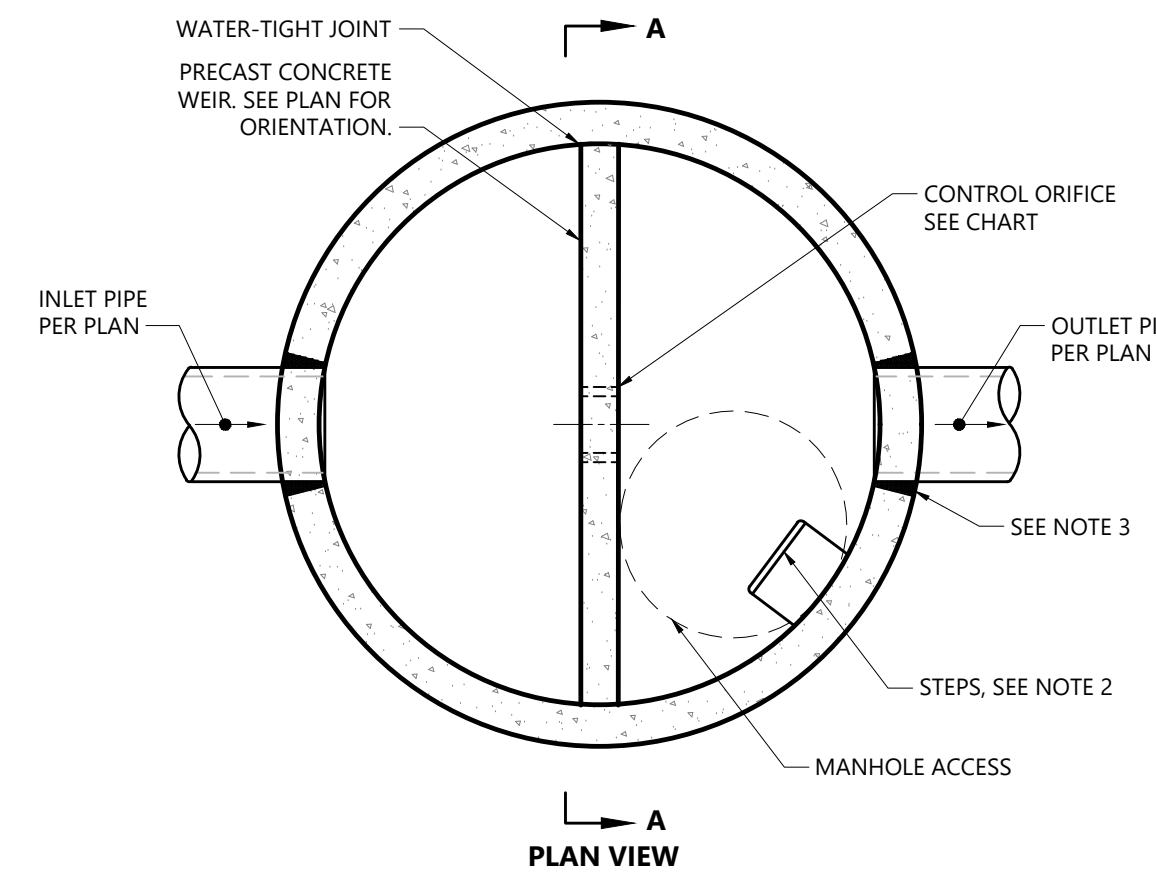
**Sanitary Sewer Manhole (SMH)** 1/16  
N.T.S. Source: VHB LD\_200

**OUTLET STRUCTURE CHART**

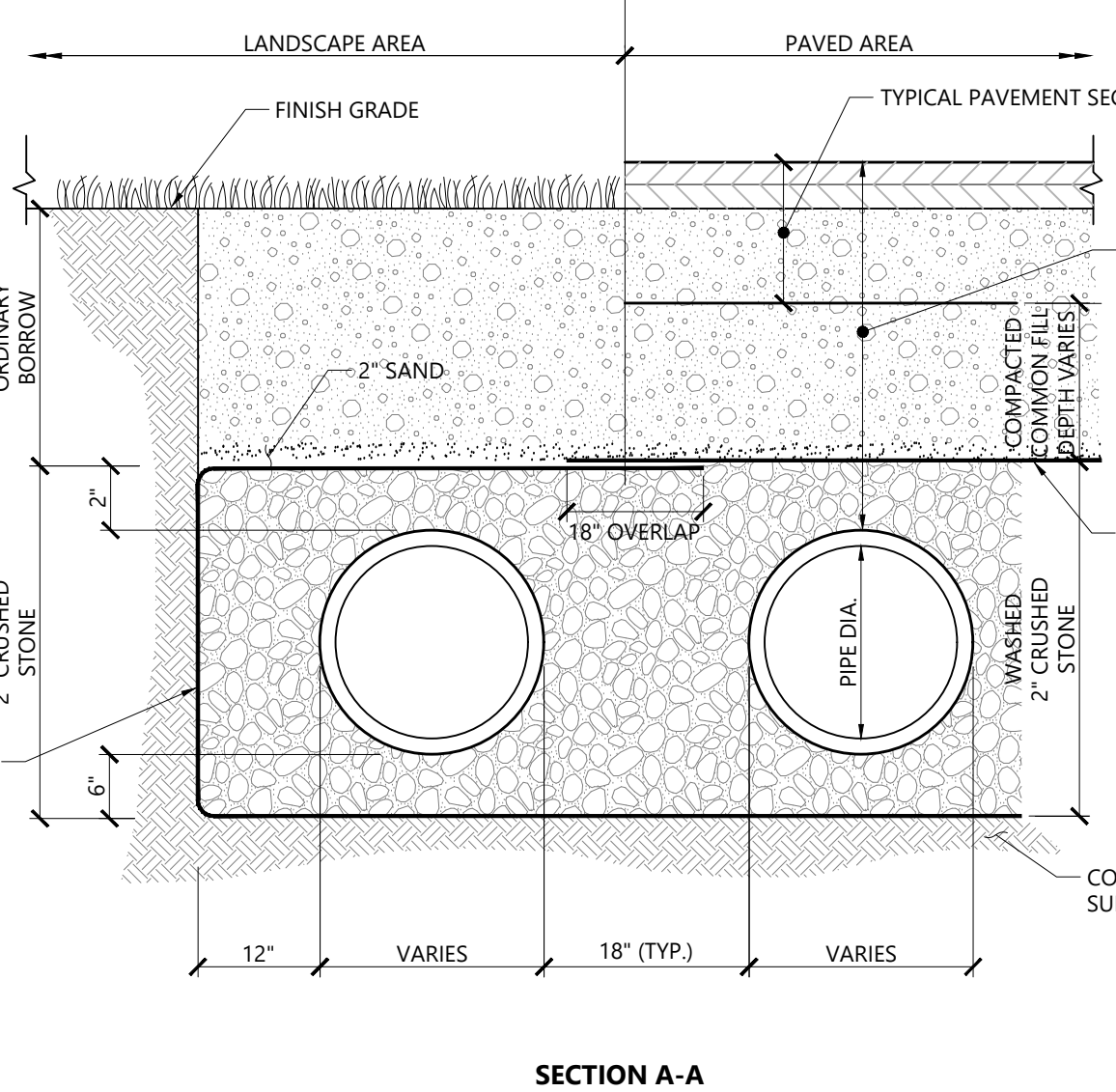
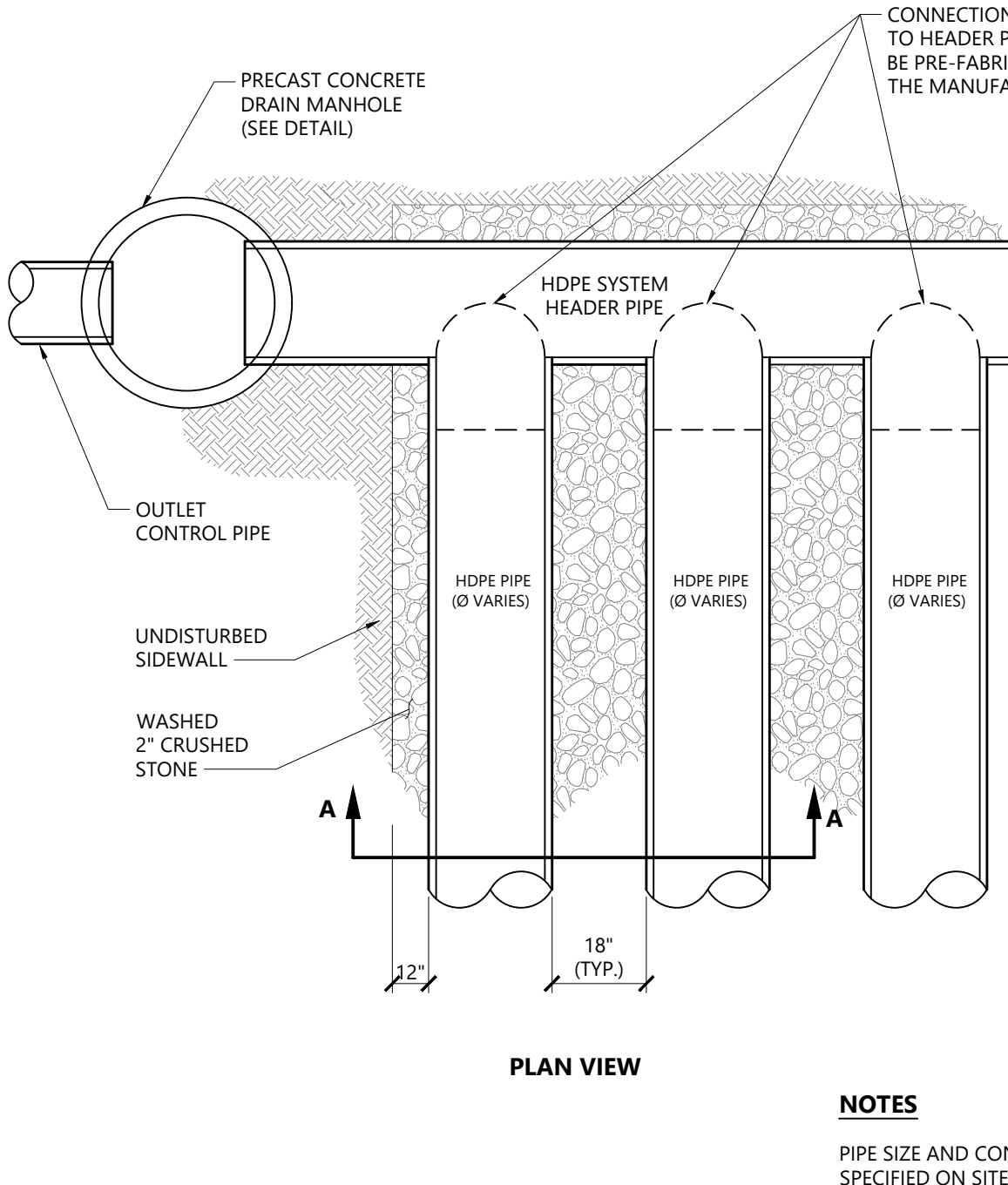
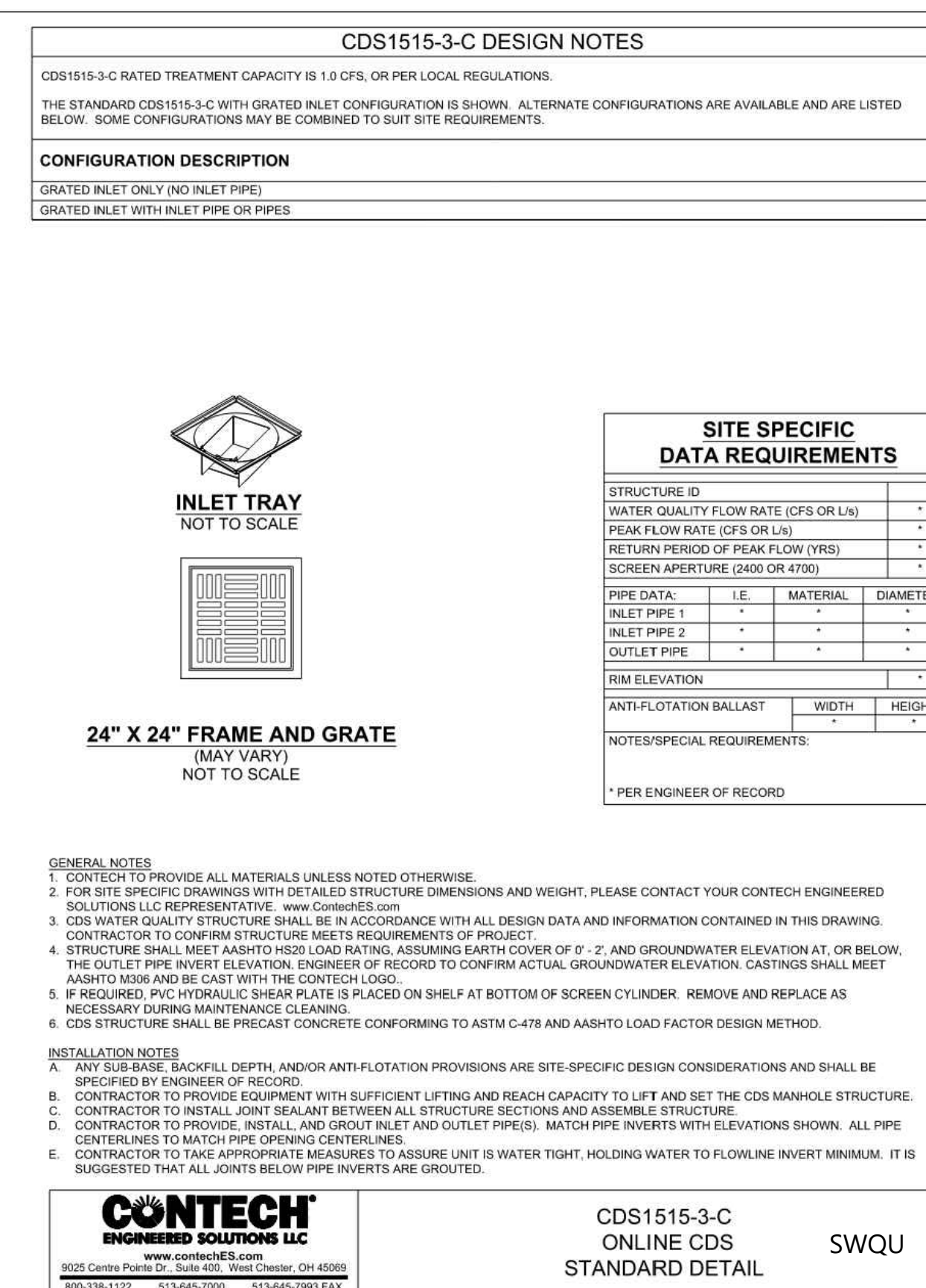
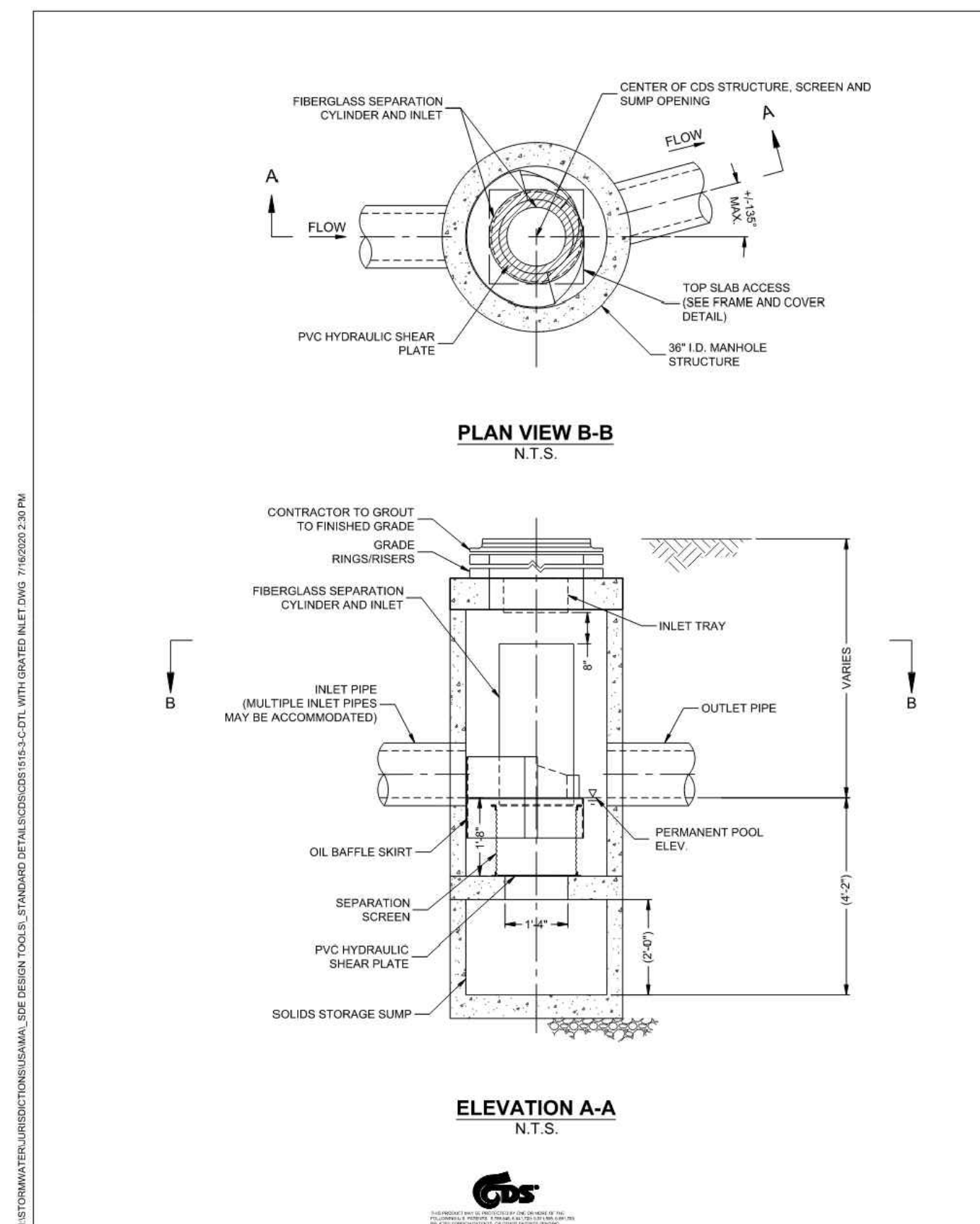
UNDER-GROUND SYSTEM	STRUCTURE NUMBER	TOP OF WEIR ELEVATION	DIMENSIONS ORIFICE A	INVERT ORIFICE A
US-5	DMH-25	49.2	6"	46.1
US-6	DMH-23	49.2	6"	45.8
US-7	DMH-17	45.1	6"	42.1
US-8	DMH-12	48.0	6"	45.0
US-9	DMH-14	45.4	6"	41.9



**Outlet Control Structure with Weir (OCS)** 3/20  
N.T.S. Source: VHB REV LD\_162A



- NOTES**
- ALL SECTIONS SHALL BE DESIGNED FOR HS-20 LOADING. DIAMETER OF STRUCTURES SHALL BE COORDINATED WITH PIPE CONFIGURATIONS.
  - COPOLYMER MANHOLE STEPS SHALL BE INSTALLED AT 12" O.C. FOR THE FULL DEPTH OF THE STRUCTURE.
  - FOR HDPE, PVC, AND DI PIPE, PROVIDE FLEXIBLE BOOT CONNECTION INSTALLED PER MANUFACTURER'S RECOMMENDATIONS. FOR RCP, PROVIDE OPENINGS FOR PIPES WITH 2" MAX. CLEARANCE TO OUTSIDE OF PIPE AND MORTAR CONNECTIONS.
  - JOINT SEALANT BETWEEN PRECAST SECTIONS SHALL BE PREFORMED BUTYL RUBBER.
  - DRAIN MANHOLE FRAME AND COVER SHALL BE SET IN FULL MORTAR BED. ADJUST TO GRADE WITH CLAY BRICK AND MORTAR (2 BRICK COURSES TYPICALLY, 5 BRICK COURSES MAXIMUM)



**Stormwater Pipe/Header System** 1/16  
N.T.S. Source: VHB REV LD\_183

**Wilson Center  
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No.	Revision	Date	App'd.

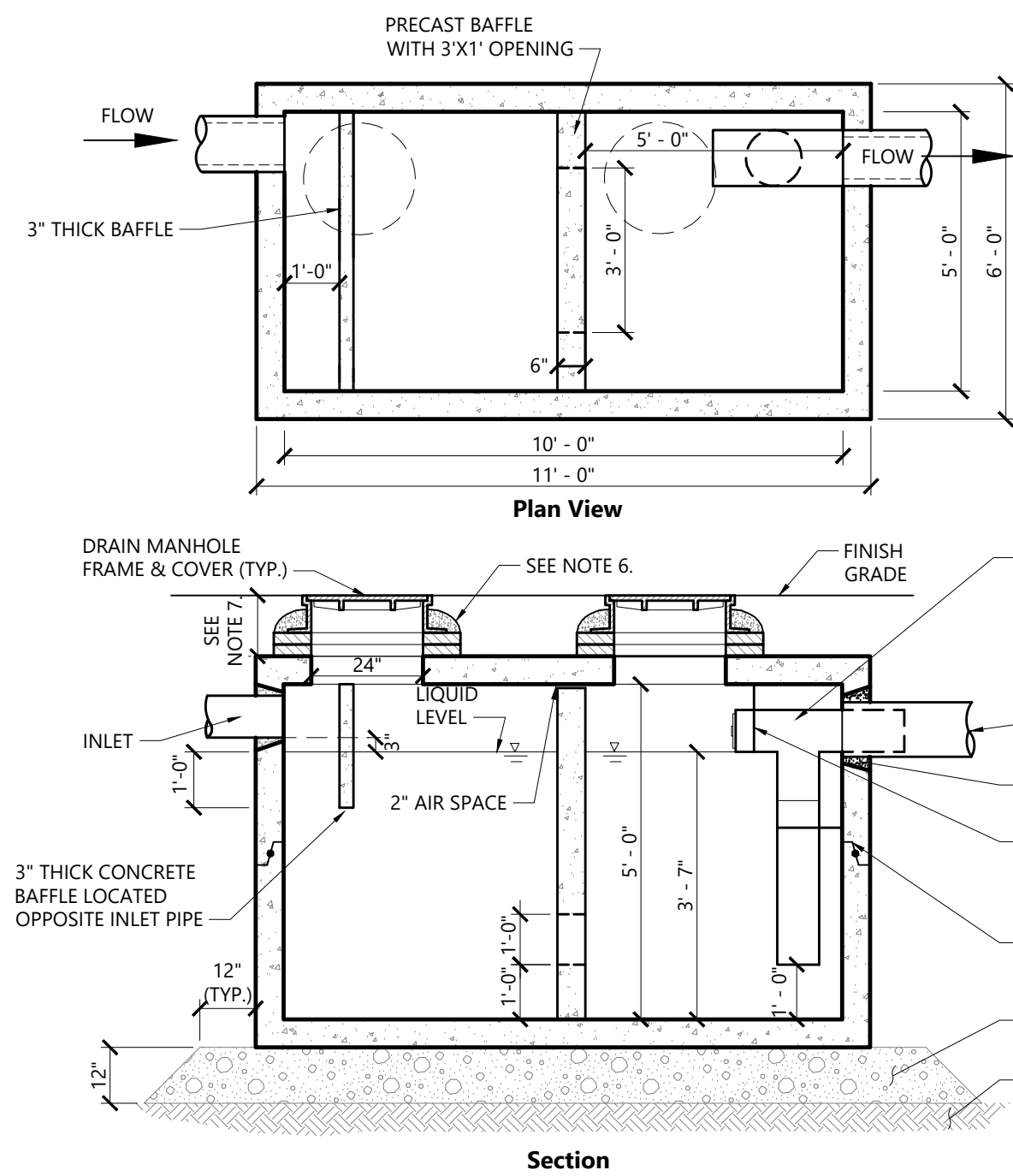
Designed by: \_\_\_\_\_ Checked by: \_\_\_\_\_  
Issued for: \_\_\_\_\_ Date: \_\_\_\_\_

**Site Plan Approval** July 28, 2023

Not For Construction  
Drawing Title  
**Details**  
Drawing Number

**C-8**

Sheet 8 of 13

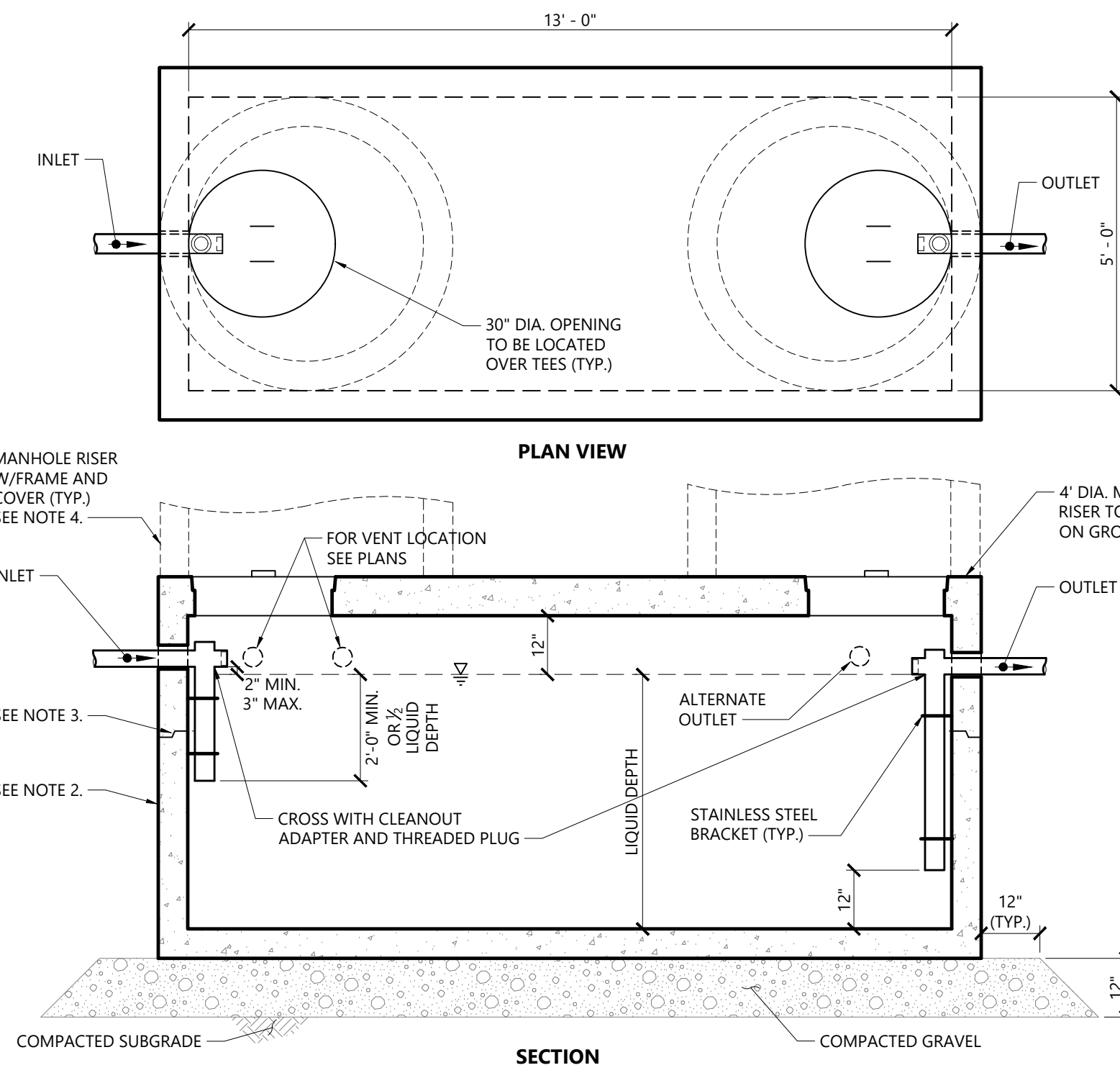


**Oil/Water Separator (OWS)**

N.T.S. Source: VHB 12/19 LD\_144

**NOTES**

- OIL/WATER SEPARATOR SHALL BE A STANDARD PRECAST SEPTIC TANK WITH PRECAST BAFFLES AS SHOWN.
- STRUCTURE SHALL BE DESIGNED FOR HS-20 LOADING.
- SEE PLANS FOR ACTUAL DISPOSITION OF PIPING LAYOUT FOR COORDINATION OF MANHOLE ACCESS AND BAFFLE.
- FOR HDPE, PVC, AND DI PIPE, PROVIDE FLEXIBLE BOOT CONNECTION INSTALLED PER MANUFACTURER'S RECOMMENDATIONS. FOR RCP, PROVIDE OPENINGS FOR PIPES WITH 2" MAX. CLEARANCE TO OUTSIDE OF PIPE AND MORTAR CONNECTIONS.
- JOINT SEALANT BETWEEN ALL SECTIONS SHALL BE PERFORMED BUTYL RUBBER.
- STANDARD DRAIN MANHOLE FRAME AND COVER SHALL BE SET IN FULL MORTAR BED. ADJUST TO GRADE WITH CLAY BRICK AND MORTAR (2 BRICK COURSES TYPICALLY, 5 BRICK COURSES MAXIMUM).
- STANDARD BARREL BLOCK AND MORTAR SHALL BE USED TO BRING MANHOLE FRAME AND COVER TO FINISHED GRADE WHEN DEPTH TO TOP OF STRUCTURE EXCEEDS 18 INCHES.
- WHEN CONNECTING TO DISCHARGE PIPE THE PVC OUTLET TEE AND PIPE SHALL BE SIZED TO FIT SNUGGLY INSIDE THE DISCHARGE PIPE AND THE JOINT SHALL BE SEALED WITH NON-SHRINK CEMENT GROUT.
- GEOMETRY OF PIPING AND LOCATION OF BAFFLES AND MANHOLE ACCESS TO BE CONSISTENT WITH UTILITY PLAN.



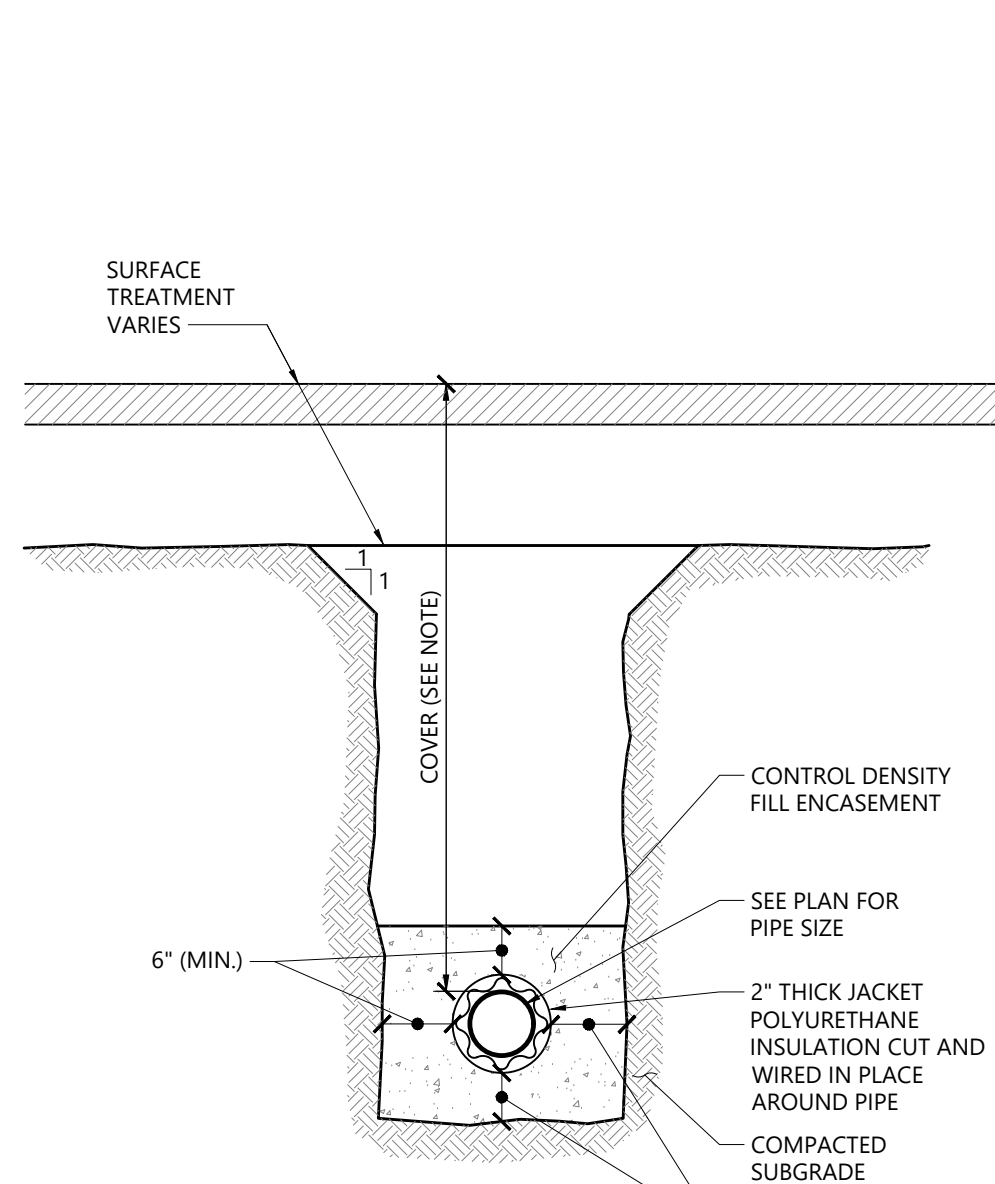
**Precast Concrete Grease Trap (GT)**

N.T.S. Source: VHB 12/19 LD\_210

GREASE TRAP	
SIZE (GAL.)	LIQUID DEPTH
2,000	4'-4"
2,500	5'-4"
3,000	6'-5"
3,500	7'-6"

**NOTES**

- STRUCTURE SHALL BE DESIGNED FOR HS-20 LOADING.
- EXTERIOR SURFACES SHALL BE GIVEN TWO COATS OF BITUMINOUS WATER-PROOFING MATERIAL.
- JOINT SEALANT BETWEEN PRECAST SECTIONS SHALL BE PERFORMED BUTYL RUBBER.
- STANDARD 30-INCH SEWER MANHOLE FRAME AND COVER SHALL BE LOCATED OVER CROSSES AND SET IN FULL MORTAR BED. ADJUST TO GRADE WITH SEWER BRICK AND MORTAR (2 BRICK COURSES TYPICALLY, 5 BRICK COURSES MAXIMUM).
- PIPING SHALL BE SCH 40 PVC WITH SOLVENT WELDED JOINTS. INTERNAL PIPE DIAMETER SHALL BE SAME SIZE AS OUTLET PIPE.
- FINAL DESIGN OF GREASE TRAP TO BE BY PLUMBING ENGINEER.
- THE INSTALLATION OF GREASE TRAP, THE PIPING TO AND 10 FEET BEYOND IS BY PLUMBER.

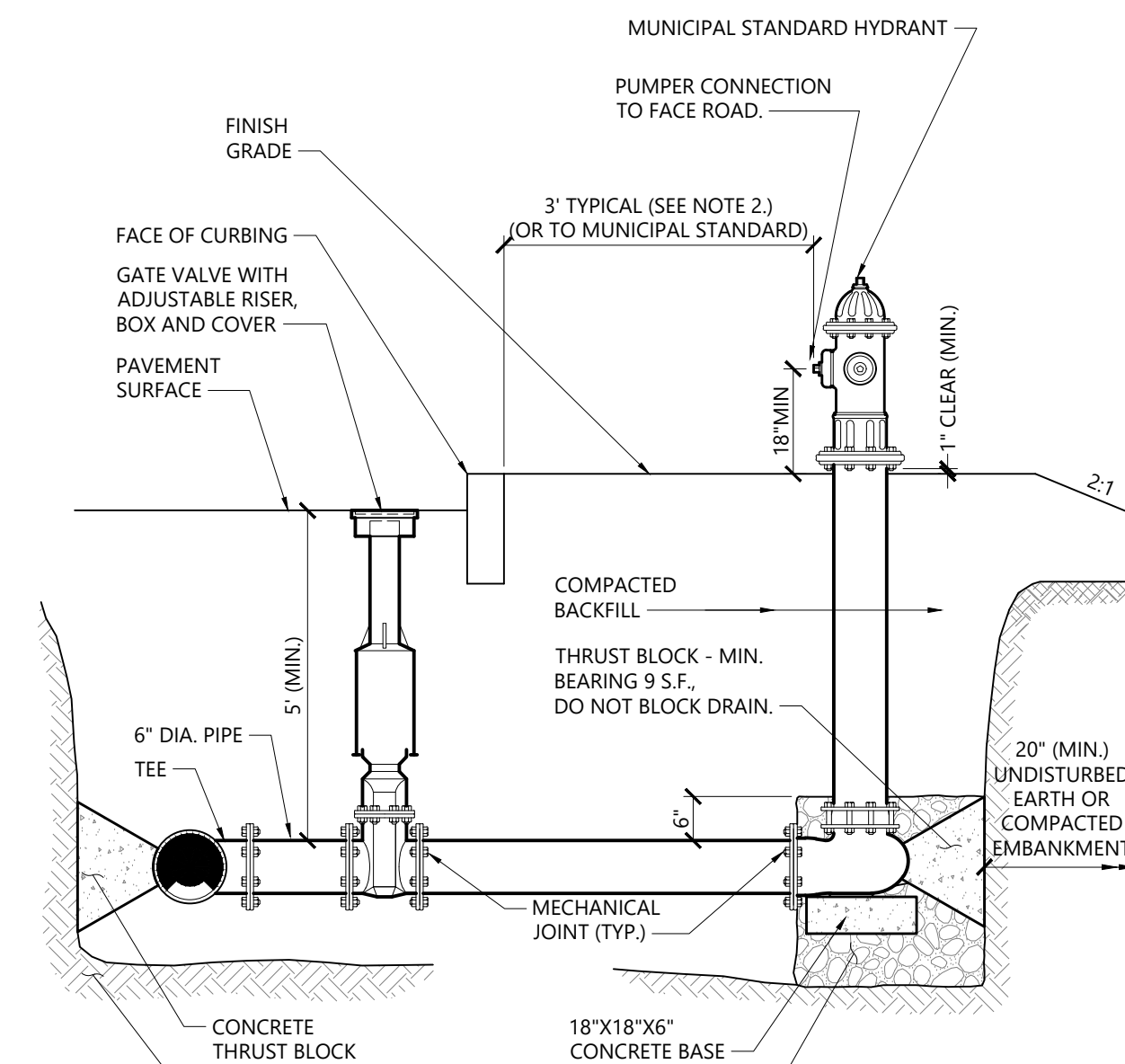


**Utility - Pipe Insulation**

N.T.S. Source: VHB 12/19 LD\_304

**NOTES**

- SEWER LINE SHALL BE INSULATED WHEN COVER IS LESS THAN FOUR (4) FEET AND WHERE SHOWN ON PLANS.
- WATER LINE SHALL BE INSULATED WHEN COVER IS LESS THAN FIVE (5) FEET AND WHERE SHOWN ON PLANS.
- BACKFILL PLACED IN UTILITY TRENCHES INCLUDING DISTURBED AREAS SURROUNDING UTILITY TRENCHES SHALL BE PLACED AND COMPACTED IN 12" (MAX.) VERTICAL LIFTS.



**Hydrant Construction**

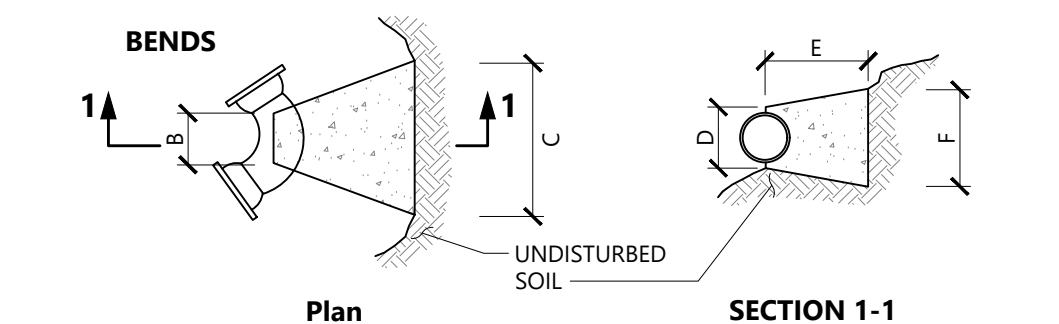
N.T.S. Source: VHB 1/18 LD\_250

**NOTES**

- CONCRETE THRUST BLOCKS TO BE USED ONLY WHERE THEY CAN BEAR ON UNDISTURBED EARTH AS SHOWN. USE CLAMPS AND TIE RODS OR OTHER ACCEPTABLE METHOD OF JOINT RESTRAINT WHERE SOIL CONDITIONS PROHIBIT THE USE OF THRUST BLOCKS.
- HYDRANT IN SIDEWALK AREAS TO BE LOCATED TO PROVIDE MINIMUM CLEAR SIDEWALK PASSAGE WIDTH OF 3 FEET AT HYDRANT.
- A 36-INCH CLEAR SPACE SHALL BE MAINTAINED AROUND THE CIRCUMFERENCE OF THE HYDRANT UNLESS OTHERWISE APPROVED BY AUTHORITY HAVING JURISDICTION.

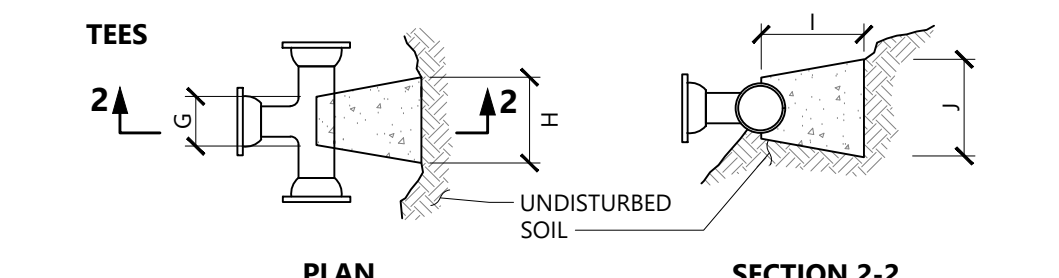
**TABLE OF DIMENSIONS**

BENDS						BENDS					
B	C	D	E	F		B	C	D	E	F	
6" 11 1/4"	8"	15"	12"	24"	12"	6" 45"	8"	30"	12"	24"	14"
6" 22 1/2"	-	19"	-	-	13"	6" 90"	-	30"	-	-	27"
8" 11 1/4"	-	20"	-	-	12"	8" 45"	-	30"	-	-	24"
8" 22 1/2"	-	22"	-	-	17"	8" 90"	-	38"	-	-	36"
12" 11 1/2"	-	30"	-	-	15"	12" 45"	-	40"	-	-	40"
12" 22 1/2"	-	35"	-	-	25"	12" 90"	-	60"	-	-	52"



**TABLE OF DIMENSIONS**

TEES				TEES				
G	H	I	J	G	H	I	J	
6"X6"X6"	12"	24"	24"	18"	12"X12"X6"	12"	24"	24"
8"X8"X6"	-	-	-	-	12"X12"X8"	-	-	24"
8"X8"X8"	-	-	-	24"	12"X12"X12"	-	36"	36"

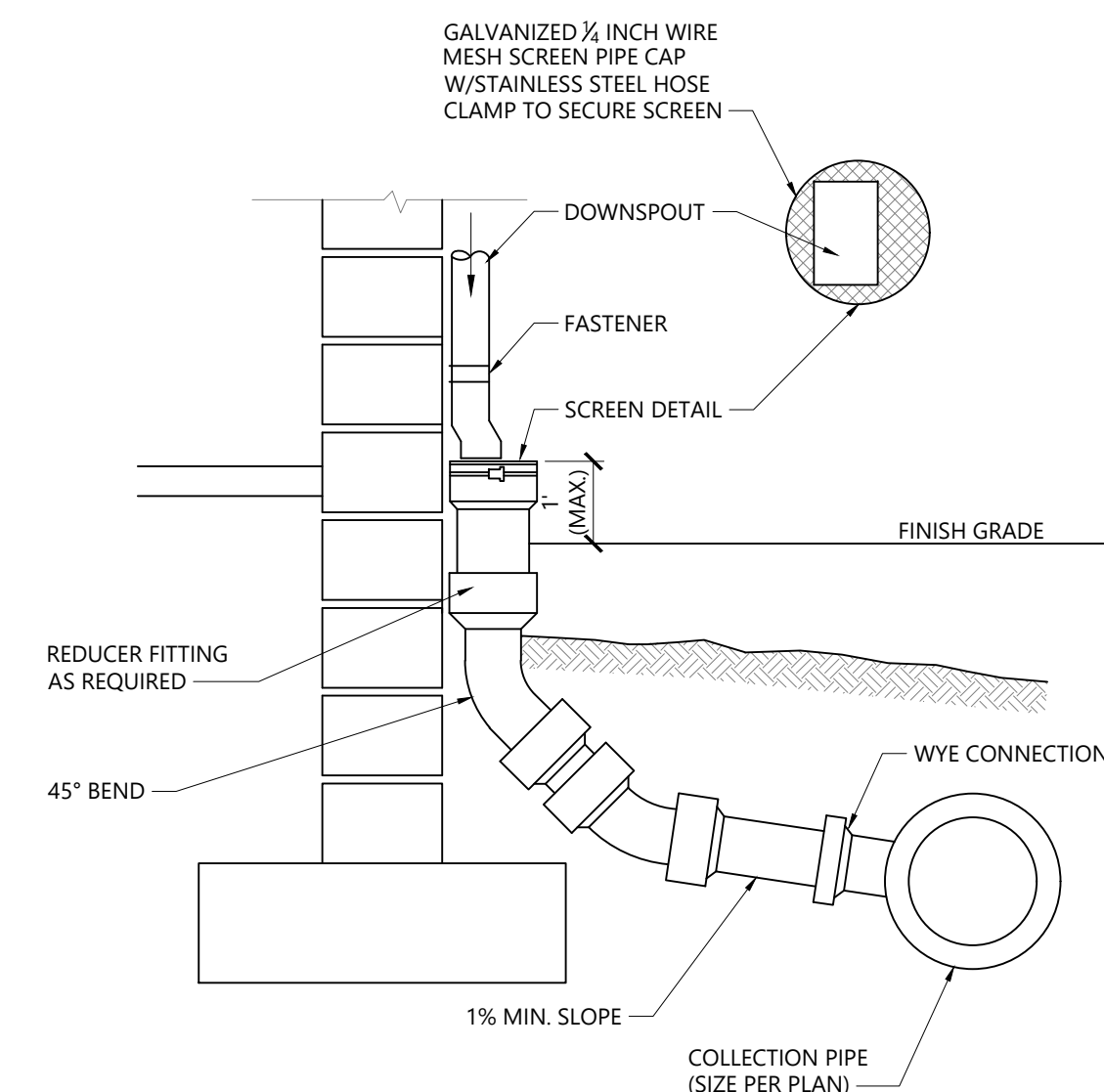


**NOTES**

- PROVIDE BLOCKS FOR TAPPING SLEEVES, DEAD ENDS, GATE VALVES, AND VERTICAL BENDS (SAME SIZE AS REQUIRED FOR TEES). PROVIDE ANCHOR RODS AT VERTICAL BENDS AND GATE VALVES.
- CONCRETE SHALL NOT BE PLACED AGAINST PIPE BEYOND FITTING.
- CONCRETE SHALL BE 3,000 PSI-TYPE I.

**Concrete Thrust Block**

N.T.S. Source: VHB 1/16 LD\_260



**Downspout Rain Leader**

N.T.S. Source: VHB 1/16 LD\_195

**Wilson Center Mixed-Use Development**


29 Windsor Avenue  
Windsor, Connecticut

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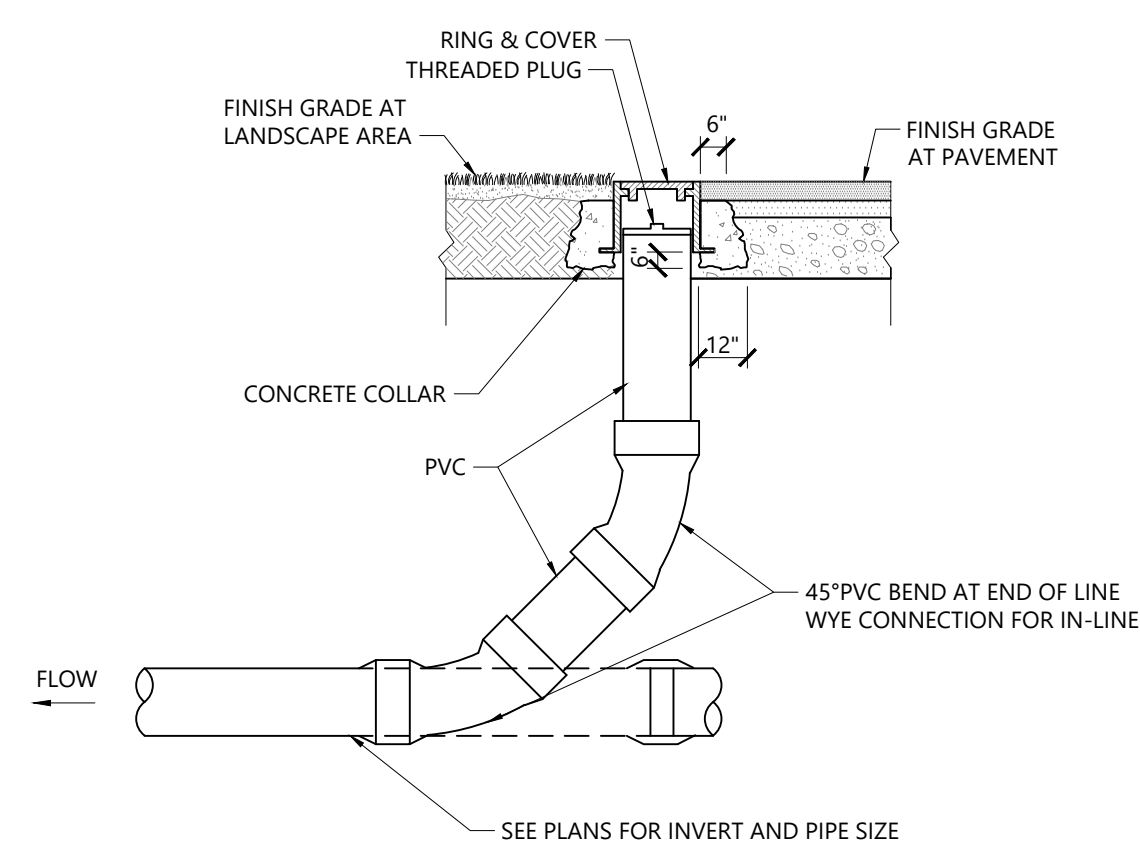
Designed by: \_\_\_\_\_ Checked by: \_\_\_\_\_  
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**Site Plan Approval** July 28, 2023

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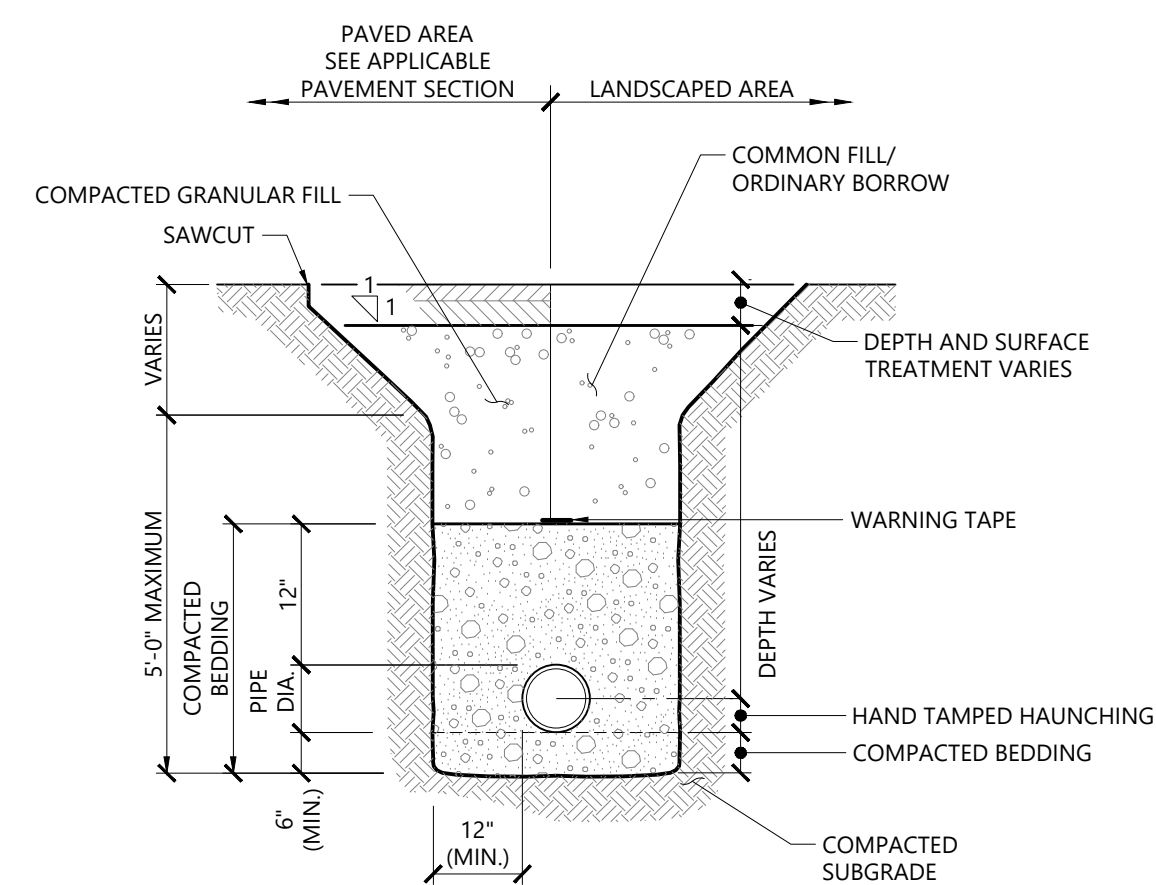
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Sheet **C-9** of 13

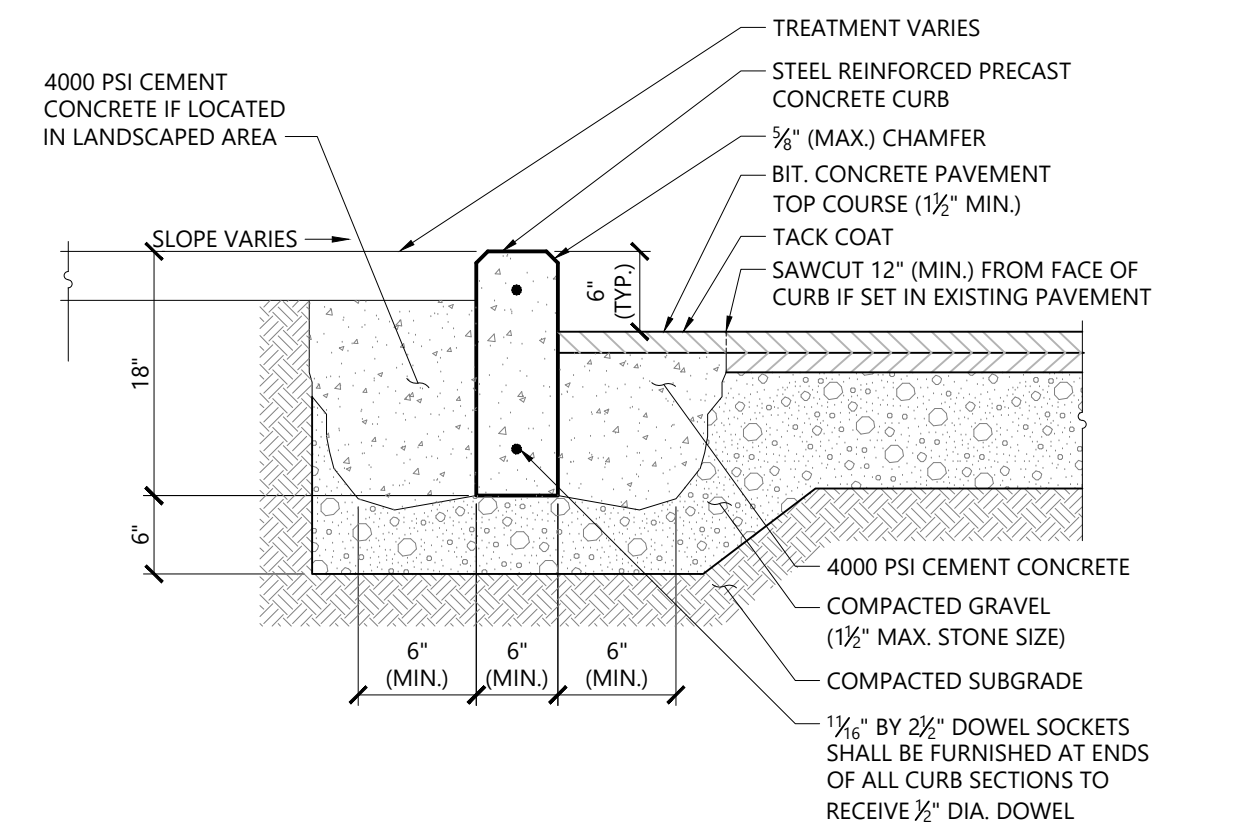


**Cleanout (CO)** 12/19  
N.T.S. Source: VHB LD\_303

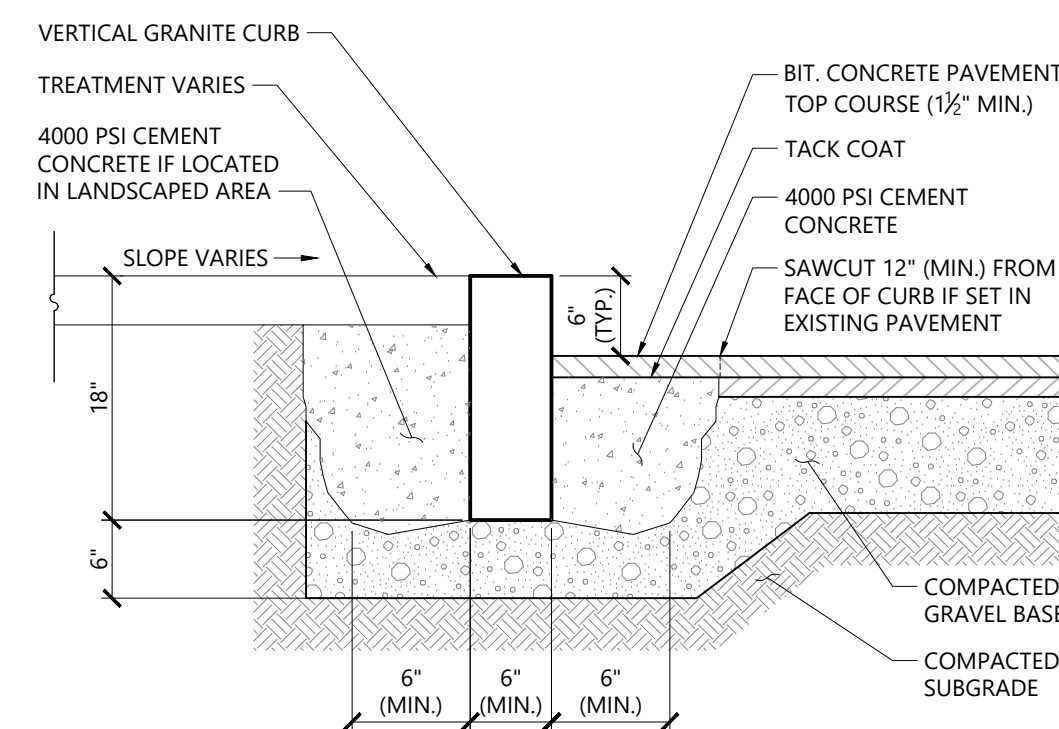


- NOTES**
- WHERE UTILITY TRENCHES ARE CONSTRUCTED THROUGH DETENTION BASIN BERMS OR OTHER SUCH SPECIAL SECTIONS, PLACE TRENCH BACKFILL WITH MATERIALS SIMILAR TO THE SPECIAL SECTION REQUIREMENTS.
  - USE METALLIC TRACING/WARNING TAPE OVER ALL PIPES.

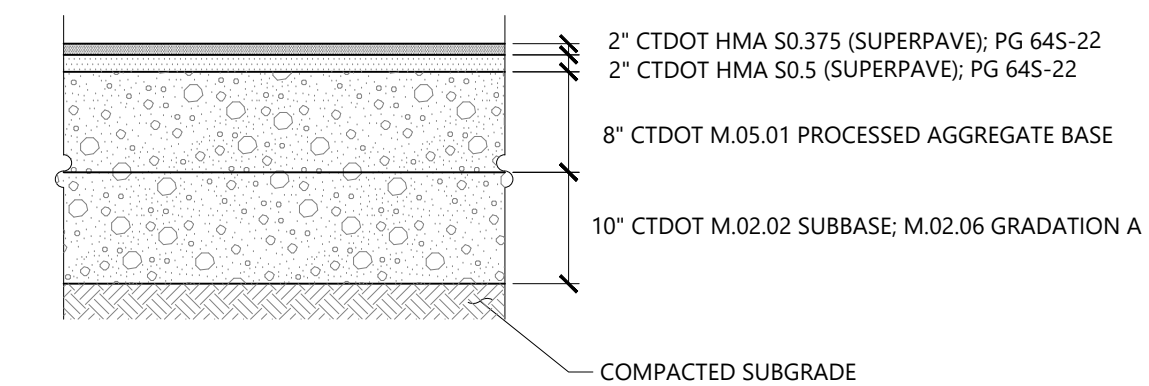
**Utility Trench** 1/16  
N.T.S. Source: VHB LD\_300



**Precast Concrete Curb (PCC)** 3/20  
N.T.S. Source: VHB REV LD\_404



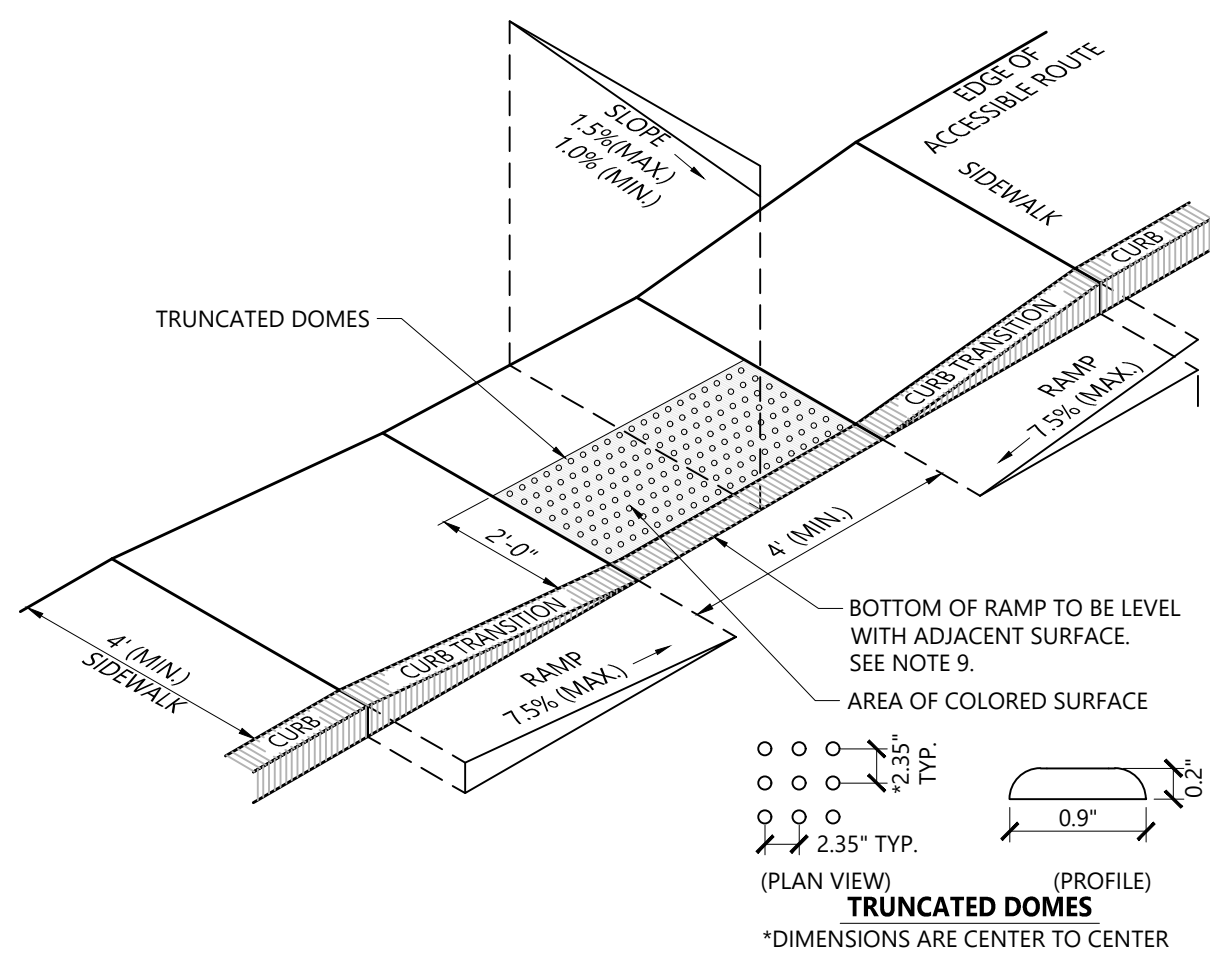
**Vertical Granite Curb (VGC)** 3/20  
N.T.S. Source: VHB REV LD\_402



**STANDARD DUTY FLEXIBLE PAVEMENT**

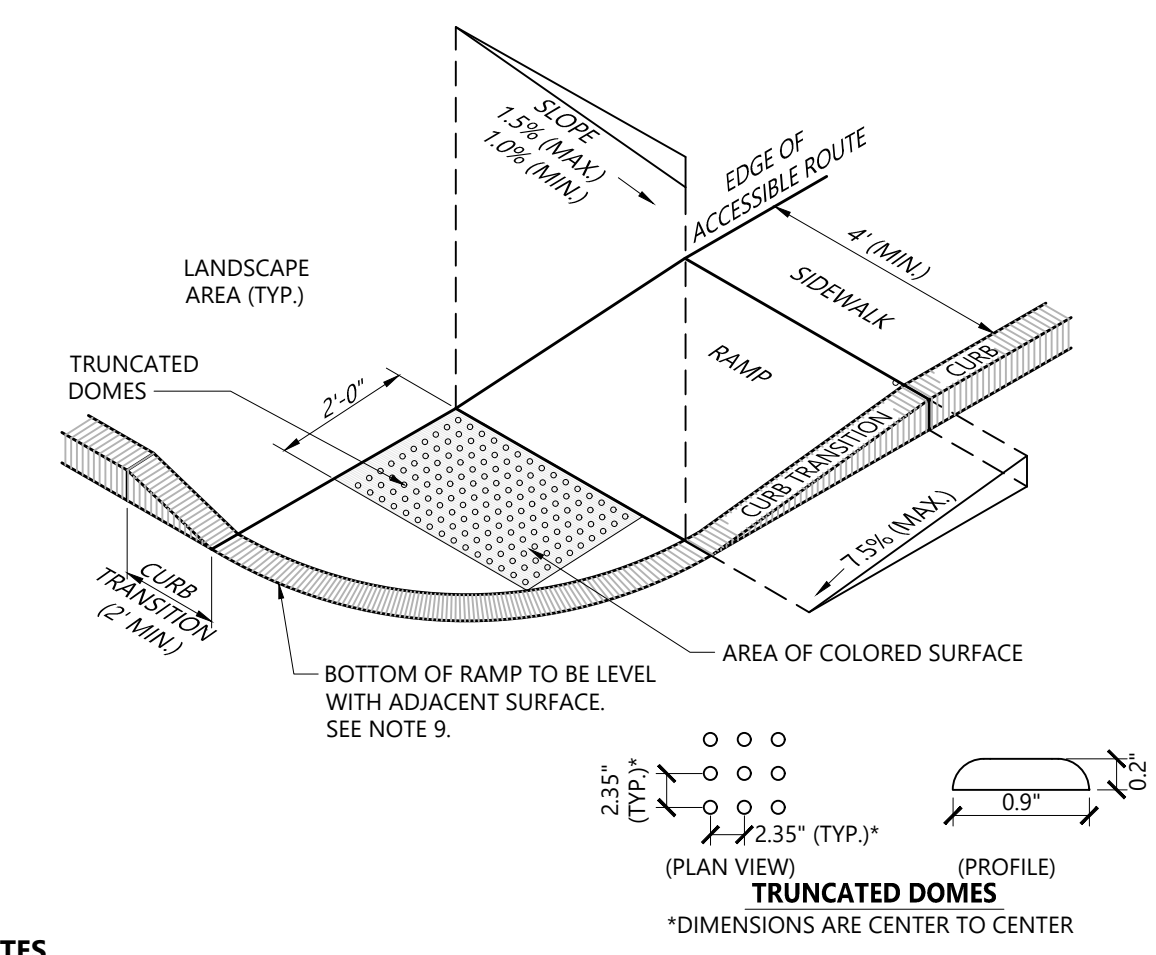
- NOTES**
- PAVEMENT SECTIONS ARE SUBJECT TO CHANGE AND WILL BE BASED ON THE RESULTS OF FURTHER GEOTECHNICAL INVESTIGATIONS.

**Bituminous Concrete Pavement Sections** 11/19  
N.T.S. Source: VHB REV LD\_430



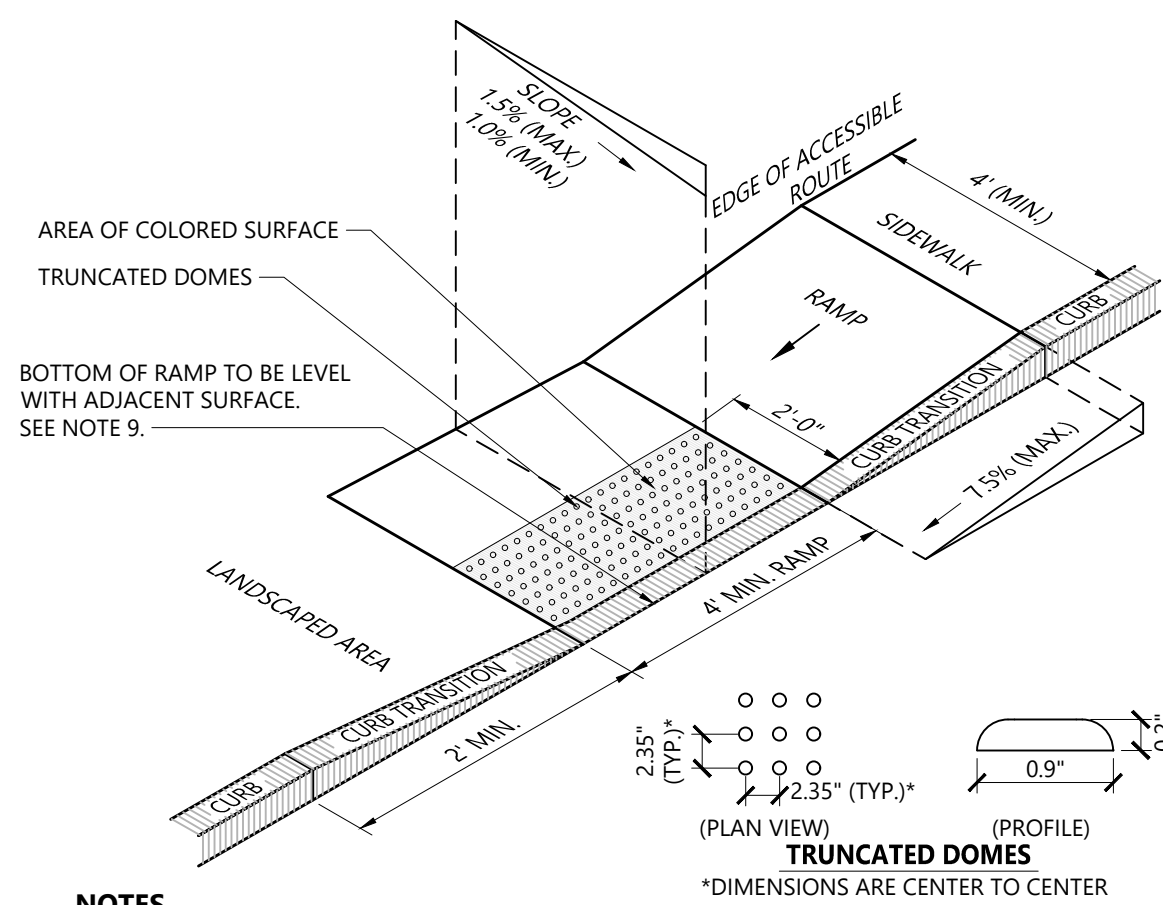
- NOTES**
- THE MAXIMUM ALLOWABLE SIDEWALK AND CURB RAMP CROSS SLOPES SHALL BE 1.5 (1% MIN).
  - THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE EXCLUDING CURB RAMPS SHALL BE 5%.
  - THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE AT CURB RAMPS SHALL BE 7.5%.
  - A MINIMUM OF 3 FEET CLEAR SHALL BE MAINTAINED AT ANY PERMANENT OBSTACLE IN ACCESSIBLE ROUTE (I.E., HYDRANTS, UTILITY POLES, TREE WELLS, SIGNS, ETC.).
  - CURB TREATMENT VARIES, SEE PLANS FOR CURB TYPE.
  - RAMP, CURB, AND ADJACENT PAVEMENTS SHALL BE GRADED TO PREVENT PONDING.
  - SEE TYPICAL SIDEWALK SECTION FOR RAMP CONSTRUCTION.
  - WHERE ACCESSIBLE ROUTES ARE LESS THAN 5' IN WIDTH (EXCLUDING CURBING) A 5' x 5' PASSING AREA SHALL BE PROVIDED AT INTERVALS NOT TO EXCEED 200 FEET.
  - ELIMINATE CURBING AT RAMP (OTHER THAN VERTICAL CURBING, WHICH SHALL BE SET FLUSH) WHERE IT ABUTS ROADWAY.
  - DETECTABLE WARNINGS SHALL CONTRAST VISUALLY WITH ADJOINING SURFACES.
  - DETECTABLE WARNINGS SHALL BE INSTALLED PERPENDICULAR TO ACCESSIBLE ROUTE.

**Accessible Curb Ramp (ACR) Type 'A-D'** 12/20  
N.T.S. Source: VHB LD\_500



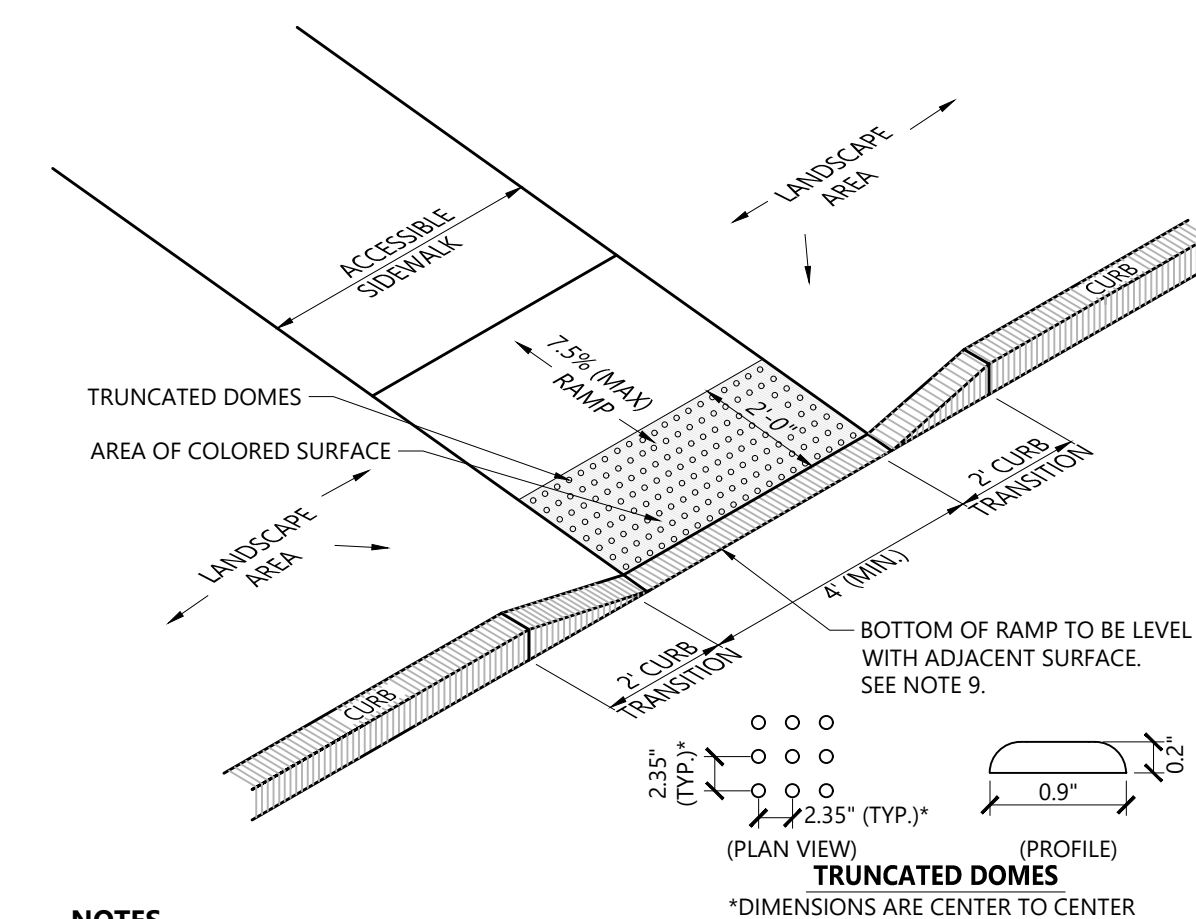
- NOTES**
- THE MAXIMUM ALLOWABLE SIDEWALK AND CURB RAMP CROSS SLOPES SHALL BE 1.5 (1% MIN).
  - THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE EXCLUDING CURB RAMPS SHALL BE 5%.
  - THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE AT CURB RAMPS SHALL BE 7.5%.
  - A MINIMUM OF 3 FEET CLEAR SHALL BE MAINTAINED AT ANY PERMANENT OBSTACLE IN ACCESSIBLE ROUTE (I.E., HYDRANTS, UTILITY POLES, TREE WELLS, SIGNS, ETC.).
  - CURB TREATMENT VARIES, SEE PLANS FOR CURB TYPE.
  - RAMP, CURB AND ADJACENT PAVEMENTS SHALL BE GRADED TO PREVENT PONDING.
  - SEE TYPICAL SIDEWALK SECTION FOR RAMP CONSTRUCTION.
  - WHERE ACCESSIBLE ROUTES ARE LESS THAN 5' IN WIDTH (EXCLUDING CURBING) A 5' x 5' PASSING AREA SHALL BE PROVIDED AT INTERVALS NOT TO EXCEED 200 FEET.
  - ELIMINATE CURBING AT RAMP WHERE IT ABUTS ROADWAY, EXCEPT WHERE VERTICAL CURBING IS INDICATED ON THE DRAWINGS TO BE INSTALLED AND SET FLUSH.
  - DETECTABLE WARNINGS SHALL CONTRAST VISUALLY WITH ADJOINING SURFACES.
  - DETECTABLE WARNINGS SHALL BE INSTALLED PERPENDICULAR TO THE ACCESSIBLE ROUTE.
  - CONTRACTOR TO SUBMIT R.F.I. FOR THIS TYPE OF ACCESSIBLE CURB RAMP FOR APEX ROADWAY CROSSINGS.

**Accessible Curb Ramp (ACR) - Type 'B-D'** 12/20  
N.T.S. Source: VHB LD\_501



- NOTES**
- THE MAXIMUM ALLOWABLE SIDEWALK AND CURB RAMP CROSS SLOPES SHALL BE 1.5 (1% MIN).
  - THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE EXCLUDING CURB RAMPS SHALL BE 5%.
  - THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE AT CURB RAMPS SHALL BE 7.5%.
  - A MINIMUM OF 3 FEET CLEAR SHALL BE MAINTAINED AT ANY PERMANENT OBSTACLE IN ACCESSIBLE ROUTE (I.E., HYDRANTS, UTILITY POLES, TREE WELLS, SIGNS, ETC.).
  - CURB TREATMENT VARIES, SEE PLANS FOR CURB TYPE.
  - RAMP, CURB AND ADJACENT PAVEMENTS SHALL BE GRADED TO PREVENT PONDING.
  - SEE TYPICAL SIDEWALK SECTION FOR RAMP CONSTRUCTION.
  - WHERE ACCESSIBLE ROUTES ARE LESS THAN 5' IN WIDTH (EXCLUDING CURBING) A 5' x 5' PASSING AREA SHALL BE PROVIDED AT INTERVALS NOT TO EXCEED 200 FEET.
  - ELIMINATE CURBING AT RAMP WHERE IT ABUTS ROADWAY, EXCEPT WHERE VERTICAL CURBING IS INDICATED ON THE DRAWINGS TO BE INSTALLED AND SET FLUSH.
  - DETECTABLE WARNINGS SHALL CONTRAST VISUALLY WITH ADJOINING SURFACES.
  - DETECTABLE WARNINGS SHALL BE INSTALLED PERPENDICULAR TO THE ACCESSIBLE ROUTE.
  - CONTRACTOR TO SUBMIT R.F.I. FOR THIS TYPE OF ACCESSIBLE CURB RAMP FOR APEX ROADWAY CROSSINGS.

**Accessible Curb Ramp (ACR) Type 'G-D'** 12/20  
N.T.S. Source: VHB REV LD\_506



- NOTES**
- THE MAXIMUM ALLOWABLE SIDEWALK AND CURB RAMP CROSS SLOPES SHALL BE 1.5 (1% MIN).
  - THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE EXCLUDING CURB RAMPS SHALL BE 5%.
  - THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE AT CURB RAMPS SHALL BE 7.5%.
  - A MINIMUM OF 3 FEET CLEAR SHALL BE MAINTAINED AT ANY PERMANENT OBSTACLE IN ACCESSIBLE ROUTE (I.E., HYDRANTS, UTILITY POLES, TREE WELLS, SIGNS, ETC.).
  - CURB TREATMENT VARIES, SEE PLANS FOR CURB TYPE.
  - RAMP, CURB AND ADJACENT PAVEMENTS SHALL BE GRADED TO PREVENT PONDING.
  - SEE TYPICAL SIDEWALK SECTION FOR RAMP CONSTRUCTION.
  - WHERE ACCESSIBLE ROUTES ARE LESS THAN 5' IN WIDTH (EXCLUDING CURBING) A 5' x 5' PASSING AREA SHALL BE PROVIDED AT INTERVALS NOT TO EXCEED 200 FEET.
  - ELIMINATE CURBING (OTHER THAN VERTICAL CURBING, WHICH SHALL BE SET FLUSH) WHERE IT ABUTS ROADWAYS.
  - DETECTABLE WARNINGS SHALL CONTRAST VISUALLY WITH ADJOINING SURFACES.
  - DETECTABLE WARNINGS SHALL BE INSTALLED PERPENDICULAR TO THE ACCESSIBLE ROUTE.

**Accessible Curb Ramp (ACR) Type 'M-D'** 12/20  
N.T.S. Source: VHB REV LD\_512

**Wilson Center  
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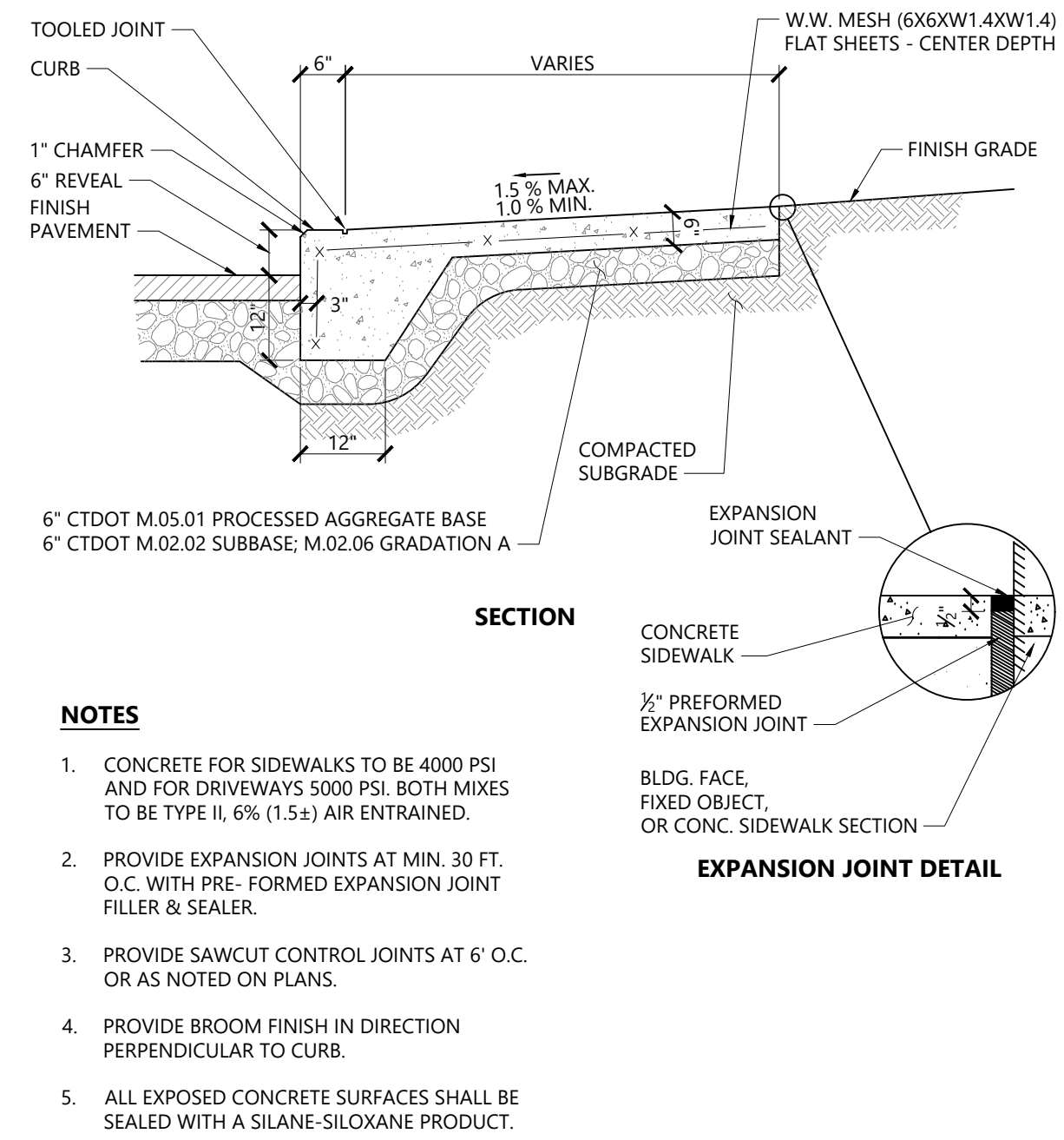
**Site Plan Approval** July 28, 2023

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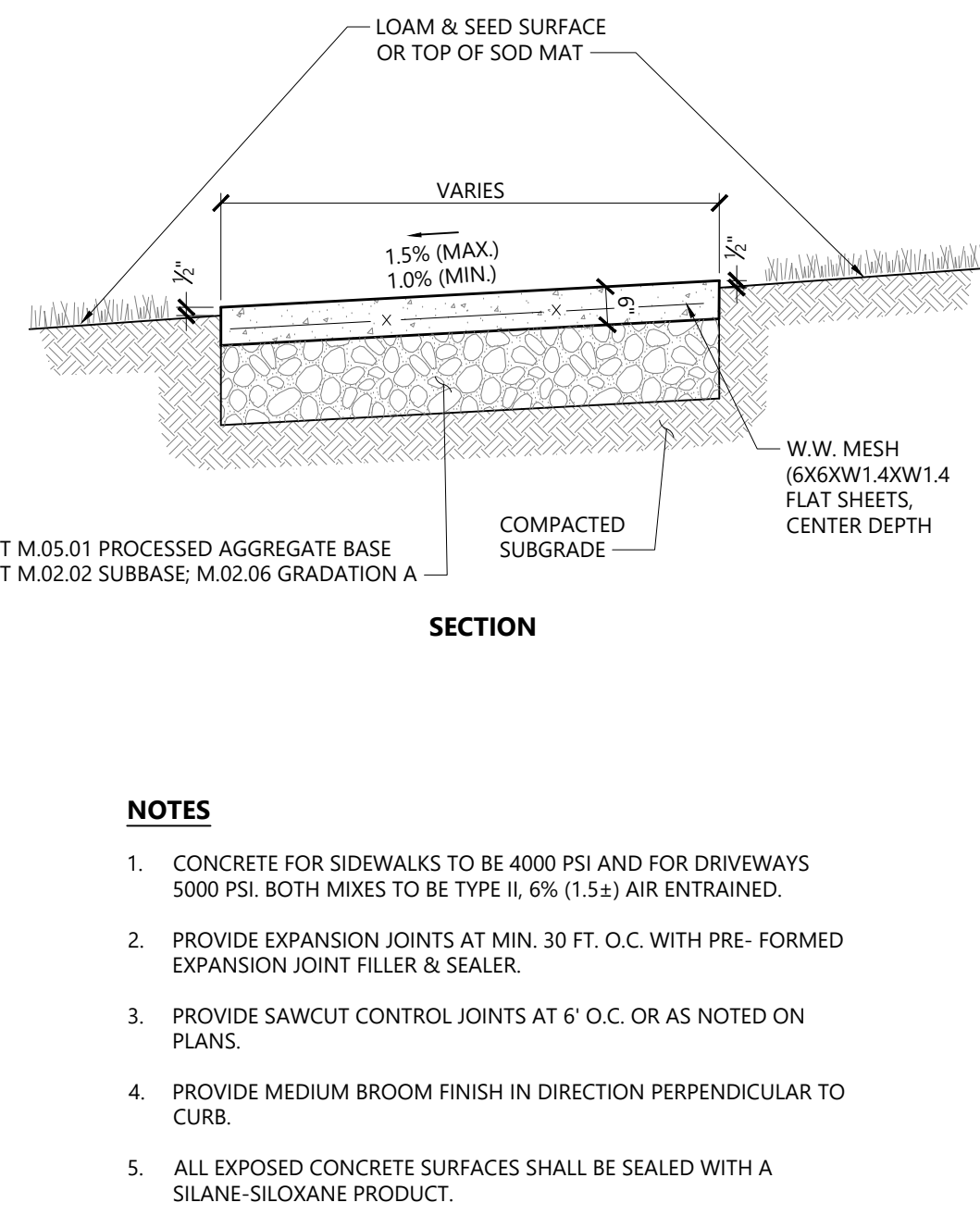
**Details**

**C-10**

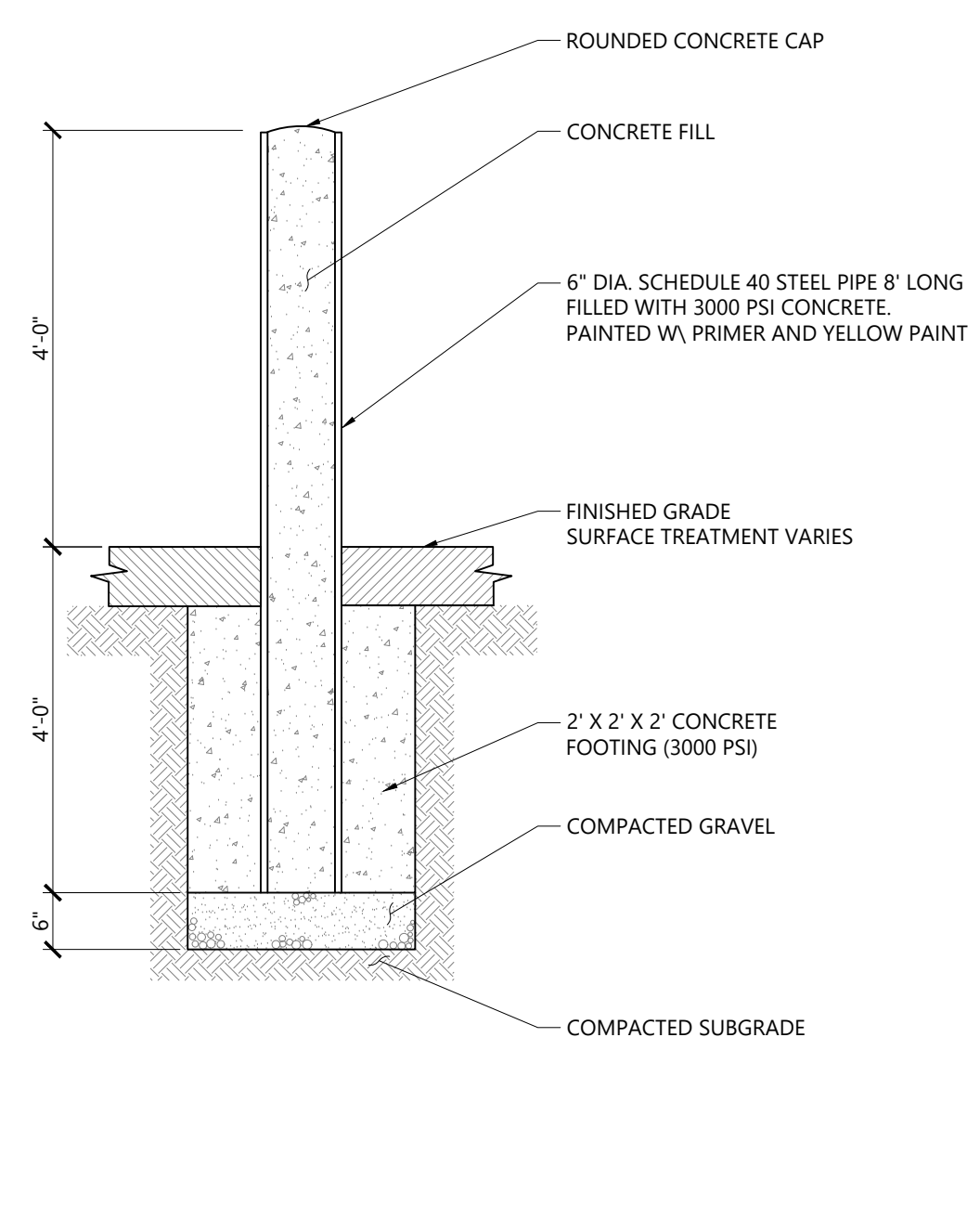
Sheet 10 of 13



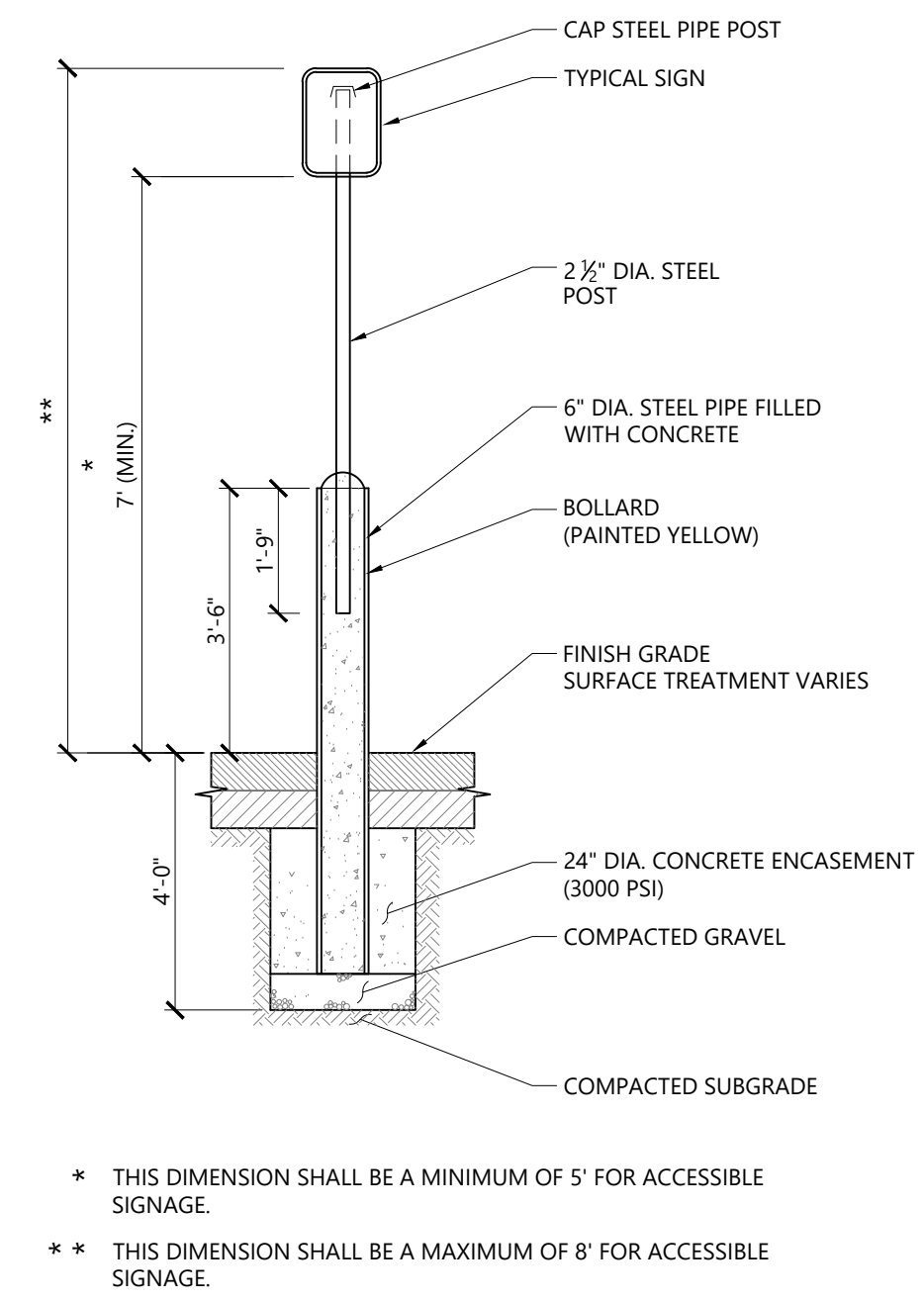
**Monolithic Concrete Curb (MCC) & Sidewalk** 3/20  
N.T.S. Source: VHB REV LD\_421



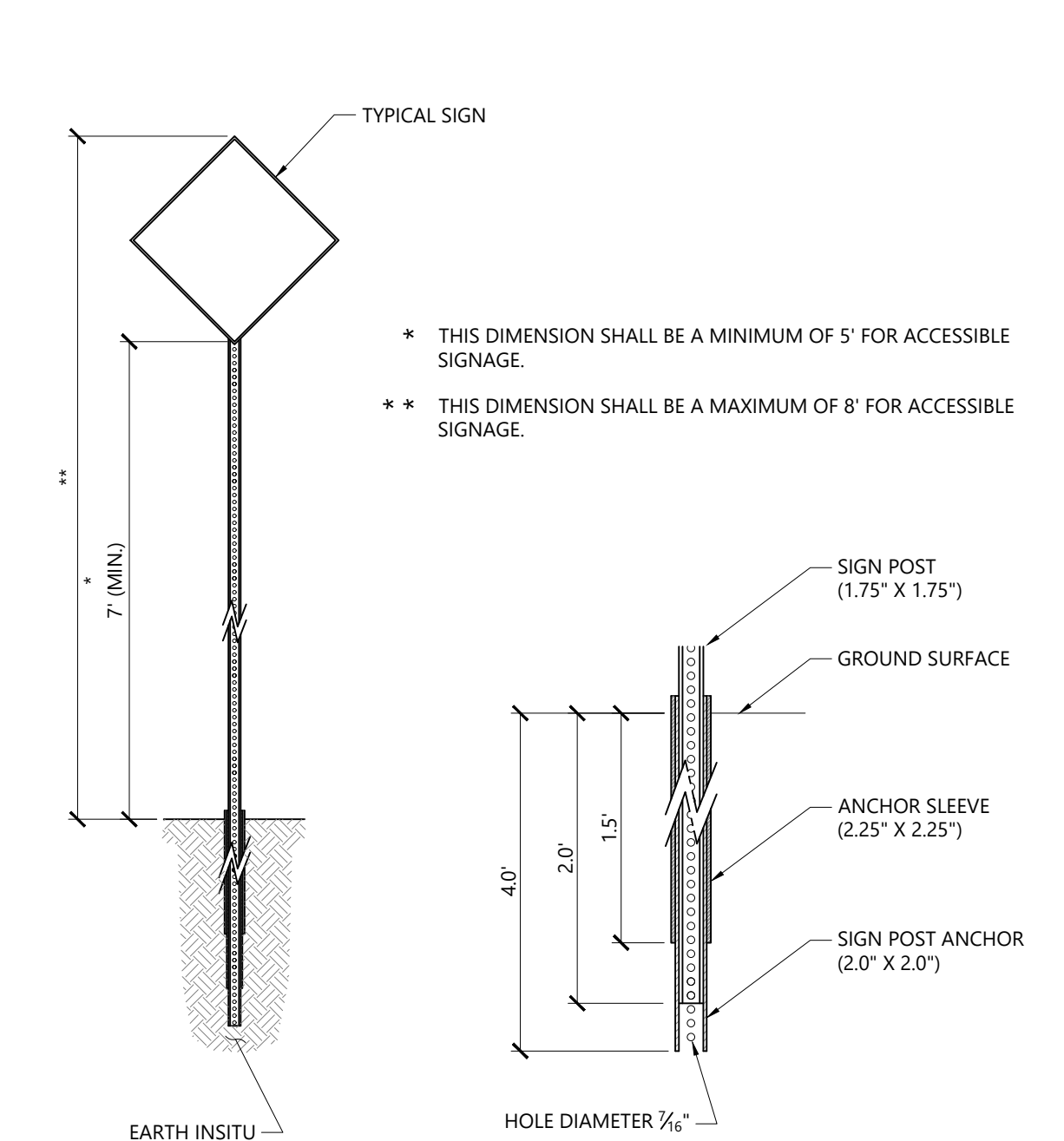
**Concrete Sidewalk in Landscape Area** 3/21  
N.T.S. Source: VHB REV LD\_426



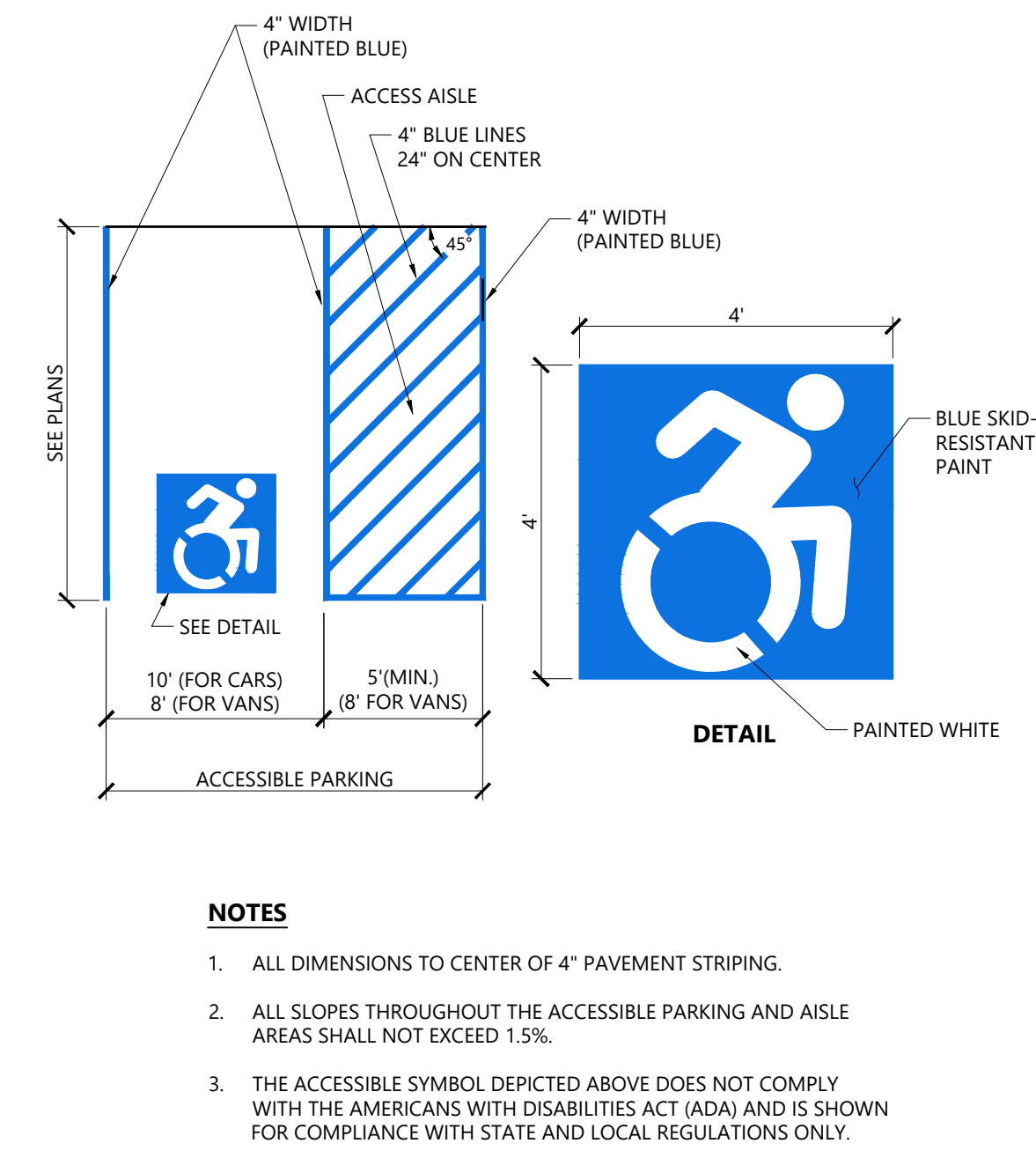
**Bollard** 12/18  
N.T.S. Source: VHB LD\_7005



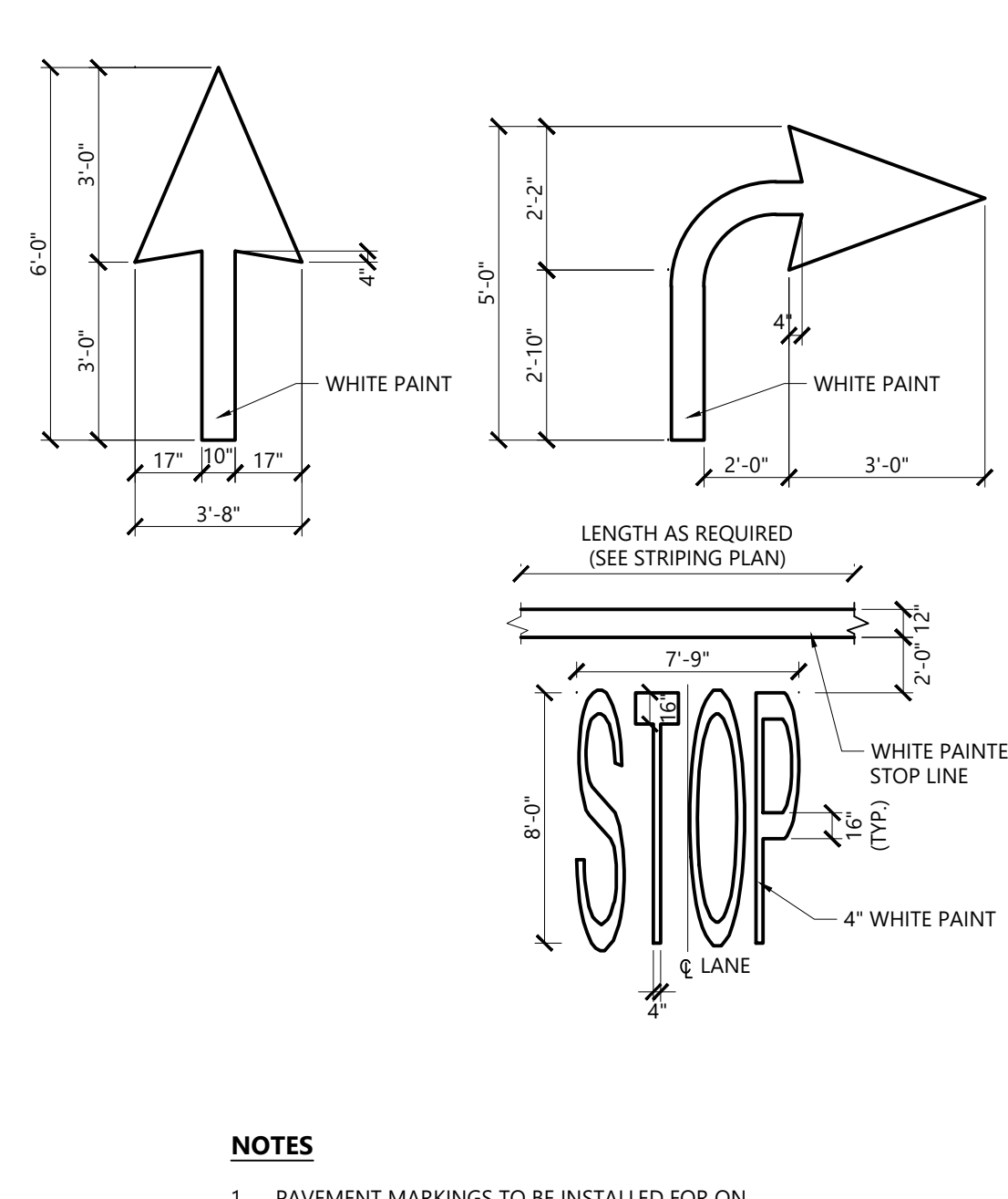
**Bollard Mounted Sign** 1/16  
N.T.S. Source: VHB LD\_7035



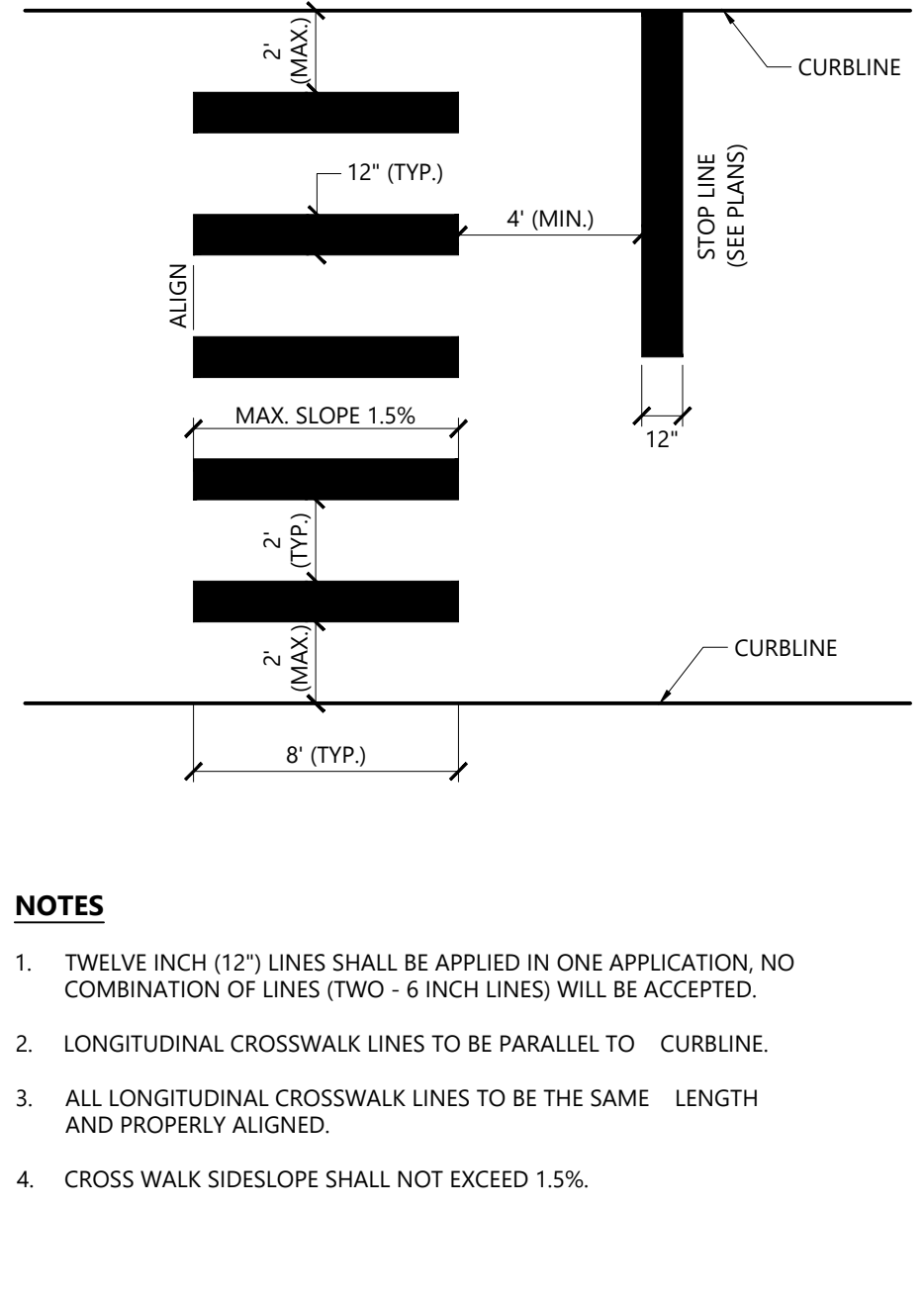
**Sign Post - Type 'B'** 1/16  
N.T.S. Source: VHB LD\_702



**Accessible Parking Space (CT ONLY)** 12/19  
N.T.S. Source: VHB REV LD\_552D



**Painted Pavement Markings - On Site** 1/16  
N.T.S. Source: VHB REV LD\_554



**Crosswalk** 1/16  
N.T.S. Source: VHB LD\_553

**Wilson Center  
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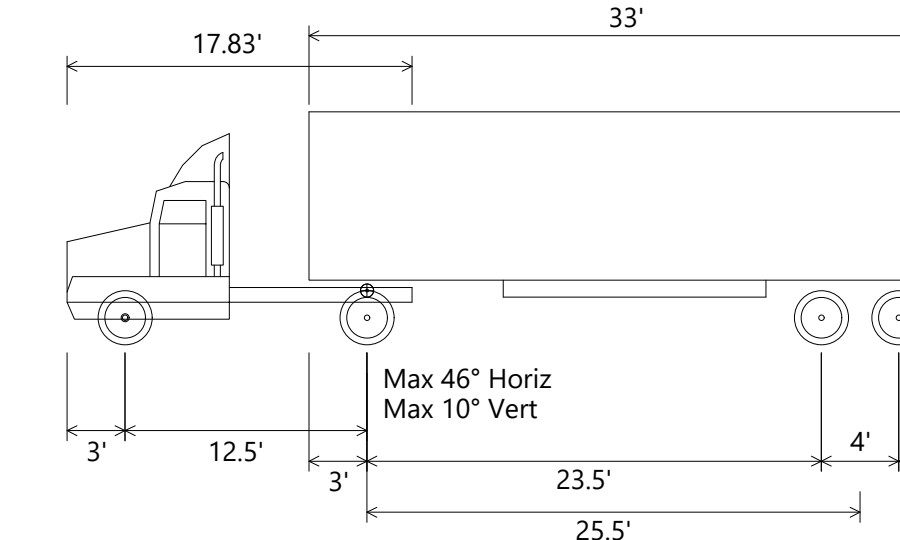
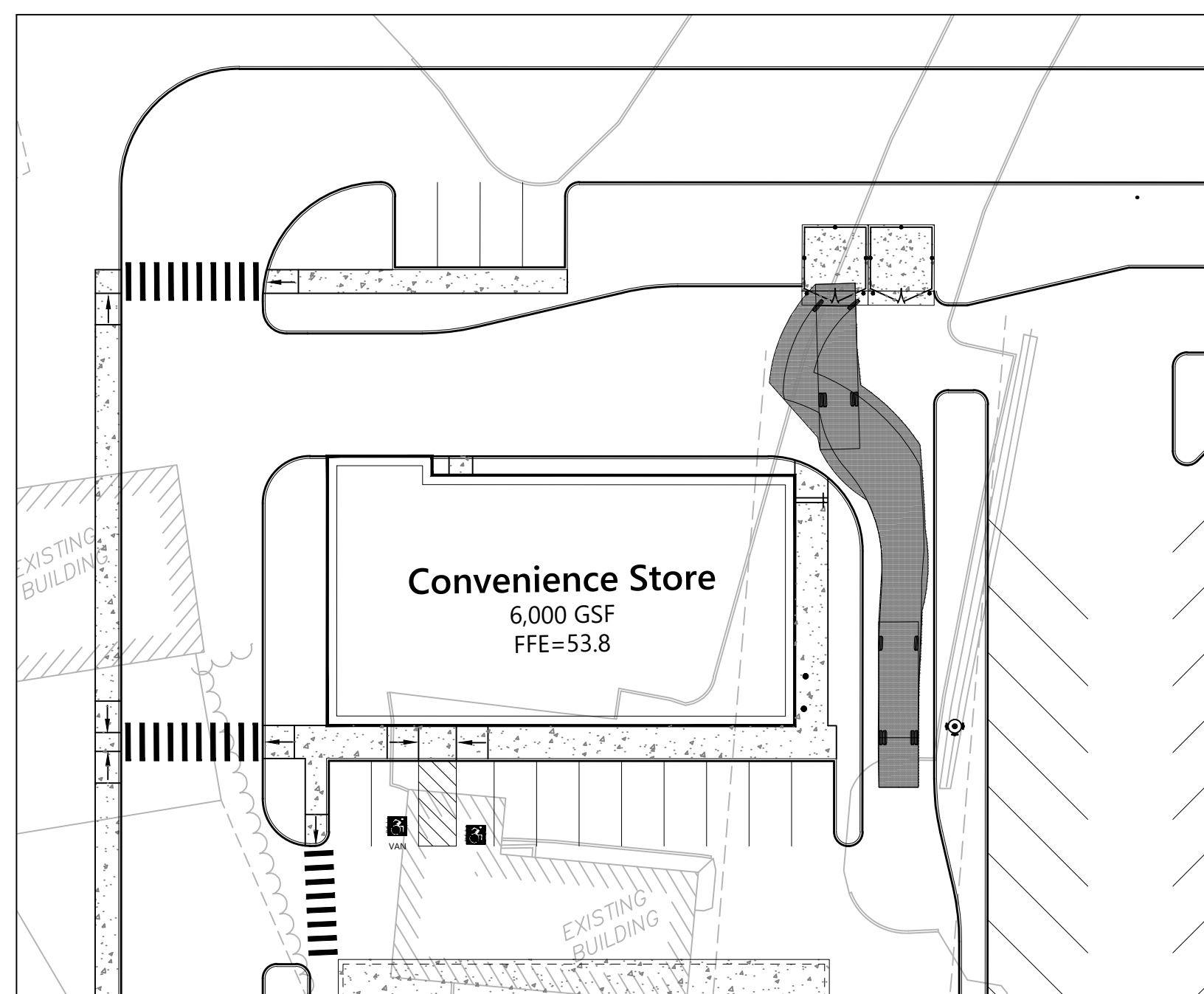
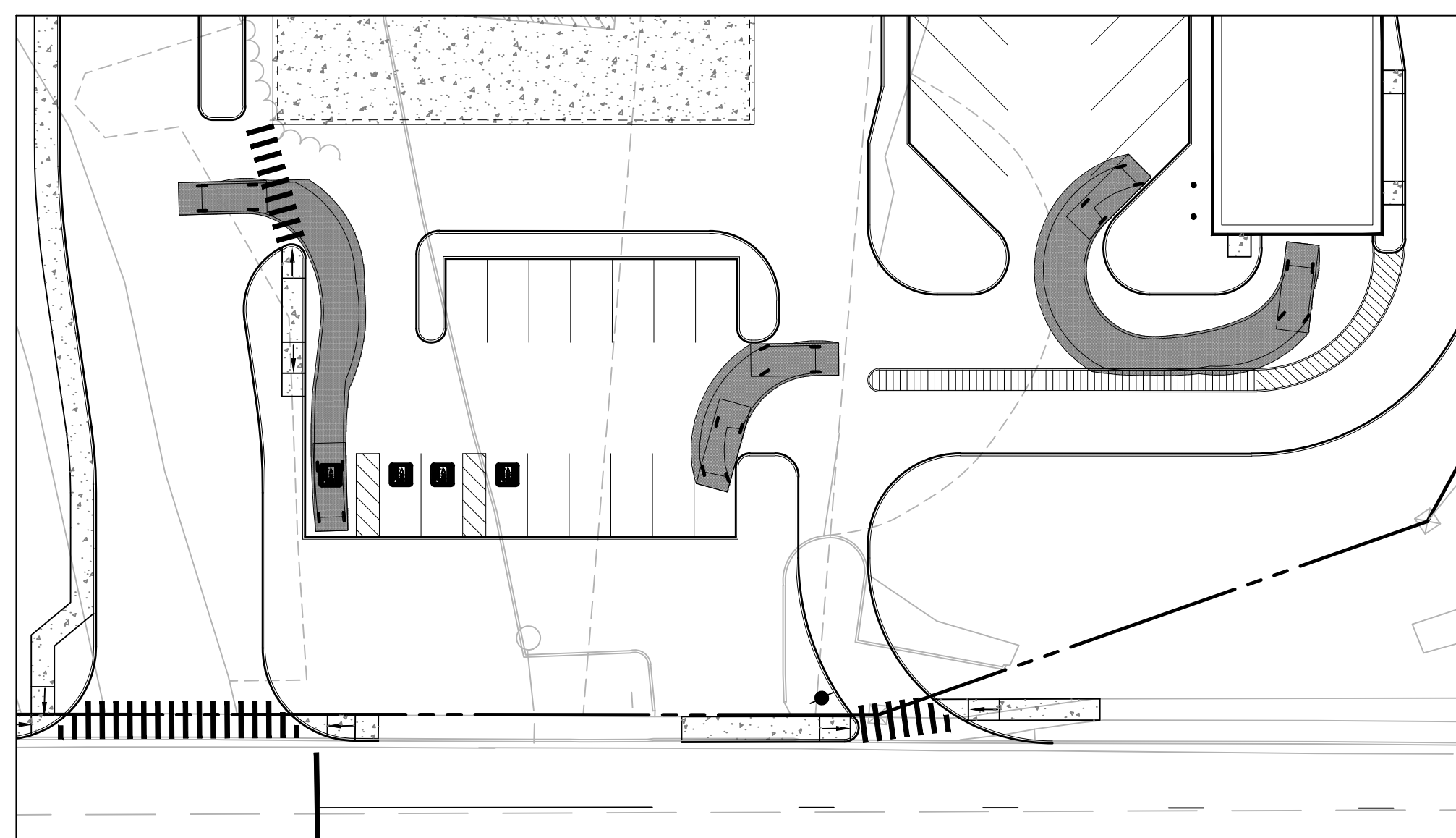
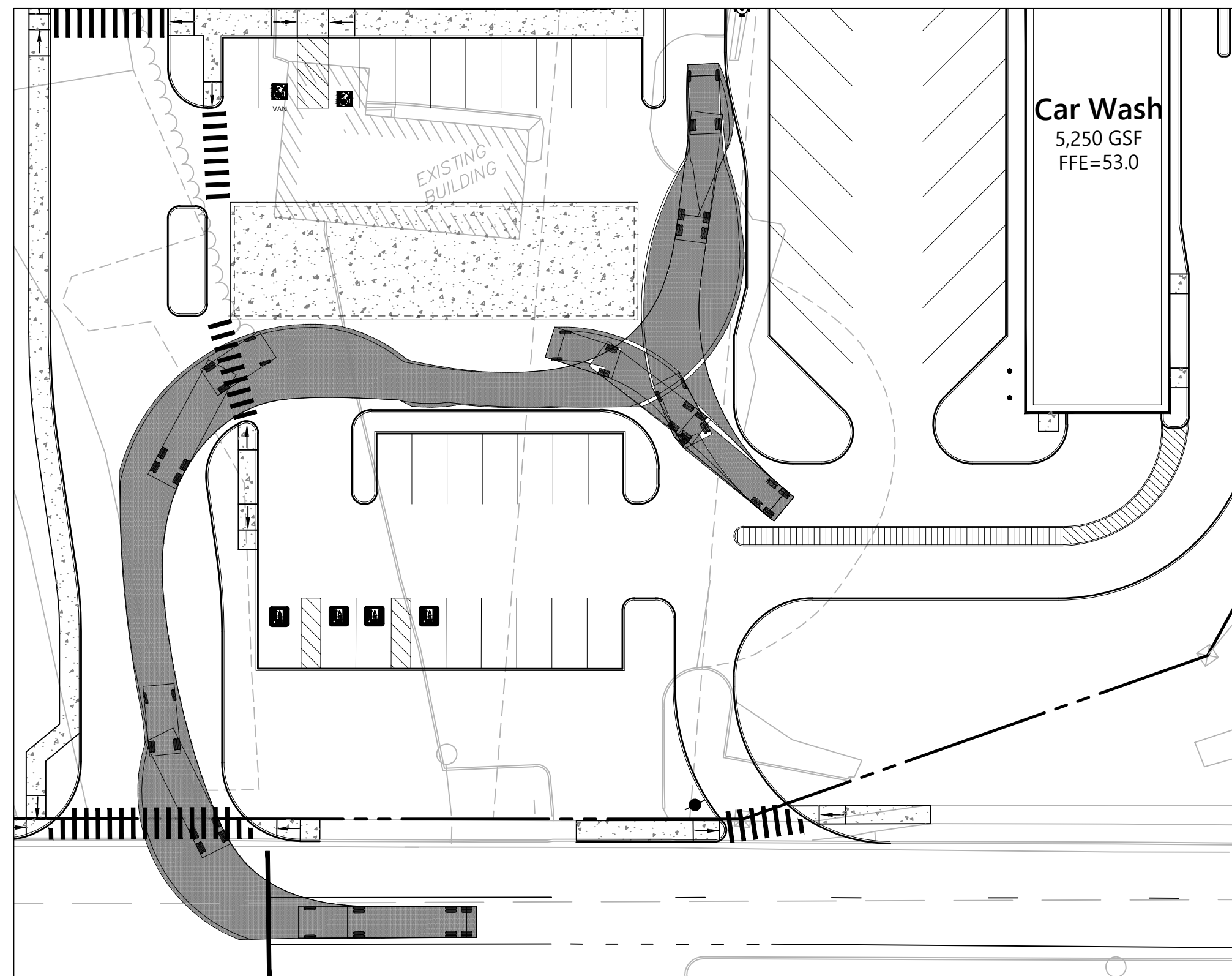
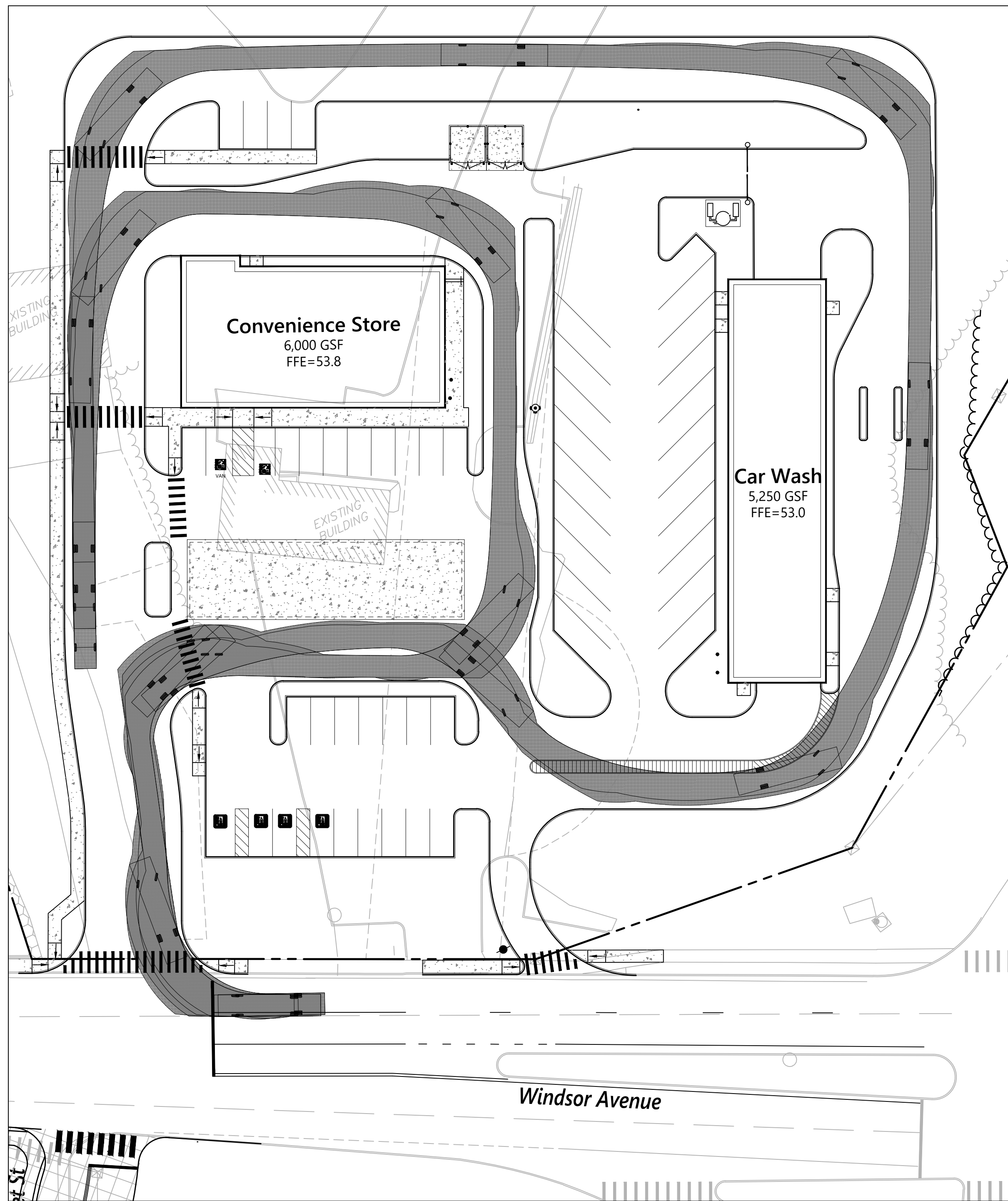
No.	Revision	Date	App'd

Designed by: \_\_\_\_\_ Checked by: \_\_\_\_\_  
Issued for: \_\_\_\_\_ Date: \_\_\_\_\_  
**Site Plan Approval** July 28, 2023

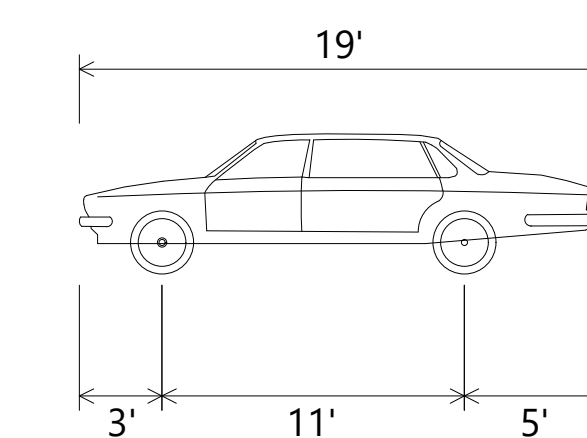
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Drawing Title  
**Details**  
Drawing Number

**C-11**  
Sheet of  
11 13  
Project Number  
42795.01

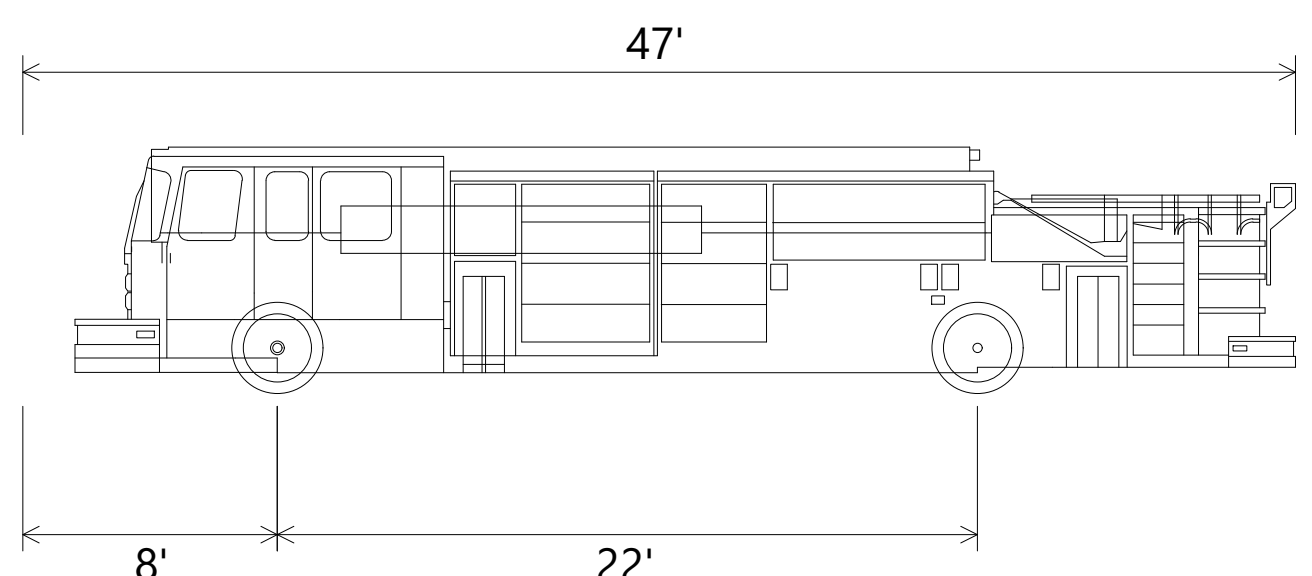




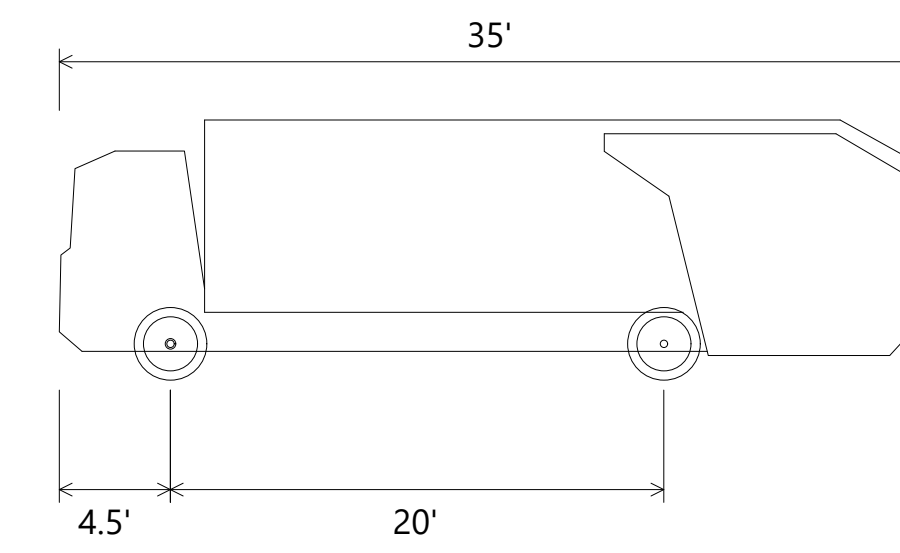
**Fuel Tanker Truck ( Similar to a WB-40 )**  
 Overall Length 45.5 ft  
 Overall Width 8 ft  
 Overall Body Height 12.1 ft  
 Min Body Ground Clearance 1.3 ft  
 Track Width 8 ft  
 Lock-to-lock time 4 s  
 Max Steering Angle (Virtual) 20.4°



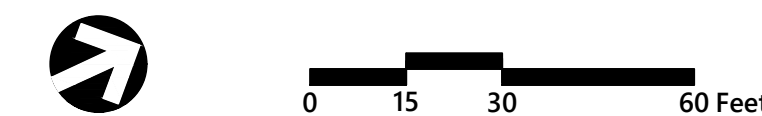
**Passenger Car**  
 Overall Length 19 ft  
 Overall Width 7 ft  
 Overall Body Height 5.1 ft  
 Min Body Ground Clearance 1.1 ft  
 Track Width 6 ft  
 Lock-to-lock time 4 s  
 Max Steering Angle (Virtual) 31.6°



**Pumper Fire Truck**  
 Overall Length 47 ft  
 Overall Width 8.17 ft  
 Overall Body Height 7.7 ft  
 Min Body Ground Clearance 0.66 ft  
 Track Width 8.17 ft  
 Lock-to-lock time 5 s  
 Max Wheel Angle 45°



**Garbage Truck**  
 Overall Length 35 ft  
 Overall Width 8.4 ft  
 Overall Body Height 10.5 ft  
 Min Body Ground Clearance 1 ft  
 Track Width 8.4 ft  
 Lock-to-lock time 6 s  
 Curb to Curb Turning Radius 29.3 ft



**Wilson Center  
 Mixed-Use Development**  
 29 Windsor Avenue  
 Windsor, Connecticut

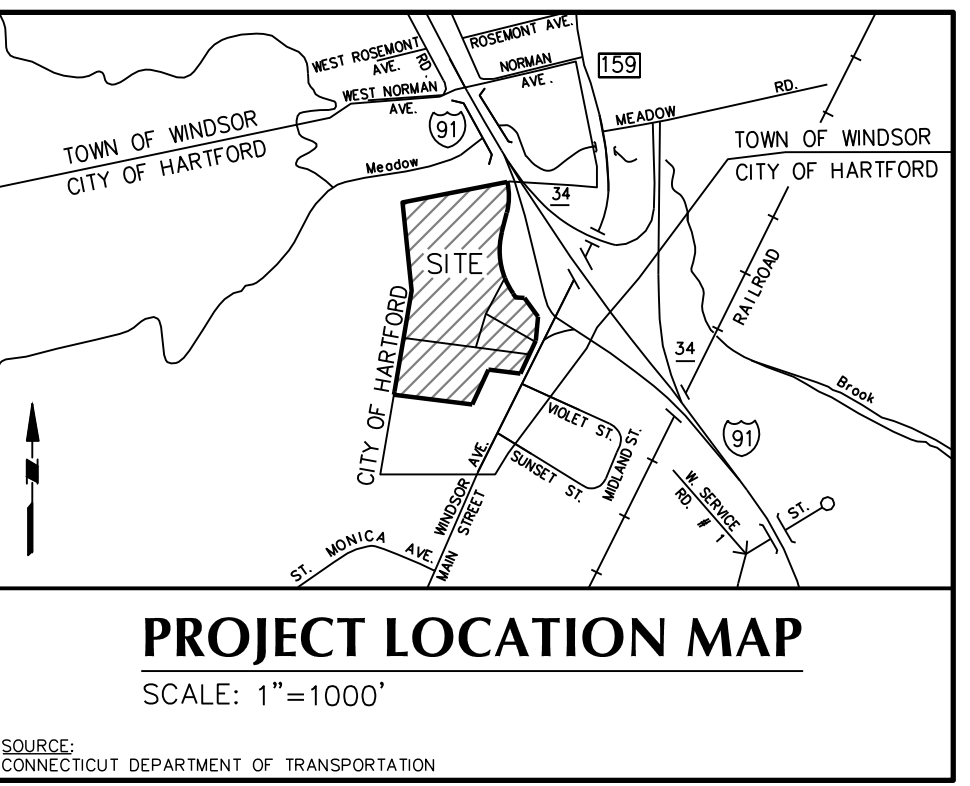
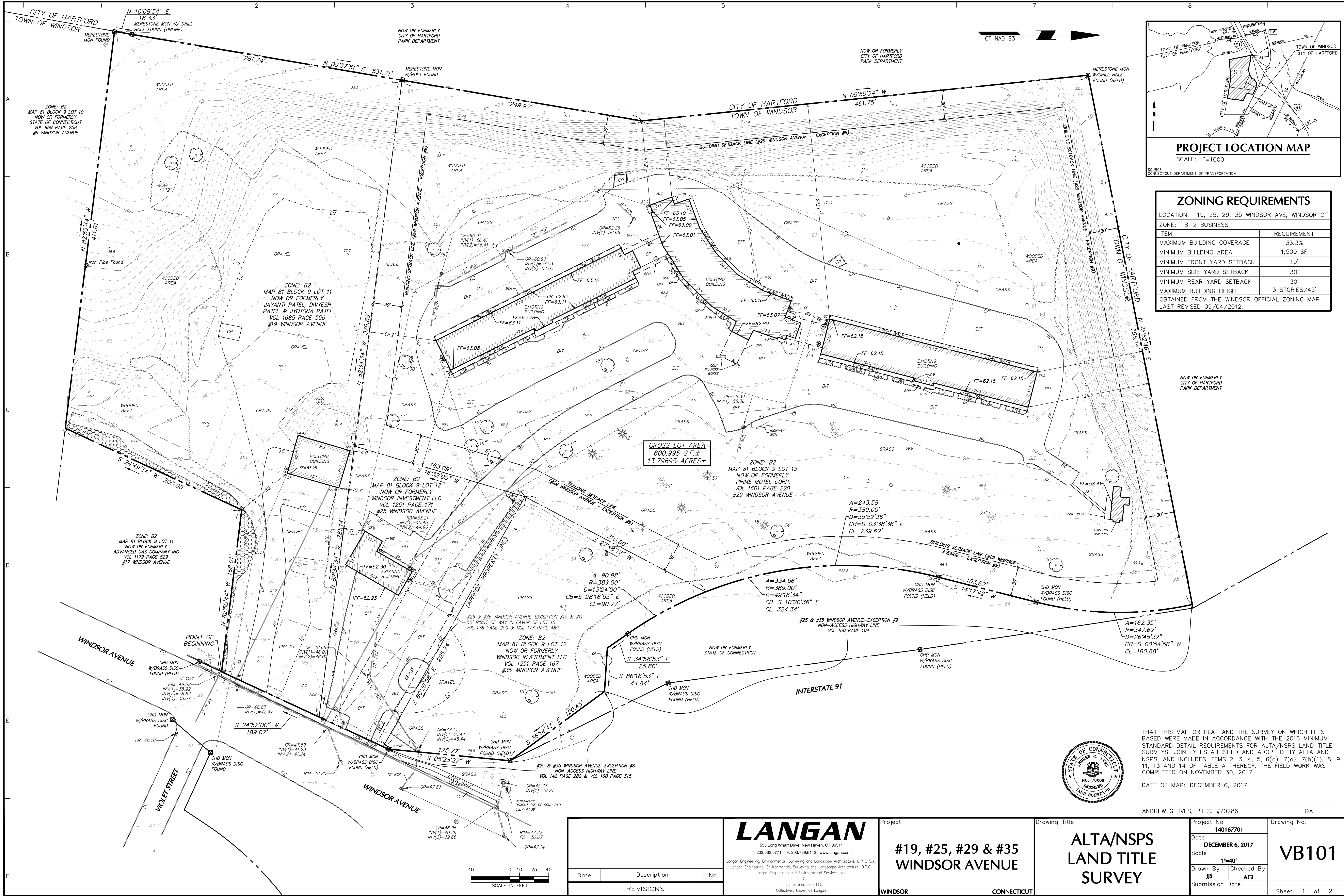
No.	Revision	Date	App'd.

Designed by \_\_\_\_\_ Checked by \_\_\_\_\_  
 Issued for \_\_\_\_\_ Date \_\_\_\_\_  
**Site Plan Approval** July 28, 2023

Not For Construction  
 Drawing Title  
**Fire Truck Route**

Project Number  
**42795.01**

Sheet **C-13** of 13



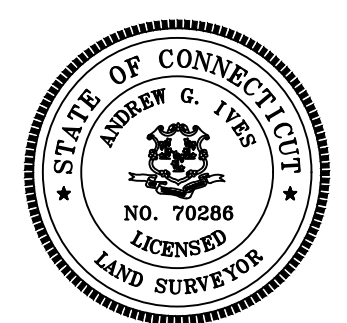
**ZONING REQUIREMENTS**

LOCATION: 19, 25, 29, 35 WINDSOR AVE, WINDSOR CT  
 ZONE: B-2 BUSINESS

ITEM	REQUIREMENT
MAXIMUM BUILDING COVERAGE	33.3%
MINIMUM BUILDING AREA	1,500 SF
MINIMUM FRONT YARD SETBACK	10'
MINIMUM SIDE YARD SETBACK	30'
MINIMUM REAR YARD SETBACK	30'
MAXIMUM BUILDING HEIGHT	3 STORIES/45'

OBTAINED FROM THE WINDSOR OFFICIAL ZONING MAP  
 LAST REVISED 09/04/2012.

NOW OR FORMERLY CITY OF HARTFORD PARK DEPARTMENT



THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2016 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 2, 3, 4, 5, 6(a), 7(a), 7(b)(1), 8, 9, 11, 13 AND 14 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON NOVEMBER 30, 2017.

DATE OF MAP: DECEMBER 6, 2017

ANDREW G. IVES, P.L.S. #70286 DATE

**LANGAN**  
 555 Long Wharf Drive, New Haven, CT 06511  
 T: 203.562.5771 F: 203.789.6142 www.langan.com  
 Langan Engineering, Environmental, Surveying and Landscape Architecture, D.P.C. S.A.  
 Langan Engineering, Environmental, Surveying and Landscape Architecture, D.P.C.  
 Langan Engineering and Environmental Services, Inc.  
 Langan CT, Inc.  
 Langan International LLC  
 Collectively known as Langan

Project  
**#19, #25, #29 & #35 WINDSOR AVENUE**  
 WINDSOR CONNECTICUT

Drawing Title  
**ALTA/NSPS LAND TITLE SURVEY**

Project No. **140167701**  
 Date **DECEMBER 6, 2017**  
 Scale **1"=40'**  
 Drawn By **JIS** Checked By **AGI**  
 Submission Date  
 Drawing No. **VB101**  
 Sheet 1 of 2

Date	Description	No.
REVISIONS		



PROJECT NO. 140167701

NOTES

1. THIS SURVEY HAS BEEN PREPARED PURSUANT TO THE REGULATIONS OF CONNECTICUT STATE AGENCIES SECTIONS 20-300b-1 THROUGH 20-300b-20 AND THE "STANDARDS FOR SURVEYS AND MAPS IN THE STATE OF CONNECTICUT" AS ADOPTED BY THE CONNECTICUT ASSOCIATION OF LAND SURVEYORS, INC. ON SEPTEMBER 26, 1996.

2. THIS SURVEY IS BASED UPON EXISTING PHYSICAL CONDITIONS FOUND AT THE SUBJECT SITE, DEED INFORMATION AND THE FOLLOWING REFERENCES:

A. MAP TITLED "TOWN OF WINDSOR MAP SHOWING LAND & RIGHTS OF ACCESS ACQUIRED FROM MARY L. CARVILLE BY THE STATE OF CONNECTICUT RELOCATION OF ROUTE U.S. 5-A", SCALE: 1"=40', DATED: MAY 1953, TOWN NO. 164, PROJECT NO. 58, SERIAL NO. 95, SHEET 1 OF 1

B. MAP TITLED "TOWN OF WINDSOR MAP SHOWING LAND RELEASED TO MARY L. CARVILLE ET AL BY THE STATE OF CONNECTICUT HARTFORD-SPRINGFIELD EXPRESSWAY", SCALE: 1"=40', DATED: OCTOBER 1955, TOWN NO. 164, PROJECT NO. 58, SERIAL NO. 1-B, SHEET 1 OF 1

C. MAP TITLED "PLOT PLAN SHOWING THE PROPERTY OF HENRY P. & MARY L. CARVILLE, WINDSOR AVE., WINDSOR, CONN.", SCALE: 1"=20', DATED: DEC. 1959, LAST REVISED: NOV. 1960, MAP VOLUME 5 PAGE 453

D. MAP TITLED "CONNECTICUT STATE HIGHWAY DEPARTMENT, RIGHT OF WAY MAP, TOWN OF WINDSOR, HARTFORD-SPRINGFIELD EXPRESSWAY FROM HARTFORD TOWN LINE NORTHERLY TO BINA AVENUE, NUMBER 164-06, SHEET NO. 1 OF 1", SCALE: 1"=100', DATED: JANUARY 29, 1960

E. MAP TITLED "RIGHT OF WAY SURVEY, STATE OF CONNECTICUT, DEPARTMENT OF TRANSPORTATION, RIGHT OF WAY MAP, TOWN OF WINDSOR, INTERSTATE ROUTE 91 IN THE VICINITY OF WINDSOR AVENUE, NUMBER 164-14, SHEET NO. 1A OF 1", SCALE: 1"=80', DATED: MARCH 30, 2001, LAST REVISED: 10/11/01

3. THE MERIDIAN OF THIS SURVEY IS REFERENCED TO CONNECTICUT STATE PLANE COORDINATE SYSTEM NAD 83 AS ESTABLISHED THROUGH GPS METHODS.

4. ELEVATIONS SHOWN ARE REFERENCED TO NAVD 88 ESTABLISHED THROUGH GPS METHODS.

5. PLANIMETRIC AND TOPOGRAPHIC INFORMATION SHOWN HEREON HAS BEEN OBTAINED FROM GROUND SURVEYS BY LANGAN CT, INC. FIELD WORK COMPLETED DURING THE MONTH OF NOVEMBER 2017.

6. AS PER THE NATIONAL FLOOD INSURANCE PROGRAM FIRM MAP ENTITLED "HARTFORD COUNTY, CONNECTICUT PANEL 366 OF 675, MAP NUMBER 09003003666, EFFECTIVE DATE SEPTEMBER 16, 2011" THE PROJECT AREA IS IN ZONE X (UNSHADED).

7. UNLESS SPECIFICALLY NOTED HEREON, STORM AND SANITARY SEWER INFORMATION (INCLUDING PIPE INVERT, PIPE MATERIAL, AND PIPE SIZE) WAS OBSERVED AND MEASURED AT FIELD LOCATED STRUCTURES (MANHOLES/CATCH BASINS, ETC.). CONDITIONS CAN VARY FROM THOSE ENCOUNTERED AT THE TIMES WHEN AND LOCATIONS WHERE DATA IS OBTAINED. DESPITE MEETING THE REQUIRED STANDARD OF CARE, THE SURVEYOR CANNOT, AND DOES NOT WARRANT THAT PIPE MATERIAL AND/OR PIPE SIZE THROUGHOUT THE PIPE RUN ARE THE SAME AS THOSE OBSERVED AT EACH STRUCTURE, OR THAT THE PIPE RUN IS STRAIGHT BETWEEN THE LOCATED STRUCTURES.

8. ADDITIONAL UTILITY (WATER, GAS, ELECTRIC ETC.) DATA MAY BE SHOWN FROM FIELD LOCATED SURFACE MARKINGS (BY OTHERS), EXISTING STRUCTURES, AND/OR FROM EXISTING DRAWINGS.

9. UNLESS SPECIFICALLY NOTED HEREON, THE SURVEYOR HAS NOT EXCAVATED TO PHYSICALLY LOCATE THE UNDERGROUND UTILITIES. THE SURVEYOR MAKES NO GUARANTEES THAT THE SHOWN UNDERGROUND UTILITIES ARE EITHER IN SERVICE, ABANDONED OR SUITABLE FOR USE, NOR ARE IN THE EXACT LOCATION OR CONFIGURATION INDICATED HEREON.

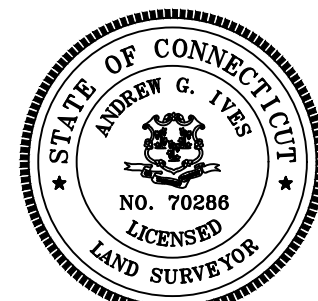
10. ALL BUILDINGS AND STRUCTURES WERE LOCATED AND MEASURED AT GROUND LEVEL. THE SURVEYOR MAKES NO DETERMINATIONS OR GUARANTEES AS TO THE ABSENCE, EXISTENCE OR LOCATION OF UNDERGROUND STRUCTURES, FOUNDATIONS, FOOTINGS, PROJECTIONS, WALLS, TANKS, SEPTIC SYSTEMS, ETC. NO TEST PITS, EXCAVATIONS OR GROUND PENETRATING RADAR WERE PERFORMED AS PART OF THIS SURVEY.

11. PRIOR TO ANY DESIGN OR CONSTRUCTION, THE PROPER UTILITY AGENCIES MUST BE CONTACTED FOR VERIFICATION OF UTILITY TYPE AND FOR FIELD LOCATIONS.

12. THIS SURVEY IS NOT VALID WITHOUT THE EMBOSSED OR INKED SEAL OF THE PROFESSIONAL.

THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2016 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 2, 3, 4, 5, 6(a), 7(a), 7(b)(1), 8, 9, 11, 13 AND 14 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON NOVEMBER 30, 2017.

DATE OF MAP: DECEMBER 6, 2017



ANDREW G. IVES, P.L.S. #70286 DATE

TITLE REPORT

#19 WINDSOR AVENUE COMMITMENT FOR TITLE INSURANCE ISSUED BY FIRST AMERICAN TITLE INSURANCE COMPANY. COMMITMENT NUMBER: HART2474357, EFFECTIVE DATE SEPTEMBER 28, 2017. SCHEDULE B SECTION II:

- 1. NOT SURVEY RELATED.
2. SURVEY PROVIDED.
3-7. NOT SURVEY RELATED.

#25 WINDSOR AVENUE COMMITMENT FOR TITLE INSURANCE ISSUED BY FIRST AMERICAN TITLE INSURANCE COMPANY. COMMITMENT NUMBER: HART2474292, EFFECTIVE DATE SEPTEMBER 28, 2017. SCHEDULE B SECTION II:

- 1. NOT SURVEY RELATED.
2. SURVEY PROVIDED.
3-7. NOT SURVEY RELATED.
8. WAIVER AND RELINQUISHMENT OF ACCESS TO ROUTE U.S. 5A AS DEFINED IN VOL 142 PAGE 282 AND VOL 160 PAGE 315. DEPICTED ON SURVEY.
9. DENIAL OF RIGHTS OF INGRESS AND EGRESS AS DEFINED IN VOL 160 PAGE 104. DEPICTED ON SURVEY.
10. RIGHT OF WAY AS DEFINED IN VOL 178 PAGE 200. DEPICTED ON SURVEY.
11. RIGHT OF WAY AS DEFINED IN VOL 178 PAGE 489.
12. NOT SURVEY RELATED.
13. ENCROACHMENT AGREEMENT AS DEFINED IN VOL 1626 PAGE 211. THE BITUMINOUS CURB ENCROACHMENT AS REFERENCED IN SAID DEED HAS BEEN REMOVED AND REPLACED WITH GRASS AREA. THIS SITE CHANGE DOES NOT AFFECT THE 50 FOOT RIGHT OF WAY EASEMENT, WHICH IS STILL ACCESSIBLE.

#29 WINDSOR AVENUE COMMITMENT FOR TITLE INSURANCE ISSUED BY FIRST AMERICAN TITLE INSURANCE COMPANY. COMMITMENT NUMBER: HART2474308, EFFECTIVE DATE SEPTEMBER 28, 2017. SCHEDULE B SECTION II:

- 1. NOT SURVEY RELATED.
2. SURVEY RELATED.
3-8. NOT SURVEY RELATED.
9. 30' BUILDING LINES AS SHOWN IN MAP #5467. DEPICTED ON SURVEY.
10. ENCROACHMENT AGREEMENT AS DEFINED IN VOL 1626 PAGE 211. THE BITUMINOUS CURB ENCROACHMENT AS REFERENCED IN SAID DEED HAS BEEN REMOVED AND REPLACED WITH GRASS AREA. THIS SITE CHANGE DOES NOT AFFECT THE 50 FOOT RIGHT OF WAY EASEMENT, WHICH IS STILL ACCESSIBLE.
#35 WINDSOR AVENUE COMMITMENT FOR TITLE INSURANCE ISSUED BY FIRST AMERICAN TITLE INSURANCE COMPANY. COMMITMENT NUMBER: HART2474303, EFFECTIVE DATE SEPTEMBER 28, 2017. SCHEDULE B SECTION II:
1. NOT SURVEY RELATED.
2. SURVEY PROVIDED.
3-7. NOT SURVEY RELATED.
8. WAIVER AND RELINQUISHMENT OF ACCESS TO ROUTE U.S. 5A AS DEFINED IN VOL 142 PAGE 282 AND VOL 160 PAGE 315. DEPICTED ON SURVEY.
9. DENIAL OF RIGHTS OF INGRESS AND EGRESS AS DEFINED IN VOL 160 PAGE 104. DEPICTED ON SURVEY.
10. RIGHT OF WAY AS DEFINED IN VOL 178 PAGE 200. DEPICTED ON SURVEY.
11. RIGHT OF WAY AS DEFINED IN VOL 178 PAGE 489.
12. NOT SURVEY RELATED.

LEGEND (NOT SHOWN TO SCALE)

Table with 4 columns listing symbols and their corresponding features: BOLLARD, COLUMN, FLAG POLE, MAILBOX, SIGN, SHRUB, TREE, CATCH BASIN, CLEANOUT, ELECTRIC METER, FIRE HYDRANT, GAS METER, GAS VALVE, GUY WIRE, LIGHT POLE, MANHOLE (TYPE AS LABELED), POWER POLE, STANDPIPE, TRAFFIC SIGNAL POLE, WATER VALVE, SPOT ELEVATION, BITUMINOUS, CONCRETE, CONCRETE PAD, LANDSCAPED AREA, BUILDING OVERHANG, BOTTOM OF WALL, EDGE OF PAVEMENT, EDGE OF GRAVEL, BITUMINOUS CURB, CONCRETE CURB, METAL GUARD RAIL, STOCKADE FENCE, CHAINLINK FENCE, TREE LINE, OVERHEAD WIRE, EASEMENT LINE, PROPERTY LINE, RIGHT-OF-WAY LINE, CONTOUR LINE, ELECTRIC MARK OUT LINE, COMMUNICATION MARK OUT LINE, GAS MARK OUT LINE.

SURVEYED DESCRIPTION

BEGINNING AT A POINT IN THE WESTERLY SIDE OF WINDSOR AVENUE, SAID POINT BEING A SOUTHEAST CORNER OF HEREIN DESCRIBED PARCEL AND THE NORTHEAST CORNER OF LAND NOW OR FORMERLY OF ADVANCED GAS COMPANY INC;

THENCE N 82° 55' 44" W BOUNDED SOUTHERLY BY LAND NOW OR FORMERLY OF ADVANCED GAS COMPANY INC A DISTANCE OF 168.01' TO A POINT;

THENCE S 24° 49' 34" W BOUNDED EASTERLY BY LAND NOW OR FORMERLY OF ADVANCED GAS COMPANY INC A DISTANCE OF 200.00' TO A POINT;

THENCE N 82° 55' 44" W BOUNDED SOUTHERLY BY LAND NOW OR FORMERLY OF STATE OF CONNECTICUT A DISTANCE OF 411.61' TO A POINT;

THENCE N 10° 08' 54" E BOUNDED WESTERLY BY LAND NOW OR FORMERLY OF CITY OF HARTFORD PARK DEPARTMENT A DISTANCE OF 18.33' TO A POINT;

THENCE N 09° 37' 51" E BOUNDED WESTERLY BY LAND NOW OR FORMERLY OF CITY OF HARTFORD PARK DEPARTMENT A DISTANCE OF 531.71' TO A POINT;

THENCE N 05° 50' 24" W BOUNDED WESTERLY BY LAND NOW OR FORMERLY OF CITY OF HARTFORD PARK DEPARTMENT A DISTANCE OF 461.75' TO A POINT;

THENCE N 78° 52' 46" E BOUNDED NORTHERLY BY LAND NOW OR FORMERLY OF CITY OF HARTFORD PARK DEPARTMENT A DISTANCE OF 555.14' TO A POINT;

THENCE CURVING TO THE RIGHT BOUNDED EASTERLY BY LAND NOW OR FORMERLY OF STATE OF CONNECTICUT, WITH AN ARC LENGTH OF 162.35', A RADIUS OF 347.62', AND INCLUDED ANGLE OF 26° 45' 32", AND A CHORD OF 160.88' WITH A BEARING OF S 00° 54' 56" W TO A POINT;

THENCE S 14° 17' 42" W BOUNDED EASTERLY BY LAND NOW OR FORMERLY OF STATE OF CONNECTICUT A DISTANCE OF 103.87' TO A POINT OF CURVATURE;

THENCE CURVING TO THE LEFT BOUNDED EASTERLY BY LAND NOW OR FORMERLY OF STATE OF CONNECTICUT, WITH AN ARC LENGTH OF 334.56', A RADIUS OF 389.00', AND INCLUDED ANGLE OF 49° 16' 34", AND A CHORD OF 324.34' WITH A BEARING OF S 10° 20' 36" E TO A POINT;

THENCE S 34° 58' 53" E BOUNDED EASTERLY BY LAND NOW OR FORMERLY OF STATE OF CONNECTICUT A DISTANCE OF 25.80' TO A POINT;

THENCE S 86° 16' 53" E BOUNDED NORTHERLY BY LAND NOW OR FORMERLY OF STATE OF CONNECTICUT A DISTANCE OF 44.84' TO A POINT;

THENCE S 36° 14' 43" E BOUNDED EASTERLY BY LAND NOW OR FORMERLY OF STATE OF CONNECTICUT A DISTANCE OF 120.45' TO A POINT IN THE WESTERLY SIDE OF WINDSOR AVENUE;

THENCE S 05° 28' 27" W ALONG THE WESTERLY SIDE OF INTERSTATE 91 A DISTANCE OF 125.77' TO A POINT;

THENCE S 24° 52' 00" W ALONG THE WESTERLY SIDE OF WINDSOR AVENUE A DISTANCE OF 189.07' TO THE POINT OF BEGINNING;

CONTAINING APPROXIMATELY 600,995 SQUARE FEET (13.79695 ACRES).

RECORD DESCRIPTION

#19 WINDSOR AVENUE A CERTAIN PIECE OR PARCEL OF LAND, WITH ALL THE BUILDINGS THEREON STANDING, SITUATED ON THE WESTERLY SIDE OF WINDSOR AVENUE, IN THE TOWN OF WINDSOR, COUNTY OF HARTFORD AND STATE OF CONNECTICUT, AND MORE PARTICULARLY BOUNDED AND DESCRIBED AS FOLLOWS:

COMMENCING AT A POINT SITUATED IN THE WESTERLY LINE OF WINDSOR AVENUE, AT THE SOUTHEAST CORNER OF LAND NOW OR FORMERLY OF CARVILLE'S RANCH HOUSE, INC., BEING THE NORTHEAST CORNER OF LAND HEREIN DESCRIBED; THENCE S 24 DEGREES 55' 07" W ALONG WINDSOR AVENUE, A DISTANCE OF 110.48 FEET TO A POINT; THENCE N 82 DEGREES 50' 11" W ALONG OTHER LAND OF CARMELLA SPONZO, X DISTANCE OF 168.00 FEET TO A POINT; THENCE S 24 DEGREES 55' 07" W ALONG OTHER LAND OF CARMELLA SPONZO, A DISTANCE OF 200.00 FEET TO A POINT; THENCE N 82 DEGREES 50' 11" W ALONG LAND NOW OR FORMERLY OF UNIVERSAL WOODWORK & SUPPLY, INC. A DISTANCE OF 411.38 FEET TO A POINT; THENCE N 10 DEGREES 11' 49" E ALONG LAND OF THE CITY OF HARTFORD, A DISTANCE OF 18.32 FEET TO A MONUMENT; THENCE N 9 DEGREES 40' 46" E ALONG LAND OF THE CITY OF HARTFORD, A DISTANCE OF 281.74 FEET TO A POINT; THENCE S 82 DEGREES 29' 01" E ALONG LAND NOW OR FORMERLY OF WINDSOR MOTEL, INC. AND LAND OF CARVILLE'S RANCH HOUSE, INC., PARTLY BY EACH, A DISTANCE OF 660.74 FEET TO THE POINT AND PLACE OF BEGINNING.

#29 WINDSOR AVENUE A CERTAIN PIECE OR PARCEL OF LAND, WITH ALL THE IMPROVEMENTS THEREON, KNOWN AS 29 WINDSOR AVENUE AND SITUATED WESTERLY OF U.S. ROUTE 5-A AND WINDSOR AVENUE IN THE TOWN OF WINDSOR, COUNTY OF HARTFORD, AND STATE OF CONNECTICUT AS SHOWN ON A CERTAIN MAP ENTITLED "BOUNDARY SURVEY PREPARED FOR PRIME HOTEL CORP., HABIB AMERICA BANK AND CHICAGO TITLE INSURANCE COMPANY, #29 WINDSOR AVENUE, WINDSOR, CONNECTICUT SCALE L " = 40', FEBRUARY 6, 2008, SCALE L " = 40 FEET, WHICH MAP WILL BE FILED IN THE OFFICE OF THE WINDSOR TOWN CLERK,

BEGINNING AT THE SOUTHWESTERLY MOST POINT ON SAID MAP, WHICH MARKS THE INTERSECTION OF THE SUBJECT PREMISES WITH LAND NOW OR FORMERLY OF CITY OF HARTFORD (KEENEY PARK) AND LAND NOW OR FORMERLY OF JAYANTI PATEL, MAHENDRA PATEL AND DIVYESH PATEL, THENCE PROCEEDING

NORTHEASTERLY, N 09° 37' 41" E, BY LAND NOW OR FORMERLY OF CITY OF HARTFORD (KEENEY PARK), A DISTANCE OF 249.97 FEET, THENCE

NORTHWESTERLY, N 05° 50' 34" W, BY LAND NOW OR FORMERLY OF CITY OF HARTFORD (KEENEY PARK), A DISTANCE OF 461.75 EAST; THENCE

NORTHEASTERLY, N 78° 52' 36" E BY LAND NOW OR FORMERLY OF CITY OF HARTFORD (KEENEY PARK), A DISTANCE OF 555.14 FEET; THENCE

SOUTHWESTERLY, IN A CURVE WITH A RADIUS OF 347.62 FEET, A DISTANCE OF 162.35 FEET TO A POINT, THENCE

SOUTHWESTERLY, S 14° 17' 32" W, A DISTANCE OF 103.87 FEET, THENCE

SOUTHERLY AGAIN, IN A CURVE WITH A RADIUS OF 389 FEET, A DISTANCE OF 243.58 FEET TO A POINT; THENCE

SOUTHWESTERLY, S 27° 48' 07" W, BY LAND NOW OR FORMERLY OF WINDSOR INVESTMENT, LLC, A DISTANCE OF 210.00 FEET; THENCE

SOUTHWESTERLY AGAIN, S 16° 31' 50" W, BY LAND NOW OR FORMERLY OF WINDSOR INVESTMENT, LLC, A DISTANCE OF 183.09 FEET; THENCE

NORTHWESTERLY, N 82° 34' 44" W BY LAND NOW OR FORMERLY OF JAYANTI PATEL, MAHENDRA PATEL AND DIVYESH PATEL, A DISTANCE OF 379.69 FEET, TO THE POINT AND PLACE OF BEGINNING.

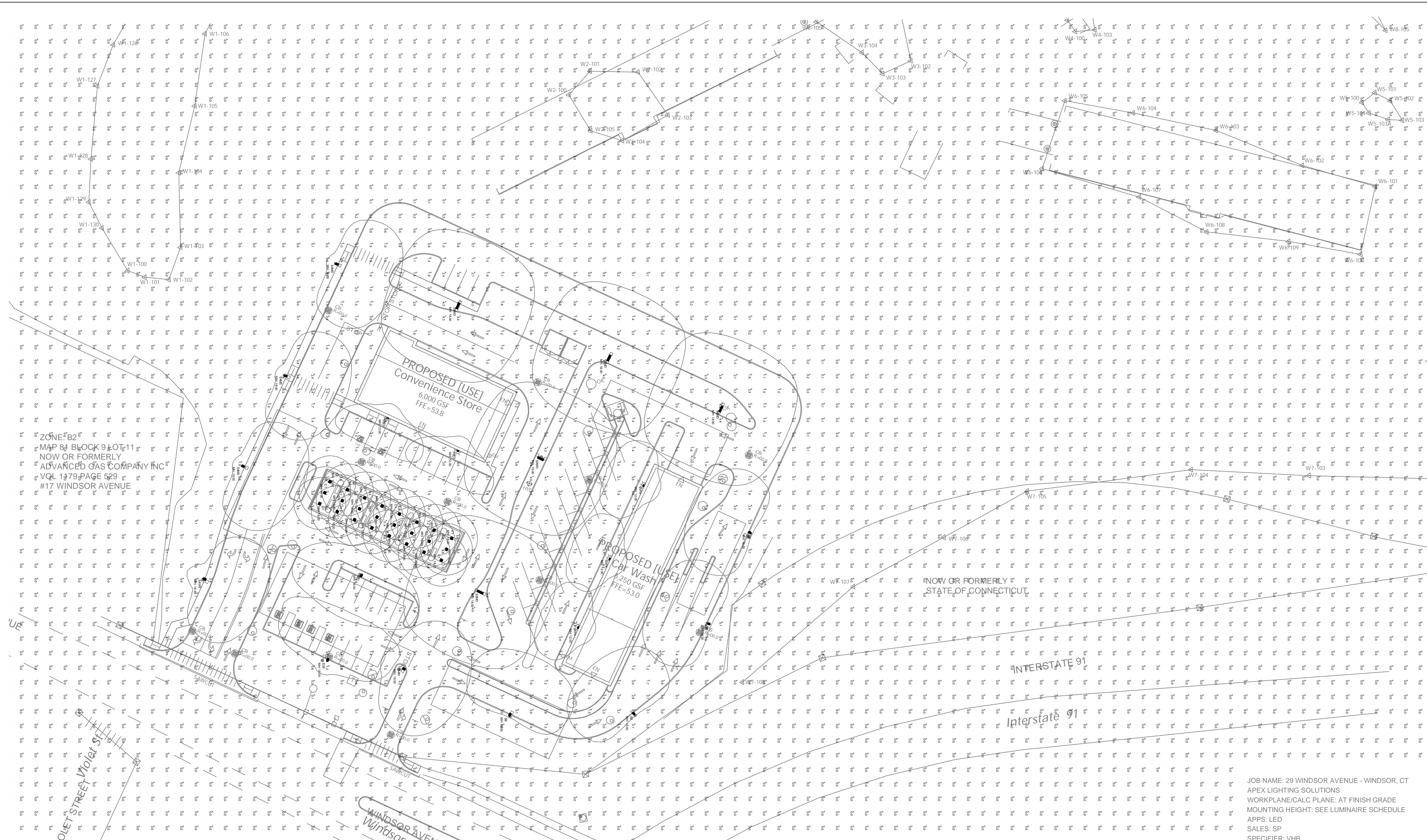
TOGETHER WITH, A FIFTY (50.00) FOOT RIGHT OF WAY TO #29 WINDSOR AVENUE FOR INGRESS AND EGRESS BY FOOT AND VEHICLE AS SHOWN ON SAID MAP

TOGETHER WITH AN ENCROACHMENT AGREEMENT BETWEEN WINDSOR INVESTMENT LLC AND PRIMA MOTEL CORP. DATED FEBRUARY 2, 2008 AND RECORDED IN VOLUME 1626 AT PAGE 211 OF THE WINDSOR LAND REWARDS.

#25 WINDSOR AVENUE A certain piece or parcel of land, with all the improvements thereon situated on the westerly side of U.S. Route 5-A and Windsor Avenue in the Town of Windsor, County of Hartford, and State of Connecticut and being more particularly bounded and described as follows, to wit:

Beginning at a point in the westerly line of Windsor Avenue, which point is the northeast corner of the within described parcel and the southeast corner of the within described parcel; thence proceeding south 82° 55' 44" west along land now or formerly of the City of Hartford, thence proceeding south 24° 49' 34" west along land now or formerly of the City of Hartford a distance of 168.01 feet to a point, at which point is located a monument; thence proceeding south 82° 55' 44" west along land now or formerly of the City of Hartford a distance of 411.61 feet to a point, at which point is located a monument; thence proceeding south 10° 08' 54" east along land now or formerly of the City of Hartford a distance of 18.33 feet to a point, at which point is located a monument; thence proceeding south 9° 37' 51" east along land now or formerly of the City of Hartford a distance of 531.71 feet to a point, at which point is located a monument; thence proceeding south 5° 50' 24" west along land now or formerly of the City of Hartford a distance of 461.75 feet to a point, at which point is located a monument; thence proceeding south 78° 52' 46" east along land now or formerly of the City of Hartford a distance of 555.14 feet to a point, at which point is located a monument; thence proceeding south 36° 14' 43" east along land now or formerly of the City of Hartford a distance of 120.45 feet to a point, at which point is located a monument; thence proceeding south 5° 28' 27" west along land now or formerly of the City of Hartford a distance of 125.77 feet to a point, at which point is located a monument; thence proceeding south 24° 52' 00" west along land now or formerly of the City of Hartford a distance of 189.07 feet to a point, at which point is located a monument; thence proceeding south 86° 16' 53" east along land now or formerly of the City of Hartford a distance of 44.84 feet to a point, at which point is located a monument; thence proceeding south 34° 58' 53" east along land now or formerly of the City of Hartford a distance of 25.80 feet to a point, at which point is located a monument; thence proceeding south 82° 55' 44" west along land now or formerly of the City of Hartford a distance of 103.87 feet to a point, at which point is located a monument; thence proceeding south 14° 17' 32" west along land now or formerly of the City of Hartford a distance of 103.87 feet to a point, at which point is located a monument; thence proceeding south 9° 37' 41" east along land now or formerly of the City of Hartford a distance of 249.97 feet to a point, at which point is located a monument; thence proceeding south 5° 50' 34" west along land now or formerly of the City of Hartford a distance of 461.75 feet to a point, at which point is located a monument; thence proceeding south 78° 52' 36" east along land now or formerly of the City of Hartford a distance of 555.14 feet to a point, at which point is located a monument; thence proceeding south 36° 14' 43" east along land now or formerly of the City of Hartford a distance of 120.45 feet to a point, at which point is located a monument; 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ZONE FB2  
 MAP 84 BLOCK 9 LOT 11  
 NOW OR FORMERLY  
 ADVANCED GAS COMPANY INC  
 VOL 1179 PAGE 529  
 #17 WINDSOR AVENUE

NOW OR FORMERLY  
 STATE OF CONNECTICUT

INTERSTATE 91

JOB NAME: 29 WINDSOR AVENUE - WINDSOR, CT  
 APEX LIGHTING SOLUTIONS  
 WORKPLANE/CALC PLANE: AT FINISH GRADE  
 MOUNTING HEIGHT: SEE LUMINAIRE SCHEDULE  
 APPS: LED  
 SALES: SP  
 SPECIFIER: VHB

Luminaire Schedule							
Qty	Label	Arrangement	Lumens	Input Watts	LLF	BUG Rating	Description
24	C1	Single	2352	20.49	0.850	N.A.	TRACELITE SCP-R-20-LG-VS-4K-WH / RECESSED IN CANOPY @ 18FT AFG
1	S2	Single	8940	54.129	0.850	B2-U0-G2	GARDCO OPF-S-A02-730-T2M-AR1-UNV-FINISH / MOUNTED TO SSA-CA-4-125-18-D1-DTX-xxx-FINISH
1	S3S	Single	6969	54.129	0.850	B1-U0-G2	GARDCO OPF-S-A02-730-T3M-AR1-UNV-FINISH-HIS / MOUNTED TO SSA-CA-4-125-18-D1-DTX-xxx-FINISH
1	S4M	Single	8989	54.129	0.850	B1-U0-G2	GARDCO OPF-S-A02-730-T4M-AR1-UNV-FINISH / MOUNTED TO SSA-CA-4-125-18-D1-DTX-xxx-FINISH
3	S4MS	Single	6993	54.129	0.850	B1-U0-G2	GARDCO OPF-S-A02-730-T4M-AR1-UNV-FINISH-HIS / MOUNTED TO SSA-CA-4-125-18-D1-DTX-xxx-FINISH
1	S4WS	Single	5733	54.129	0.850	B1-U0-G2	GARDCO OPF-S-A02-730-T4W-AR1-UNV-FINISH-HIS / MOUNTED TO SSA-CA-4-125-18-D1-DTX-xxx-FINISH
5	S5MD	Back-Back	9265	54.129	0.850	B3-U0-G2	GARDCO OPF-S-A02-730-T5M-AR1-UNV-FINISH / MOUNTED TO SSA-CA-4-125-18-D2-DTX-xxx-FINISH
4	SB	Single	6234	54.129	0.850	B0-U0-G2	GARDCO OPF-S-A02-730-BLC-AR1-UNV-FINISH / MOUNTED TO SSA-CA-4-125-18-D1-DTX-xxx-FINISH
2	WP3	Single	3687	34.3	0.850	B1-U0-G1	STONCO LPW16-30-WW-G3-3-UNV-FINISH / WALL MOUNTED @ 12FT AFG TO BOF
3	WP4	Single	3638	34.3	0.850	B1-U0-G1	STONCO LPW16-30-WW-G3-4-UNV-FINISH / WALL MOUNTED @ 12FT AFG TO BOF

Calculation Summary						
Label	Grid Height	Avg	Max	Min	Avg/Min	Max/Min
GAS STATION CANOPY	0	8.80	11.4	5.4	1.63	2.11
EXCLUSIVE VEHICLE USE	0	1.82	6.2	0.2	9.10	31.00
UNPAVED	0	0.04	4.7	0.0	N.A.	N.A.
PARKING, LOADING, PEDESTRIAN	0	1.98	5.8	0.4	4.95	14.50

**GENERAL DISCLAIMER:**

Calculations have been performed according to IES standards and good practice. Some differences between measured values and calculated results may occur due to tolerances in calculation methods, testing procedures, component performance, measurement techniques and field conditions such as voltage and temperature variations. Input data used to generate the attached calculations such as room dimensions, reflectances, furniture and architectural elements significantly affect the lighting calculations. If the real environment conditions do not match the input data, differences will occur between measured values and calculated values.

\* LLF Determined Using Current Published Lamp Data

**NOTE TO REVIEWER:**

Total Light Loss Factor (LLF) applied at time of design is determined by applying the Lamp Lumen Depreciation (LLD) from current lamp manufacturer's catalog, a Luminaire Dirt Depreciation Factor (LDD) based on IES recommended values and a Ballast Factor (BF) from current ballast specification sheets. Application of an incorrect Light Loss Factor (LLF) will result in forecasts of performance that will not accurately depict actual results. For proper comparison of photometric layouts, it is essential that you insist all designers use correct Light Loss Factors.



PROJECT TITLE:  
 WILSON CENTER  
 29 WINDSOR AVE  
 WINDSOR, CT

SCALE: 1"=40'-0"

DATE: 6/5/23

DRAWING TITLE:  
 SITE LIGHTING  
 PHOTOMETRIC CALCULATION

DRAWN BY: LED  
 SHEET:

SL-2



**Greylock Design Associates**  
Landscape Architecture & Land Planning

• 4 Lime Kiln Rd. Lenox, MA 01240 P. 413-637-8366  
• 784 B Tremont St. Boston, MA 02118 P. 617-351-0108

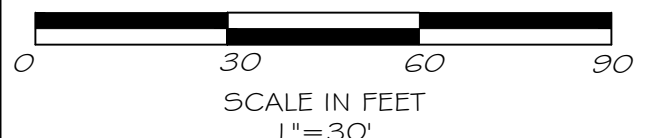
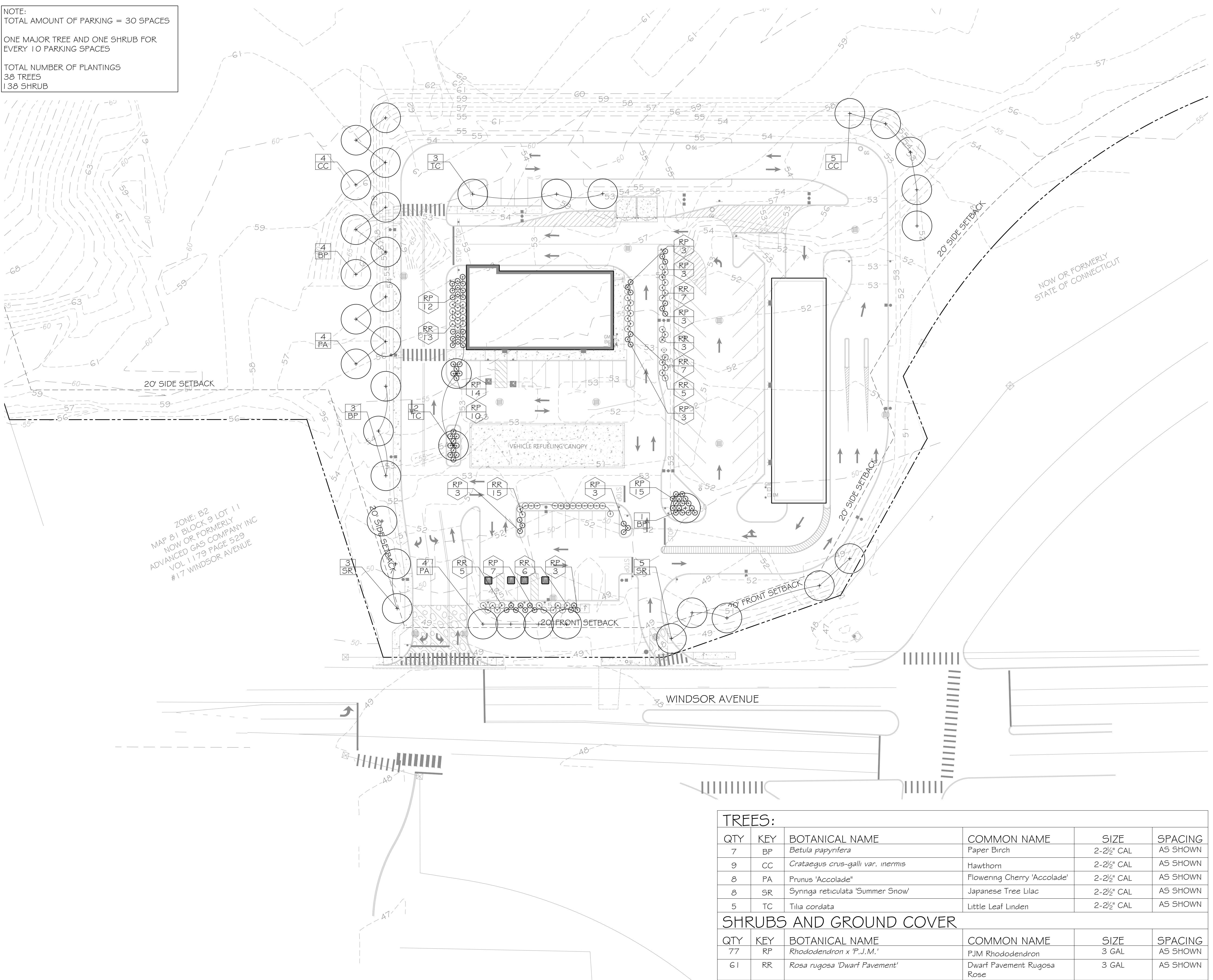
**PLAN NOTES**

PROPERTY LINE AND EXISTING CONDITIONS INFORMATION OBTAINED FROM DRAWING PREPARED BY VHB PREPARED MARCH 30, 2022.

**PLANTING NOTES**

- ALL PLANT MATERIAL TO BE HEALTHY NURSERY GROWN STOCK SUBJECT TO A.A.N. STANDARDS AND FREE OF DISEASE & INSECT INFESTATION, AND SHALL BE WANTED FOR ONE YEAR FROM THE DATE OF PROJECT SUBSTANTIAL COMPLETION. THE CONTRACTOR SHALL SUPPLY ALL PLANTS IN QUANTITIES SUFFICIENT TO COMPLETE THE WORK SHOWN ON THE DRAWINGS AND LISTED IN THE PLANT LIST. IN THE EVENT OF A DISCREPANCY BETWEEN QUANTITIES SHOWN IN THE PLANT LIST AND THOSE REQUIRED BY THE DRAWINGS, THE LARGER NUMBER SHALL APPLY.
- ALL PLANTS SHALL BE APPROVED PRIOR TO INSTALLATION AND SHALL BE LOCATED ON SITE BY THE CONTRACTOR FOR APPROVAL OF THE LANDSCAPE ARCHITECT. ANY INSTALLATIONS WHICH WERE NOT APPROVED BY THE LANDSCAPE ARCHITECT AND WHICH ARE SUBSEQUENTLY REQUESTED TO BE MOVED WILL BE DONE AT THE CONTRACTOR'S EXPENSE. NO SUBSTITUTIONS OF PLANT MATERIAL ARE TO BE MADE WITHOUT APPROVAL OF LANDSCAPE ARCHITECT. SUBSTITUTIONS WILL REQUIRE APPROVAL OF PLANNING DEPARTMENT.
- PRECISE LOCATION OF ITEMS NOT DIMENSIONED ON THE PLAN ARE TO BE FIELD STAKED BY THE CONTRACTOR AND SHALL BE SUBJECT TO THE REQUIREMENTS SPECIFIED IN THE PREVIOUS NOTE. ALL SHRUB MASSING AND TREE PLANTS SHALL BE MULCHED TO A DEPTH OF NO GREATER THAN 2" OF PINE BARK MULCH.
- TREES SHALL NOT BE STAKED OR GUYED UNLESS OTHERWISE INDICATED.
- THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGED VEGETATION AND SHALL REPLACE OR REPAIR ANY DAMAGED MATERIAL, AT HIS OWN EXPENSE. THE CONTRACTOR SHALL CONTACT "CALL BEFORE YOU DIG" PRIOR TO CONSTRUCTION.
- ALL SHRUB AND GROUND COVER PLANTINGS AREAS SHALL HAVE CONTINUOUS BEDS OF TOPSOIL 12" DEEP.
- AREAS DISTURBED BY CONSTRUCTION AND NOT RESTORED WITH IMPERVIOUS SURFACES (BUILDINGS, PAVEMENTS, WALKS, ETC.) SHALL RECEIVE 4" LOAM & SEED.
- THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UTILITIES IN THE FIELD WHERE PLANT MATERIAL MAY INTERFERE WITH THEM. THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT TO COORDINATE THEIR INSTALLATION.
- CONTRACTOR SHALL CONDUCT A SOIL ANALYSIS AND CONSULT LANDSCAPE ARCHITECT PRIOR TO INCORPORATING ALL NECESSARY SOIL AMENDMENTS FOR HEALTHY PLANT DEVELOPMENT.
- ALL EXISTING RILL, GULLY OR CHANNEL EROSION SHALL BE FILLED WITH APPROPRIATE BACKFILL MATERIAL, FINE RAKED, SCARIFIED AND STABILIZED WITH APPROPRIATE VEGETATIVE MATERIAL AND/OR APPROPRIATE SEDIMENTATION AND EROSION CONTROL MEASURES.
- ADJUSTMENTS IN THE LOCATION OF THE PROPOSED PLANT MATERIAL AS A RESULT OF EXISTING VEGETATION TO REMAIN SHALL BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- THE CONTRACTOR IS RESPONSIBLE FOR ALL MAINTENANCE REPAIR AND REPLACEMENT OF PLANT MATERIAL, AS REQUIRED, FOR THE DURATION OF THE PROJECT AND SUBSEQUENT WARRANTY PERIOD.
- PLANTINGS SHOULD BE INSTALLED DURING PRIME PLANTING MONTHS (MARCH - MAY AND SEPTEMBER - OCTOBER). PLANTINGS INSTALLED IN THE DRY SUMMER MONTHS AND OR LAWNS SEEDED OUT OF SPRING OR FALL PERIODS IF ALLOWED BY OWNER WILL REQUIRE AGGRESSIVE IRRIGATION PROGRAMS AT THE CONTRACTOR'S EXPENSE UNLESS OTHERWISE DIRECTED BY THE OWNER.
- UPON COMPLETION OF PLANTING, REMOVE FROM SITE ALL EXCESS SOIL, MULCH, AND MATERIALS AND DEBRIS RESULTING FROM WORK OPERATIONS. CLEAN UP SHOULD BE COMPLETED AT THE END OF EACH WORKING DAY. RESTORE TO ORIGINAL CONDITIONS ALL DAMAGED PAVEMENTS, PLANTING AREAS, STRUCTURES AND LAWN AREAS RESULTING FROM LANDSCAPING OPERATIONS.
- CONTRACTOR SHALL SURVEY, LOCATE, AND PROTECT ALL TREES WITHIN AREAS SHOWN AS "EXISTING TREES TO REMAIN" WITHIN THE DEVELOPMENT ENVELOPE FOR REVIEW BY LANDSCAPE ARCHITECT PRIOR TO CLEANING OPERATIONS. ALL EXISTING PLANT MATERIAL LISTED TO BE REMOVED SHALL INCLUDE ROOTS SYSTEMS AND BE REMOVED OFF SITE.
- PERENNIALS ARE TO BE PLACED 2' ON CENTER SPACING AND IN ODD NUMBER GROUPS OF 3, 5, 7...
- PLANT SYMBOLOLOGY SHOWN IS REPRESENTATIVE OF PLANTS THAT ARE OF 2/3 MATURITY OR 5-10 YEARS OF GROWTH.
- ALL PROPOSED PLANTS ARE NON-INVASIVE, AS DETERMINED FROM THE LATEST "CONNECTICUT INVASIVE PLANT LIST" DATED OCTOBER, 2018 PUBLISHED BY THE CONNECTICUT INVASIVE PLANTS COUNCIL.
- THE SIGNIFICANT TREES NOTED ON THE PLAN ARE TO REMAIN AND SHALL BE PRUNED TO THE EXTENT REQUIRED IN ORDER TO PROTECT THEM DURING CONSTRUCTION.
- CONTRACTOR SHALL COORDINATE WITH ADJACENT OWNERS PRIOR TO PRUNING ANY TREES WHOSE CANOPIES OVERHANG THE PROPERTY.

NOTE:  
TOTAL AMOUNT OF PARKING = 30 SPACES  
ONE MAJOR TREE AND ONE SHRUB FOR EVERY 10 PARKING SPACES  
TOTAL NUMBER OF PLANTINGS  
38 TREES  
138 SHRUB



**TREES:**

QTY	KEY	BOTANICAL NAME	COMMON NAME	SIZE	SPACING
7	BP	<i>Betula papyrifera</i>	Paper Birch	2-2½" CAL	AS SHOWN
9	CC	<i>Crataegus crus-galli var. inermis</i>	Hawthorn	2-2½" CAL	AS SHOWN
8	PA	<i>Prunus 'Accolade'</i>	Flowering Cherry 'Accolade'	2-2½" CAL	AS SHOWN
8	SR	<i>Syringa reticulata 'Summer Snow'</i>	Japanese Tree Lilac	2-2½" CAL	AS SHOWN
5	TC	<i>Tilia cordata</i>	Little Leaf Linden	2-2½" CAL	AS SHOWN

**SHRUBS AND GROUND COVER**

QTY	KEY	BOTANICAL NAME	COMMON NAME	SIZE	SPACING
77	RP	<i>Rhododendron x 'P.J.M.'</i>	PJM Rhododendron	3 GAL	AS SHOWN
61	RR	<i>Rosa rugosa 'Dwarf Pavement'</i>	Dwarf Pavement Rugosa Rose	3 GAL	AS SHOWN

0	Permitting	07.28.2023	RTA
No.	Revision	Date	Appr.

Project Title  
**MIXED-USE DEVELOPMENT**  
29 Windsor Avenue  
Windsor, Connecticut

Designed by	Checked by
Issued for	Date
<b>Permit</b>	<b>June 6, 2023</b>

Drawing Title  
**PLANTING PLAN**

Drawing Number

**L-401**

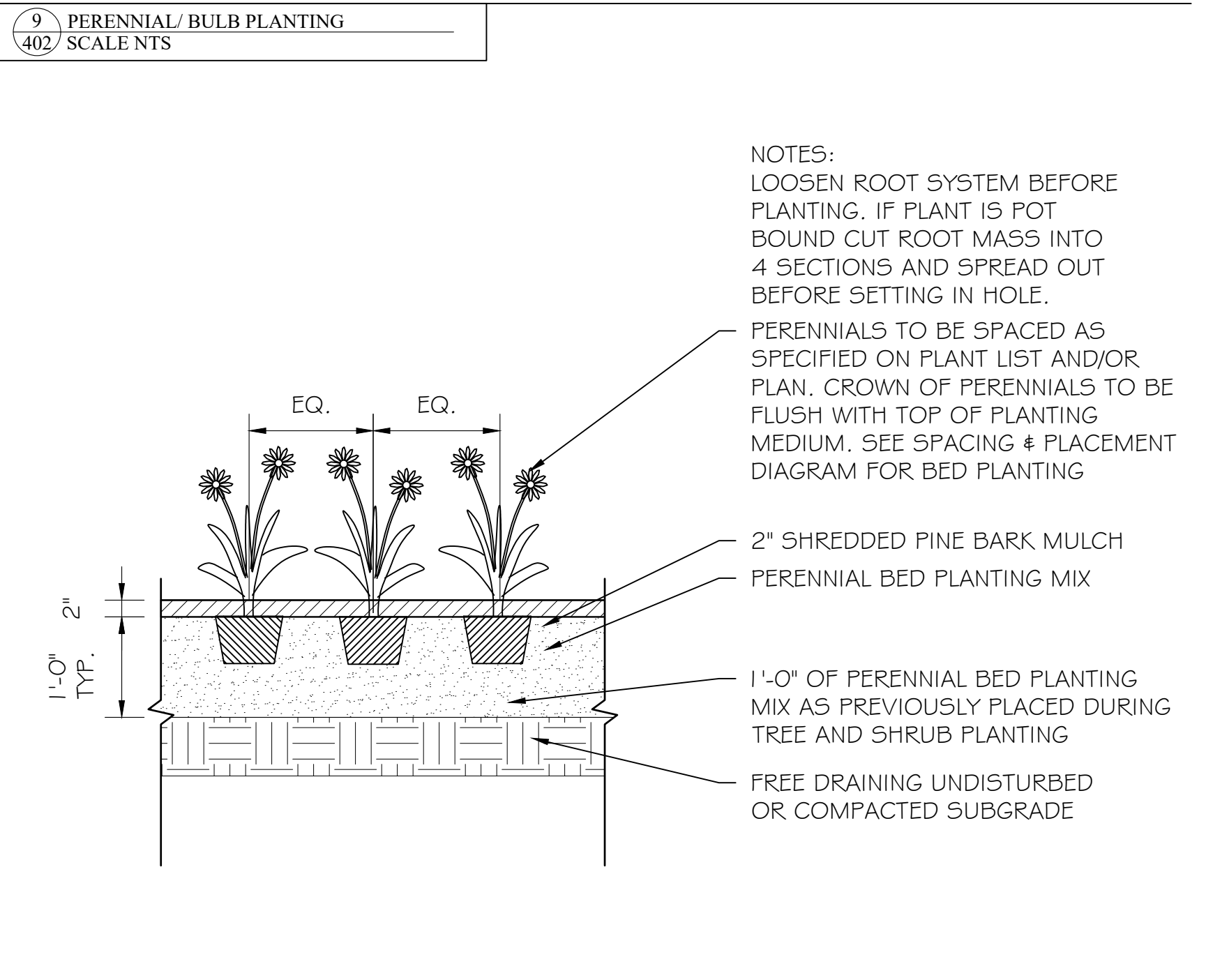
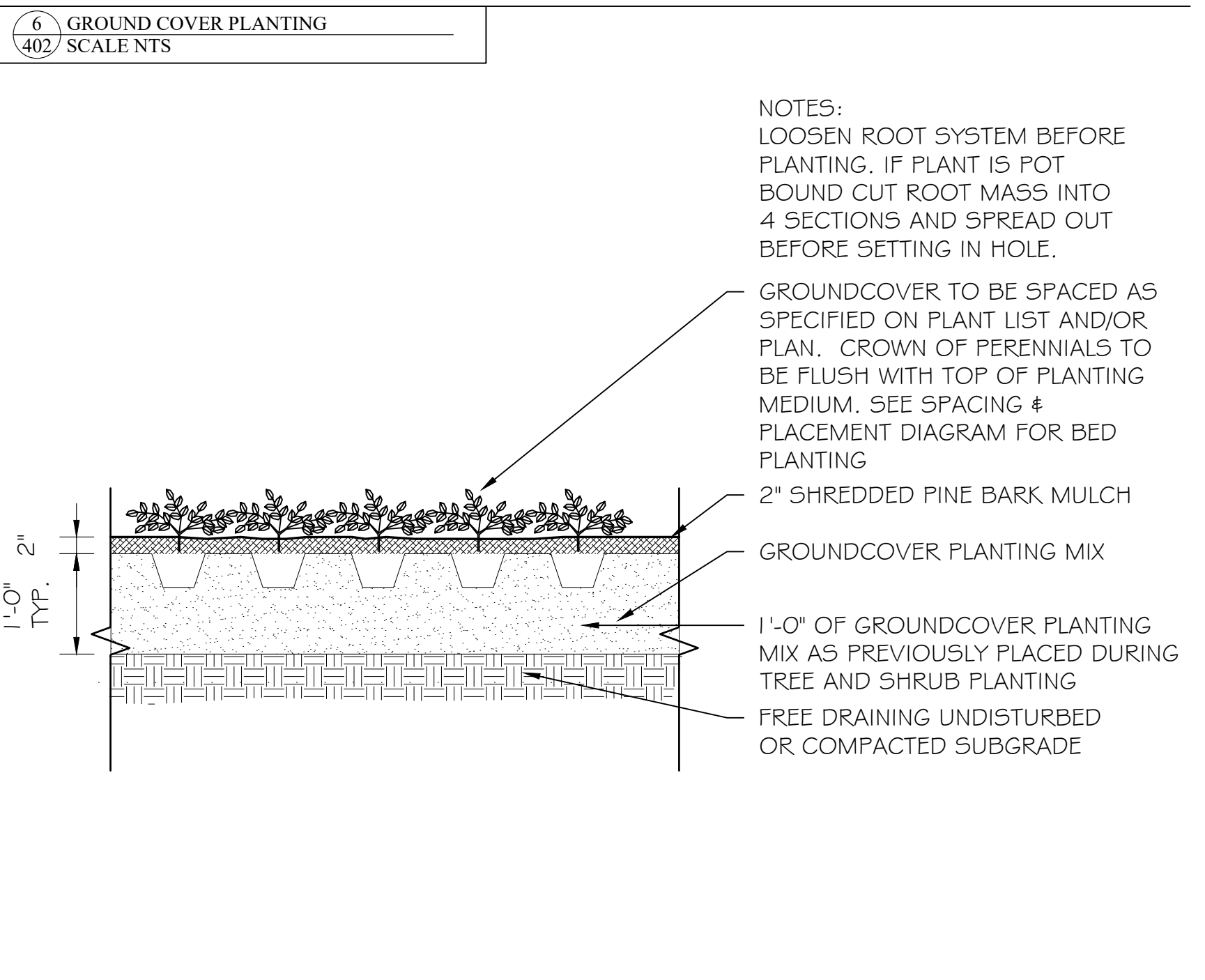
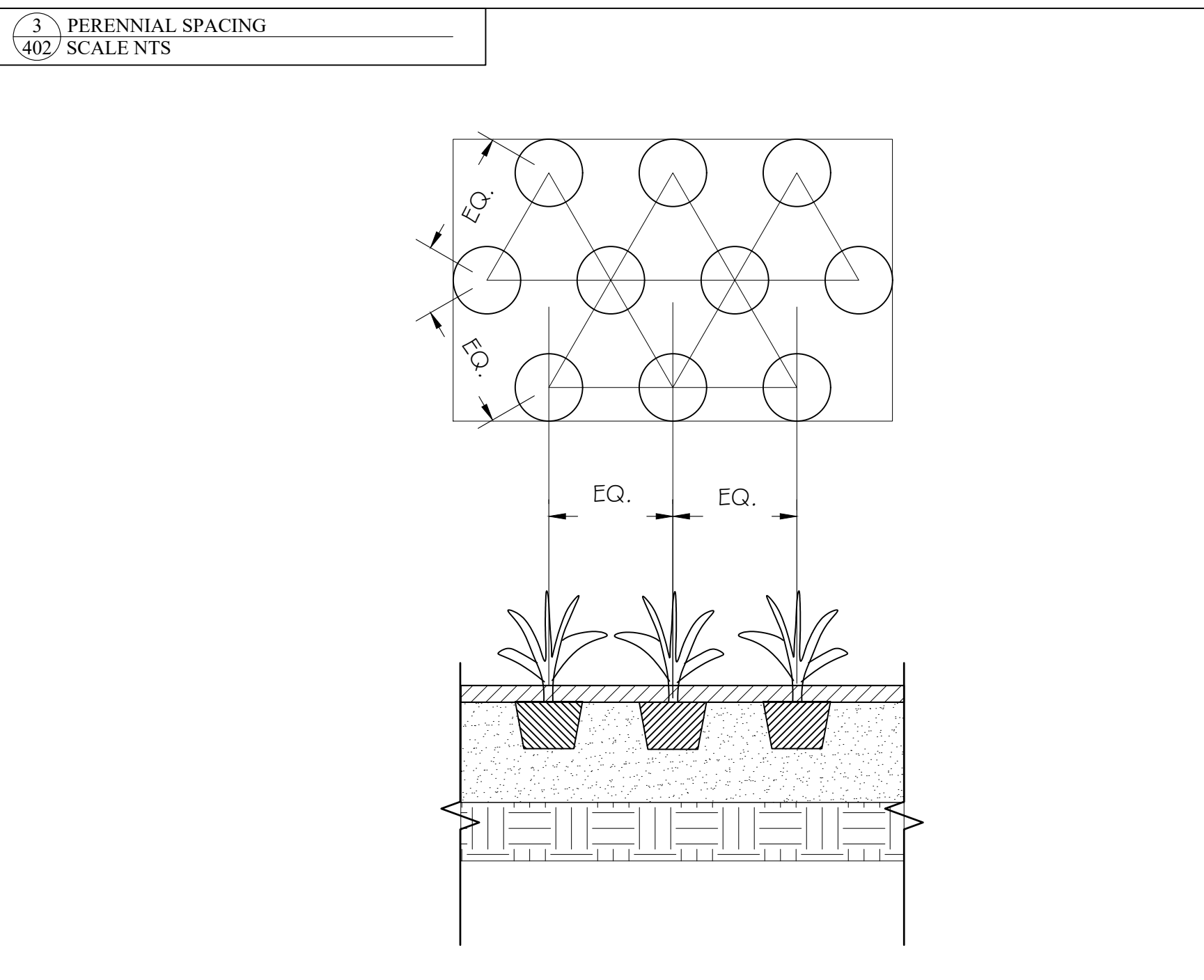
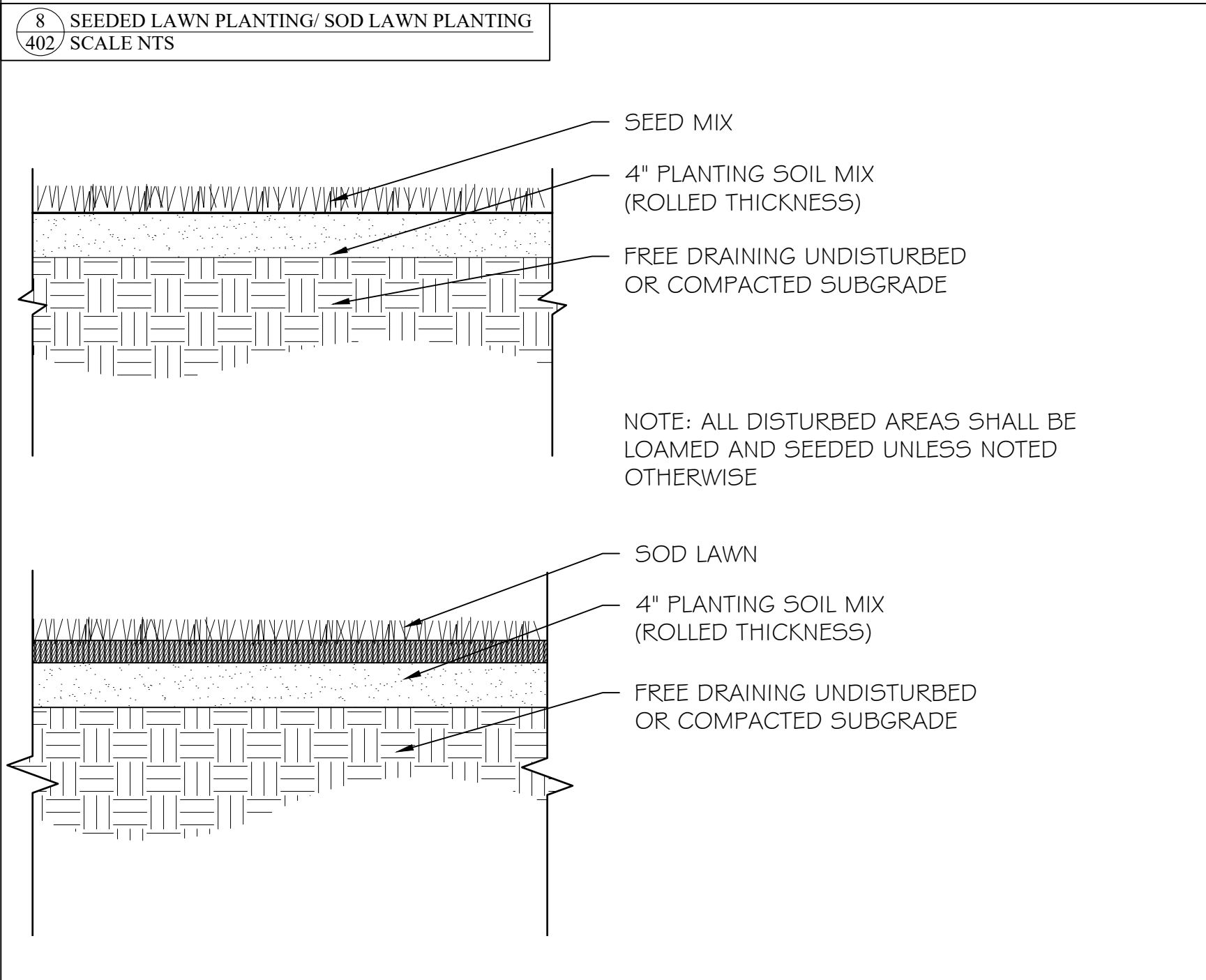
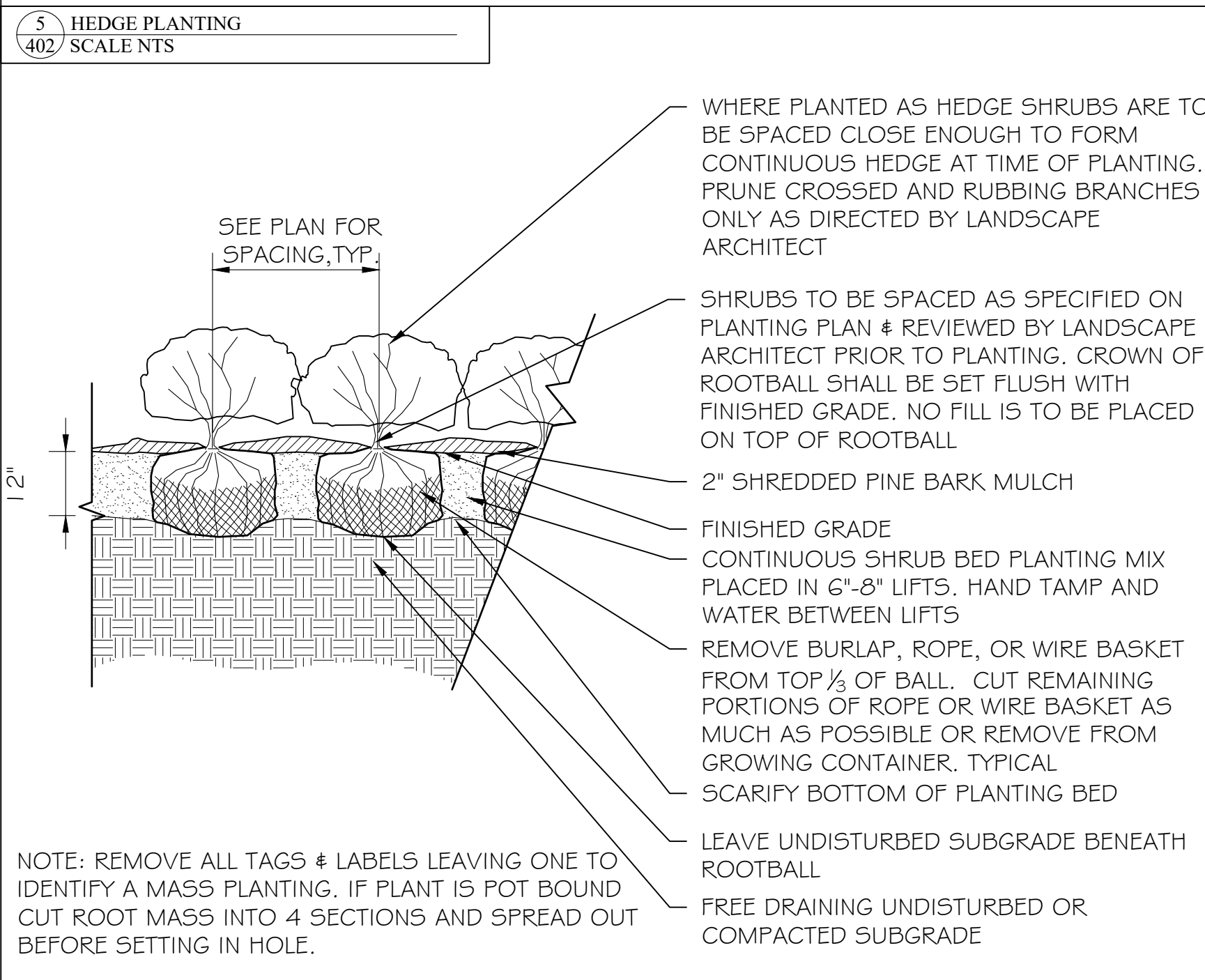
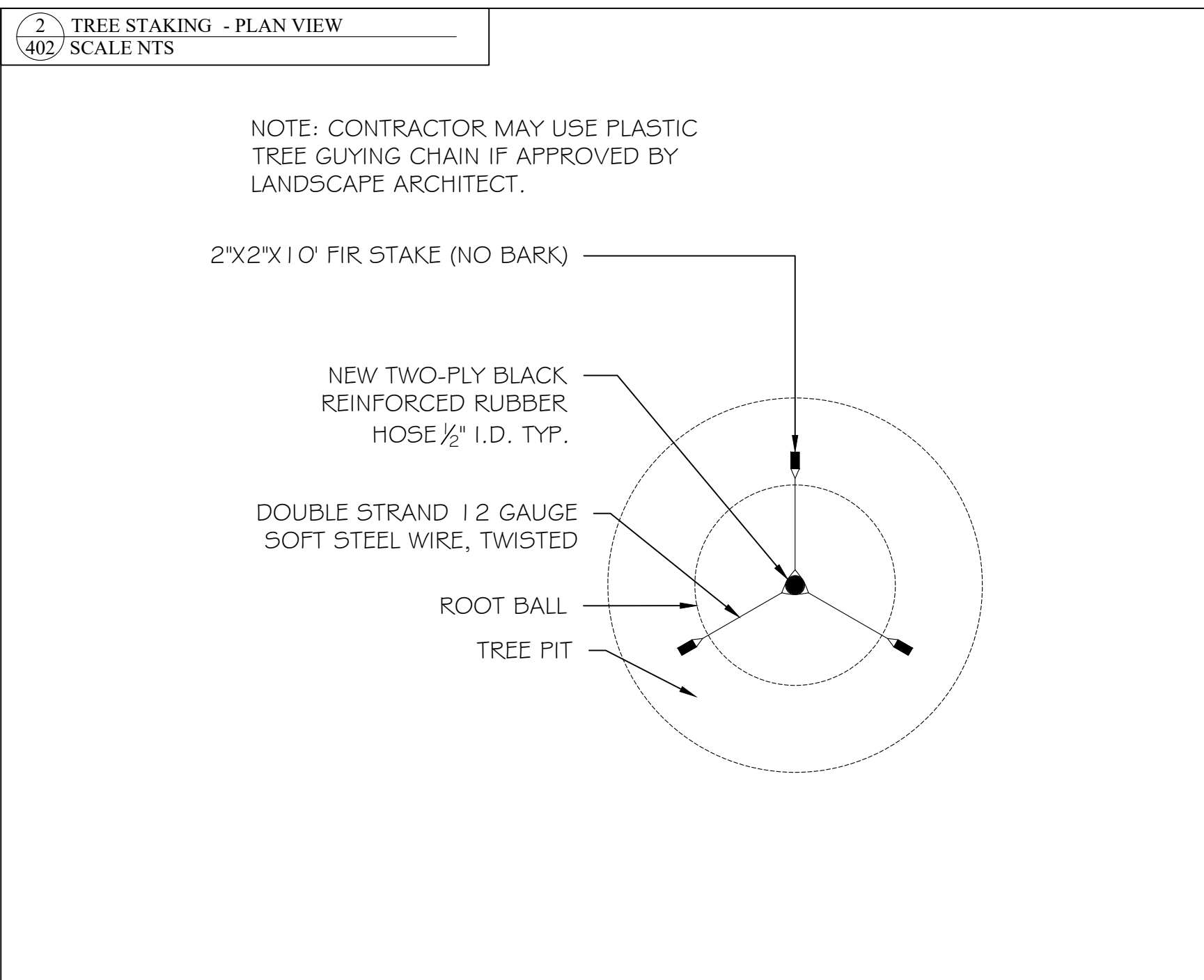
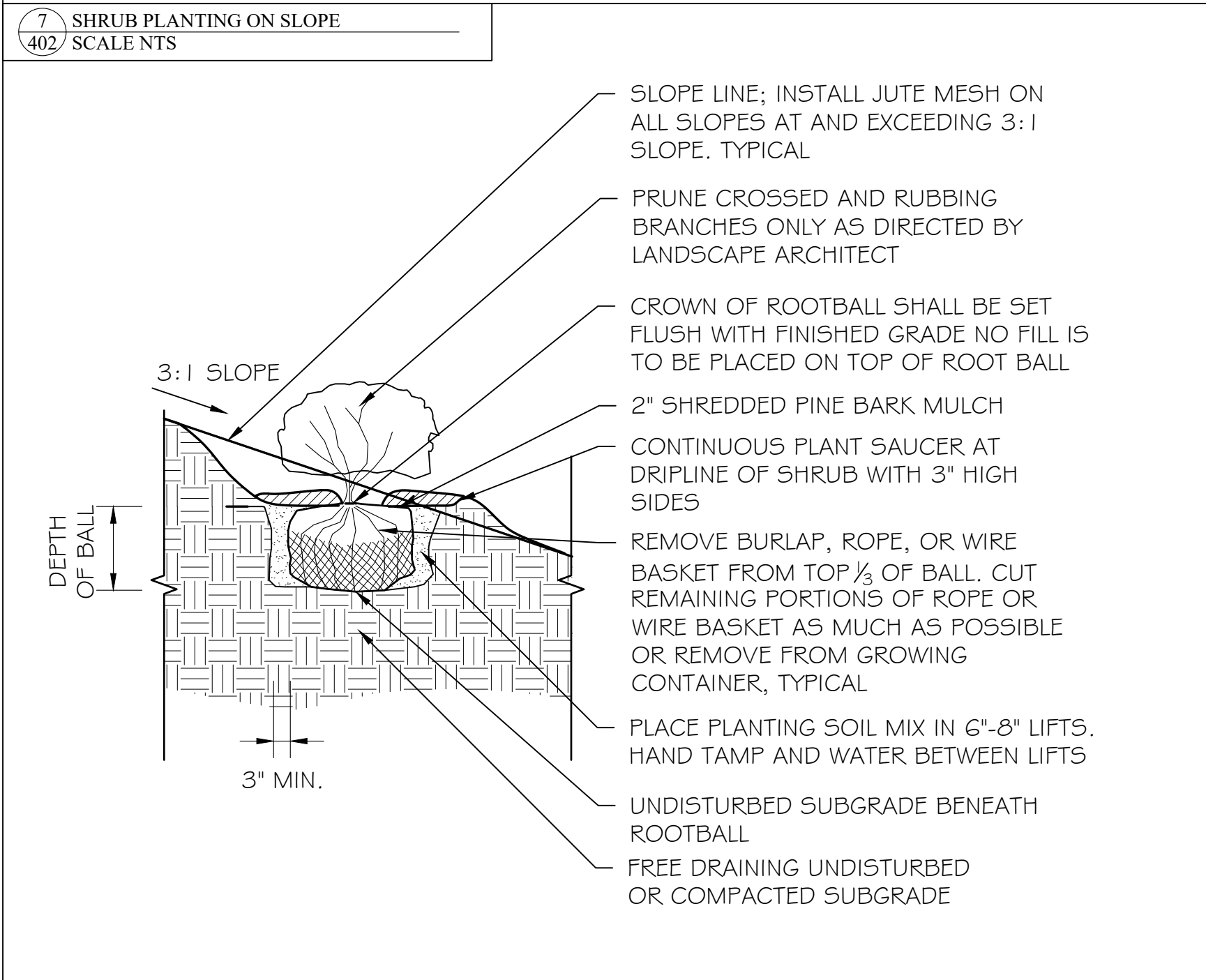
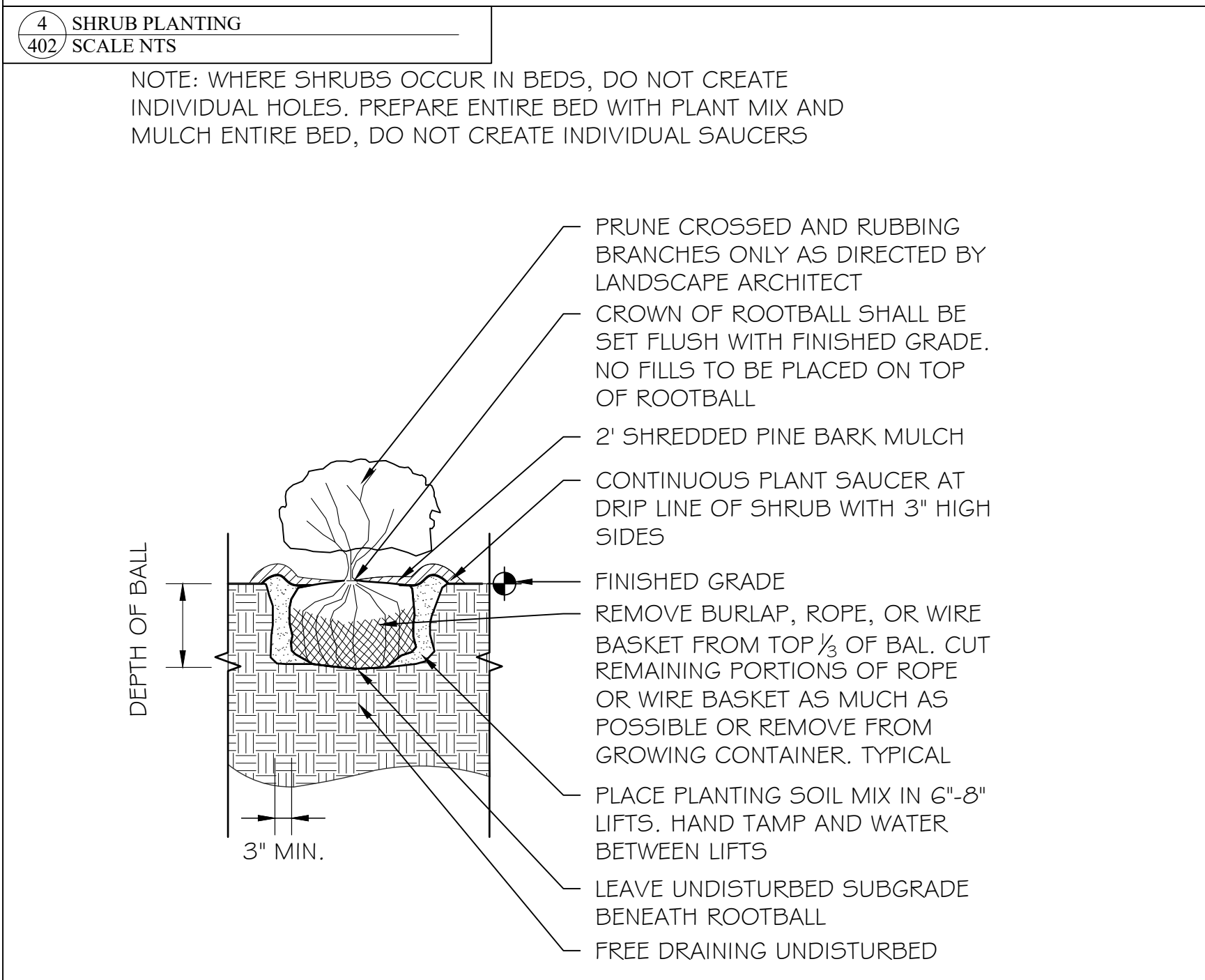
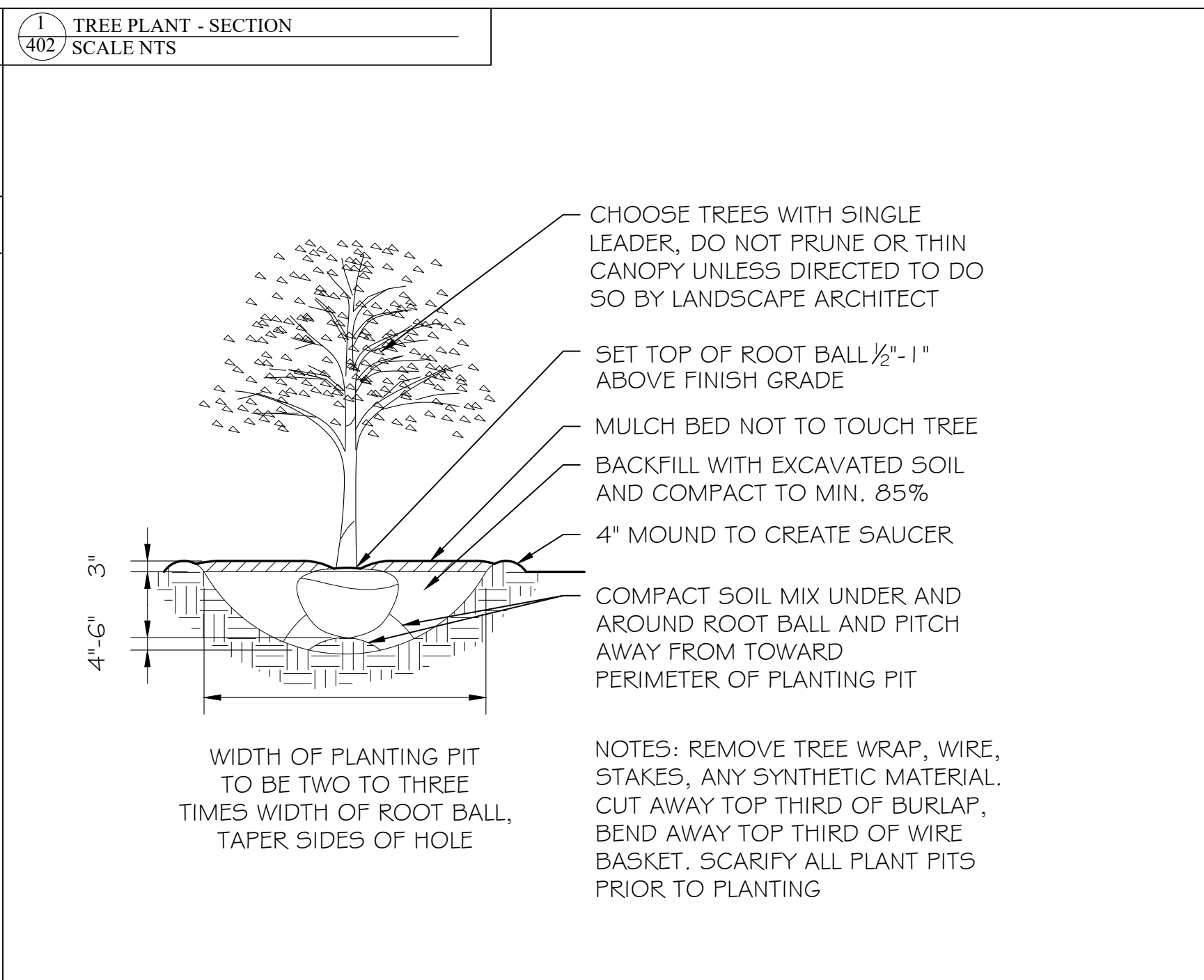
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**PLAN NOTES**

PROPERTY LINE AND EXISTING CONDITIONS INFORMATION OBTAINED FROM DRAWING PREPARED BY VHB PREPARED MARCH 30, 2022.

**PLANTING NOTES**

- ALL PLANT MATERIAL TO BE HEALTHY NURSERY GROWN STOCK SUBJECT TO A.A.N. STANDARDS AND FREE OF DISEASE & INSECT INFESTATION, AND SHALL BE WARRANTED FOR ONE YEAR FROM THE DATE OF PROJECT SUBSTANTIAL COMPLETION.
- THE CONTRACTOR SHALL SUPPLY ALL PLANTS IN QUANTITIES SUFFICIENT TO COMPLETE THE WORK SHOWN ON THE DRAWINGS AND LISTED IN THE PLANT LIST. IN THE EVENT OF A DISCREPANCY BETWEEN QUANTITIES SHOWN IN THE PLANT LIST AND THOSE REQUIRED BY THE DRAWINGS, THE LARGER NUMBER SHALL APPLY.
- ALL PLANTS SHALL BE APPROVED PRIOR TO INSTALLATION AND SHALL BE LOCATED ON SITE BY THE CONTRACTOR FOR APPROVAL OF THE LANDSCAPE ARCHITECT. ANY INSTALLATIONS WHICH WERE NOT APPROVED BY THE LANDSCAPE ARCHITECT AND WHICH ARE SUBSEQUENTLY REQUESTED TO BE MOVED WILL BE DONE AT THE CONTRACTOR'S EXPENSE. NO SUBSTITUTIONS OF PLANT MATERIAL ARE TO BE MADE WITHOUT APPROVAL OF LANDSCAPE ARCHITECT. SUBSTITUTIONS WILL REQUIRE APPROVAL OF PLANNING DEPARTMENT.
- PRECISE LOCATION OF ITEMS NOT DIMENSIONED ON THE PLAN ARE TO BE FIELD STAKED BY THE CONTRACTOR AND SHALL BE SUBJECT TO THE REQUIREMENTS SPECIFIED IN THE PREVIOUS NOTE.
- ALL SHRUB MASSING AND TREE PITS SHALL BE MULCHED TO A DEPTH OF NO GREATER THAN 2" OF PINE BARK MULCH.
- TREES SHALL NOT BE STAKED OR GUYED UNLESS OTHERWISE INDICATED.
- THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGED VEGETATION AND SHALL REPLACE OR REPAIR ANY DAMAGED MATERIAL, AT HIS OWN EXPENSE. THE CONTRACTOR SHALL CONTACT "CALL BEFORE YOU DIG" AT PRIOR TO CONSTRUCTION.
- ALL SHRUB AND GROUND COVER PLANTINGS AREAS SHALL HAVE CONTINUOUS BEDS OF TOPSOIL 1 2" DEEP.
- AREAS DISTURBED BY CONSTRUCTION AND NOT RESTORED WITH IMPERVIOUS SURFACES (BUILDINGS, PAVEMENTS, WALKS, ETC.) SHALL RECEIVE 4" LOAM & SEED
- THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UTILITIES IN THE FIELD WHERE PLANT MATERIAL MAY INTERFERE WITH UTILITIES. THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT TO COORDINATE THEIR INSTALLATION.
- CONTRACTOR SHALL CONDUCT A SOIL ANALYSIS AND CONSULT LANDSCAPE ARCHITECT PRIOR TO INCORPORATING ALL NECESSARY SOIL AMENDMENTS FOR HEALTHY PLANT DEVELOPMENT.
- ALL EXISTING RILL, GULLY OR CHANNEL EROSION SHALL BE FILLED WITH APPROPRIATE BACKFILL MATERIAL, FINE RAKED, SCARIFIED AND STABILIZED WITH APPROPRIATE VEGETATIVE MATERIAL AND/OR APPROPRIATE SEDIMENTATION AND EROSION CONTROL MEASURES.
- ADJUSTMENTS IN THE LOCATION OF THE PROPOSED PLANT MATERIAL AS A RESULT OF EXISTING VEGETATION TO REMAIN SHALL BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- THE CONTRACTOR IS RESPONSIBLE FOR ALL MAINTENANCE REPAIR AND REPLACEMENT OF PLANT MATERIAL, AS REQUIRED, FOR THE DURATION OF THE PROJECT AND SUBSEQUENT WARRANTY PERIOD.
- PLANTINGS SHOULD BE INSTALLED DURING PRIME PLANTING MONTHS (MARCH - MAY AND SEPTEMBER - OCTOBER). PLANTINGS INSTALLED IN THE DRY SUMMER MONTHS AND OR LAWN SEEDED OUT OF SPRING OR FALL PERIODS IF ALLOWED BY OWNER WILL REQUIRE AGGRESSIVE IRRIGATION PROGRAMS AT THE CONTRACTOR'S EXPENSE UNLESS OTHERWISE DIRECTED BY OWNER.
- UPON COMPLETION OF PLANTING, REMOVE FROM SITE ALL EXCESS SOIL, MULCH, AND MATERIALS AND DEBRIS RESULTING FROM WORK OPERATIONS. CLEAN UP SHOULD BE COMPLETED AT THE END OF EACH WORKING DAY. RESTORE TO ORIGINAL CONDITIONS ALL DAMAGED PAVEMENTS, PLANTING AREAS, STRUCTURES AND LAWN AREAS RESULTING FROM LANDSCAPING OPERATIONS.
- CONTRACTOR SHALL SURVEY, LOCATE, AND PROTECT ALL TREES WITHIN AREAS SHOWN AS "EXISTING TREES TO REMAIN" WITHIN THE DEVELOPMENT ENVELOPE FOR REVIEW BY LANDSCAPE ARCHITECT PRIOR TO CLEARING OPERATIONS. ALL EXISTING PLANT MATERIAL LISTED TO BE REMOVED SHALL INCLUDE ROOTS SYSTEMS AND BE REMOVED OFF SITE.
- PERENNIALS ARE TO BE PLACED 2" ON CENTER SPACING AND IN ODD NUMBER GROUPS OF 3, 5, 7.
- PLANT SYMBOLS SHOWN IS REPRESENTATIVE OF PLANTS THAT ARE OF 2/3 MATURITY OR 5-10 YEARS OF GROWTH.
- ALL PROPOSED PLANTS ARE NON-INVASIVE, AS DETERMINED FROM THE LATEST "CONNECTICUT INVASIVE PLANT LIST" DATED OCTOBER, 2018 PUBLISHED BY THE CONNECTICUT INVASIVE PLANTS COUNCIL.
- THE SIGNIFICANT TREES NOTED ON THE PLAN ARE TO REMAIN AND SHALL BE PRUNED TO THE EXTENT REQUIRED IN ORDER TO PROTECT THEM DURING CONSTRUCTION.
- CONTRACTOR SHALL COORDINATE WITH ADJACENT OWNERS PRIOR TO PRUNING ANY TREES WHOSE CANOPIES OVERHANG THE PROPERTY.



**Greylock Design Associates**  
Landscape Architecture & Land Planning

• 4 Lime Kiln Rd. Lenox, MA 01240 P. 413-637-8366

• 784 B Tremont St. Boston, MA 02118 P. 617-351-0108

0 Permitting 07.28.2023 RTA  
No. Revision Date Apprd.

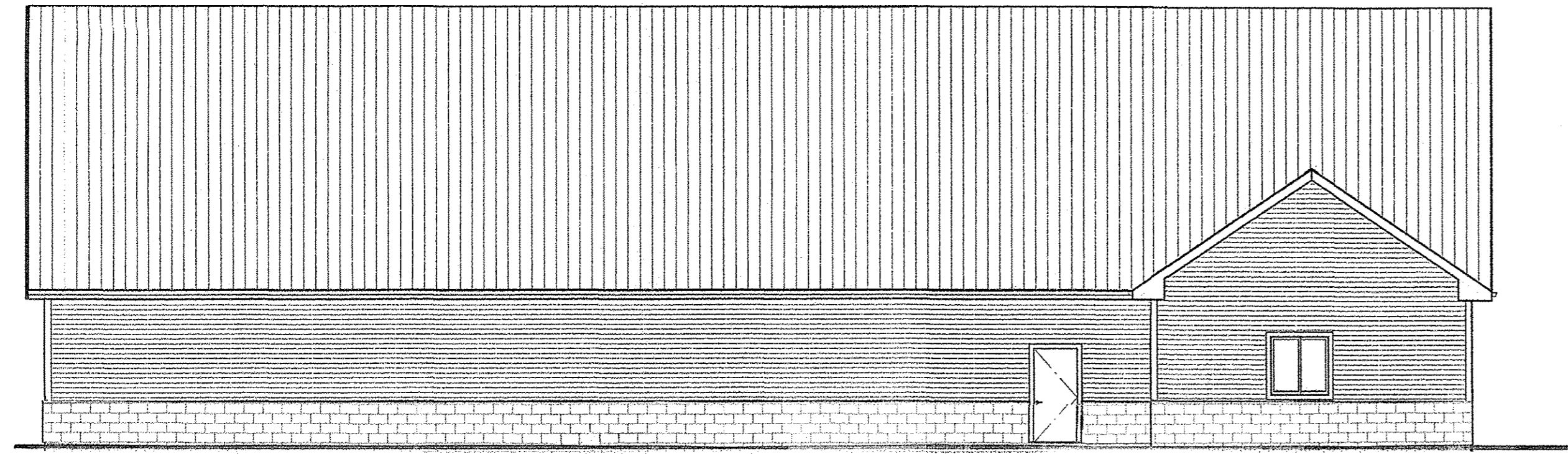
Project Title  
**MIXED-USE DEVELOPMENT**  
29 Windsor Avenue  
Windsor, Connecticut

Designed by GDA Checked by RTA  
Issued for Date

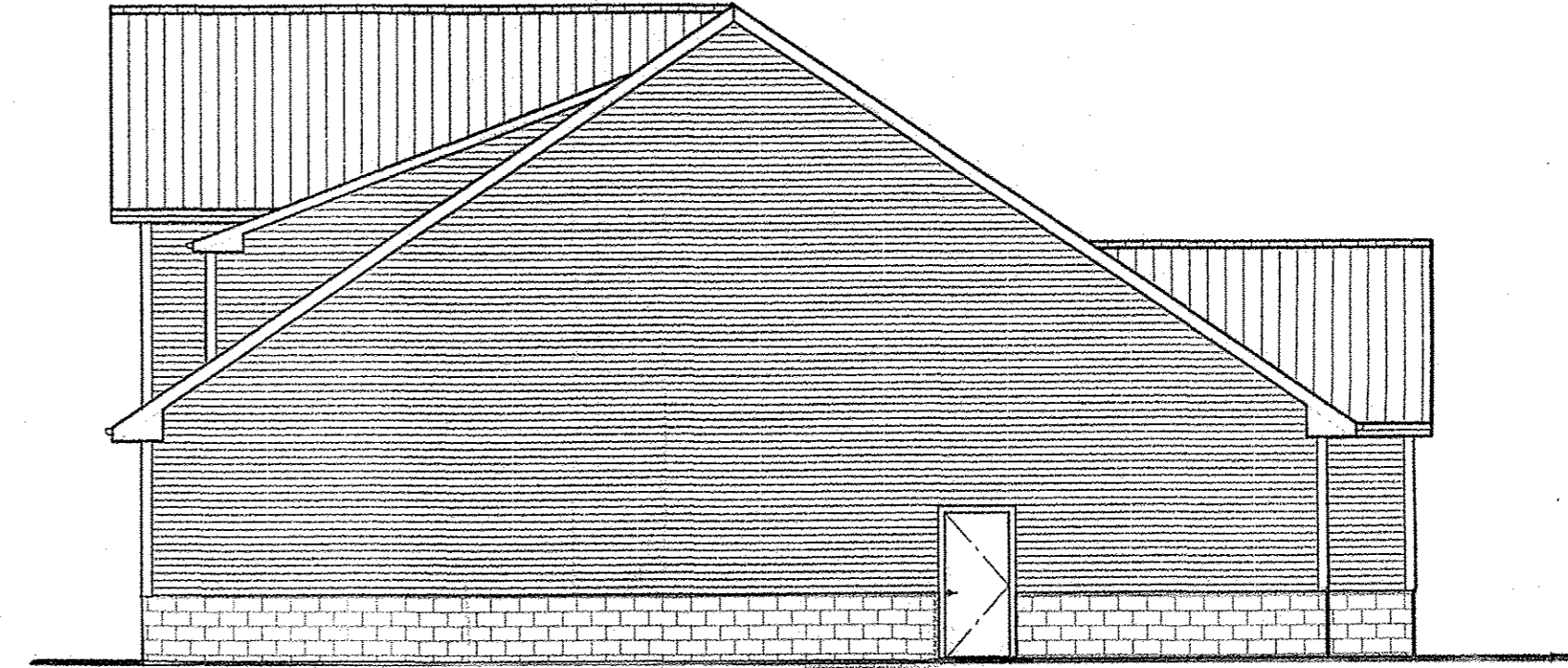
Permit July 28, 2023

Drawing Title  
**PLANTING DETAILS**  
Drawing Number

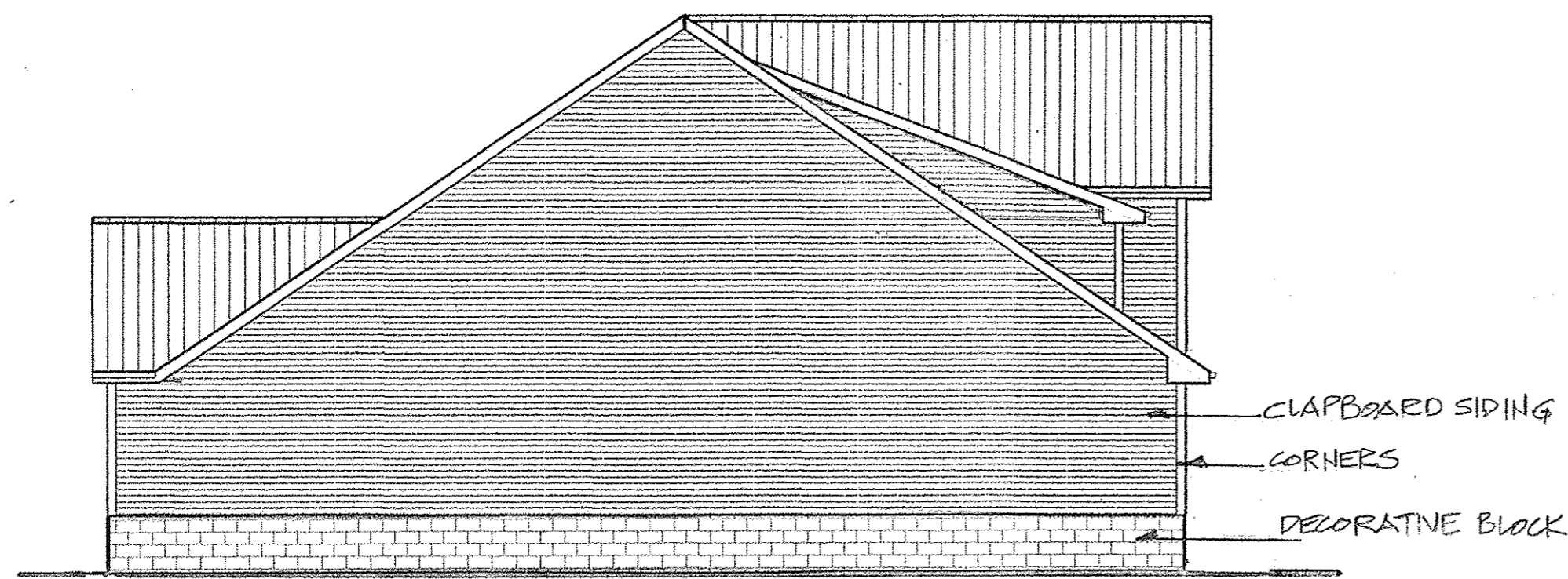
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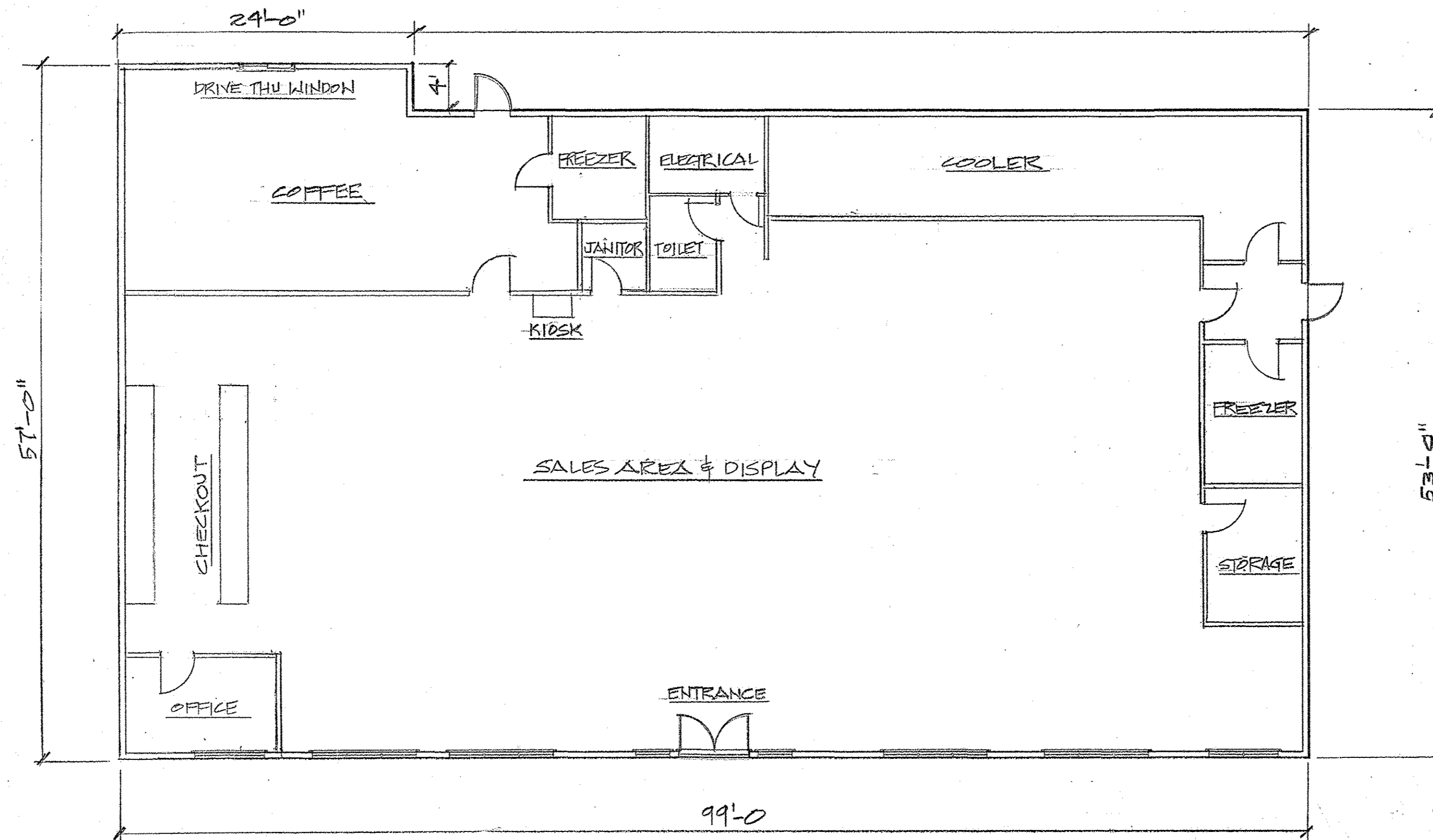
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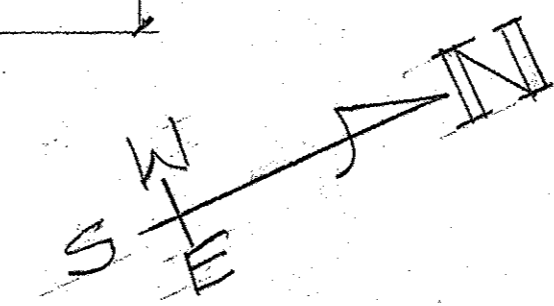
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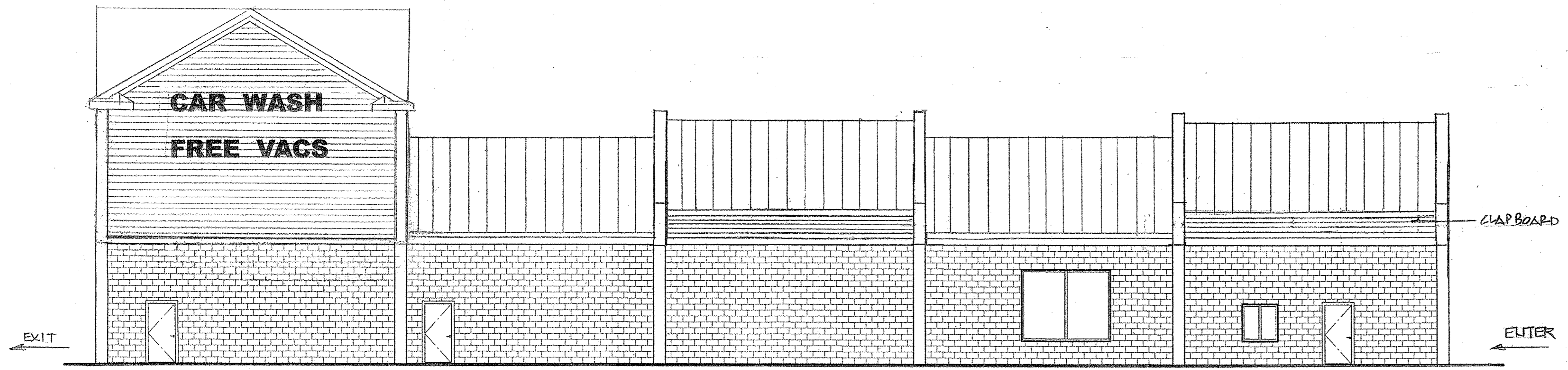
FLOOR PLAN



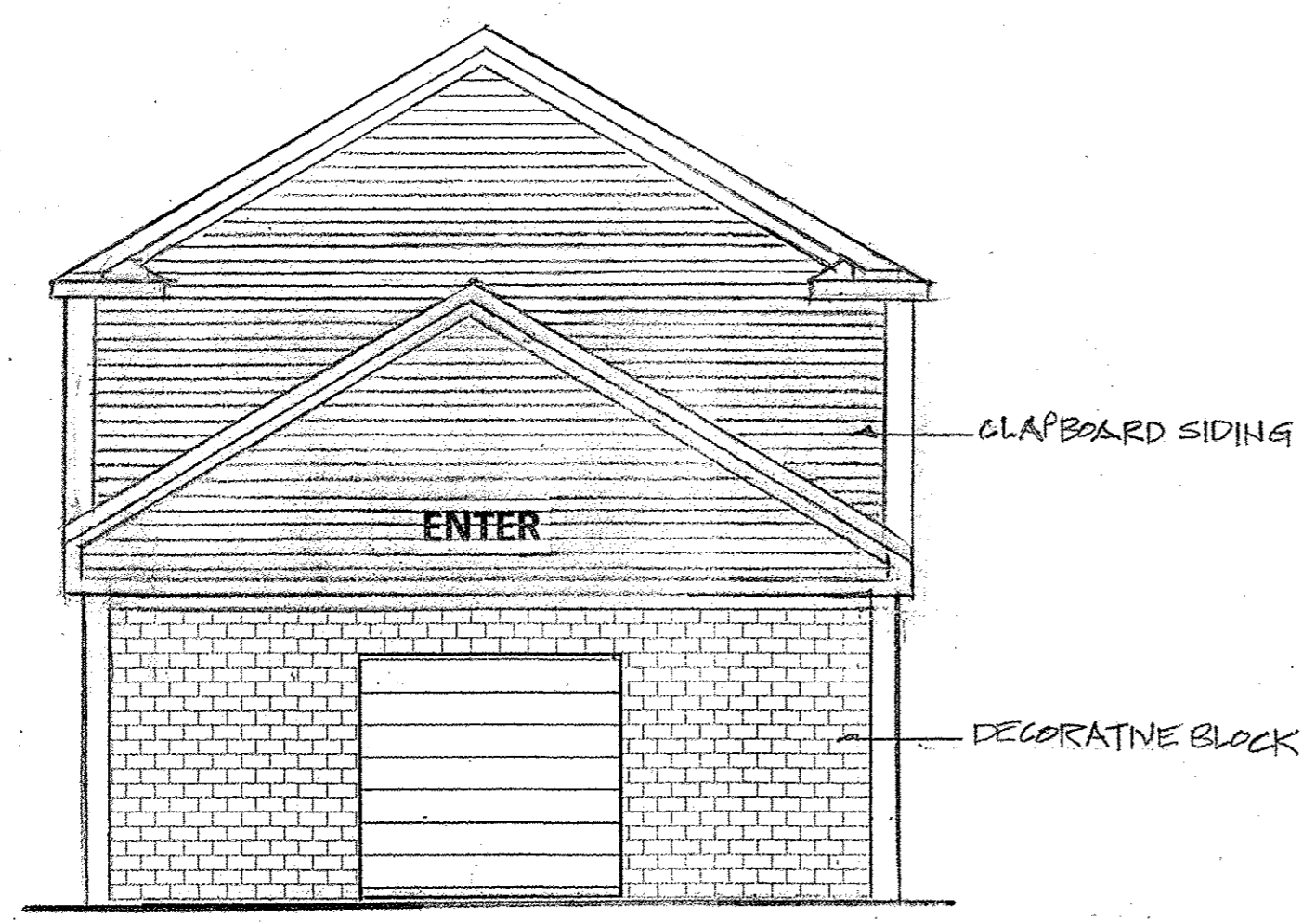
DATE: 7-6-2023 REVISIONS:

PROPOSED CONVENIENCE STORE  
29 WINDSOR AVENUE  
WILSON CENTER  
WINDSOR, CT  
SCALE: 1/8" = 1'-0"

A1



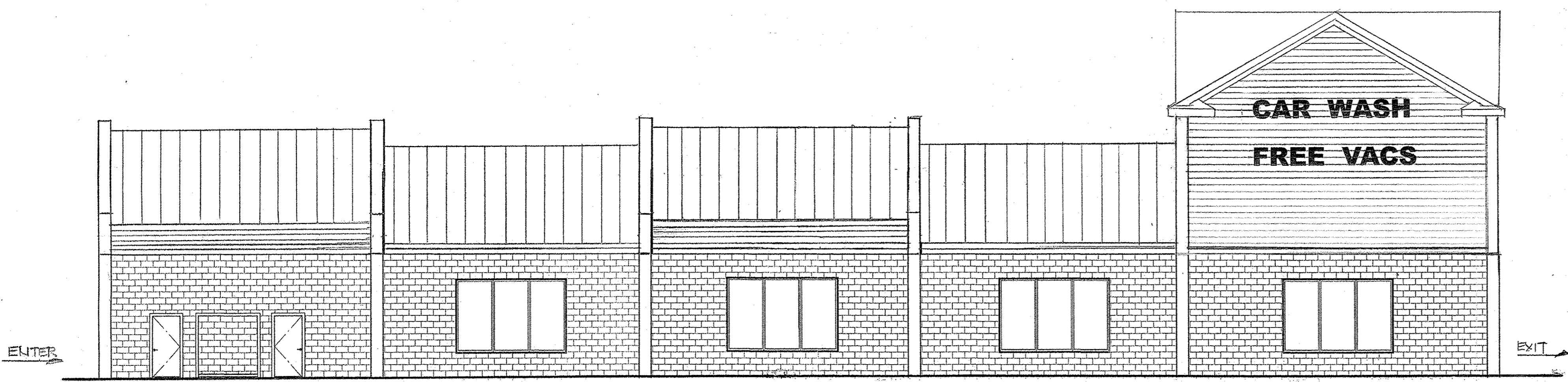
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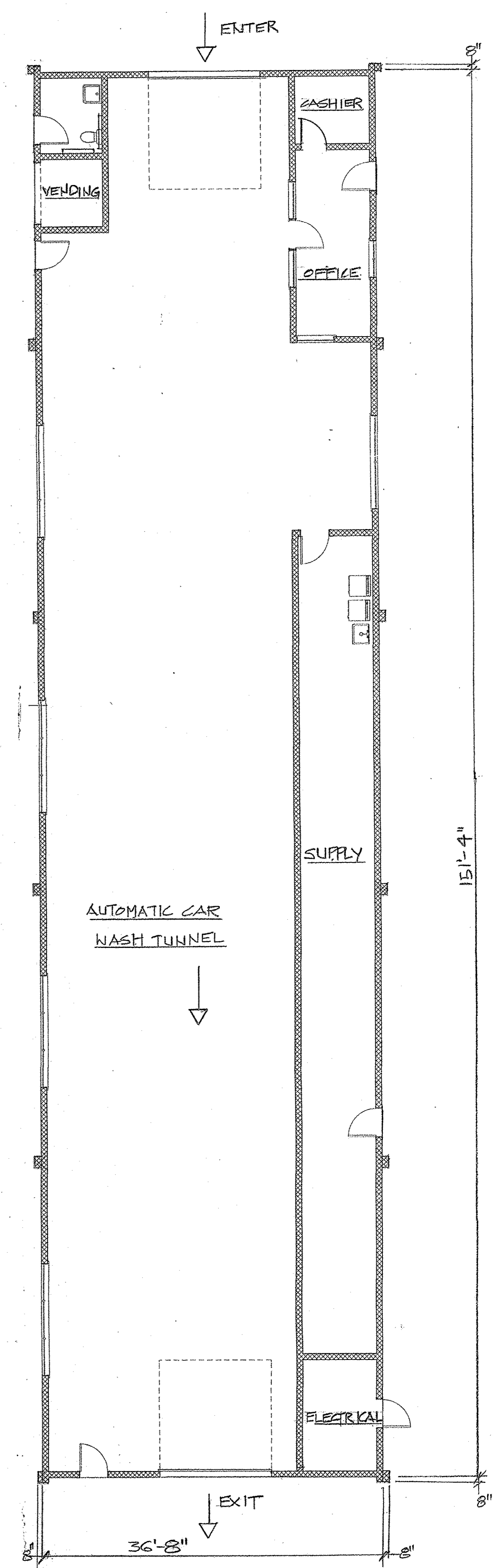
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SOUTH



FLOOR PLAN

DATE: 7-6-2023 REVISIONS: 7-11-23

PROPOSED CAR WASH  
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 WILSON CENTER  
 WINDSOR, CT  
 SCALE: 1/8" = 1'-0"

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