Site Plans

Issued for Site Plan Approval

Date Issued July 28, 2023

Latest Issue July 28, 2023

Wilson Center Mixed-Use Development

29 Windsor Ave Windsor, CT 06095

Owner

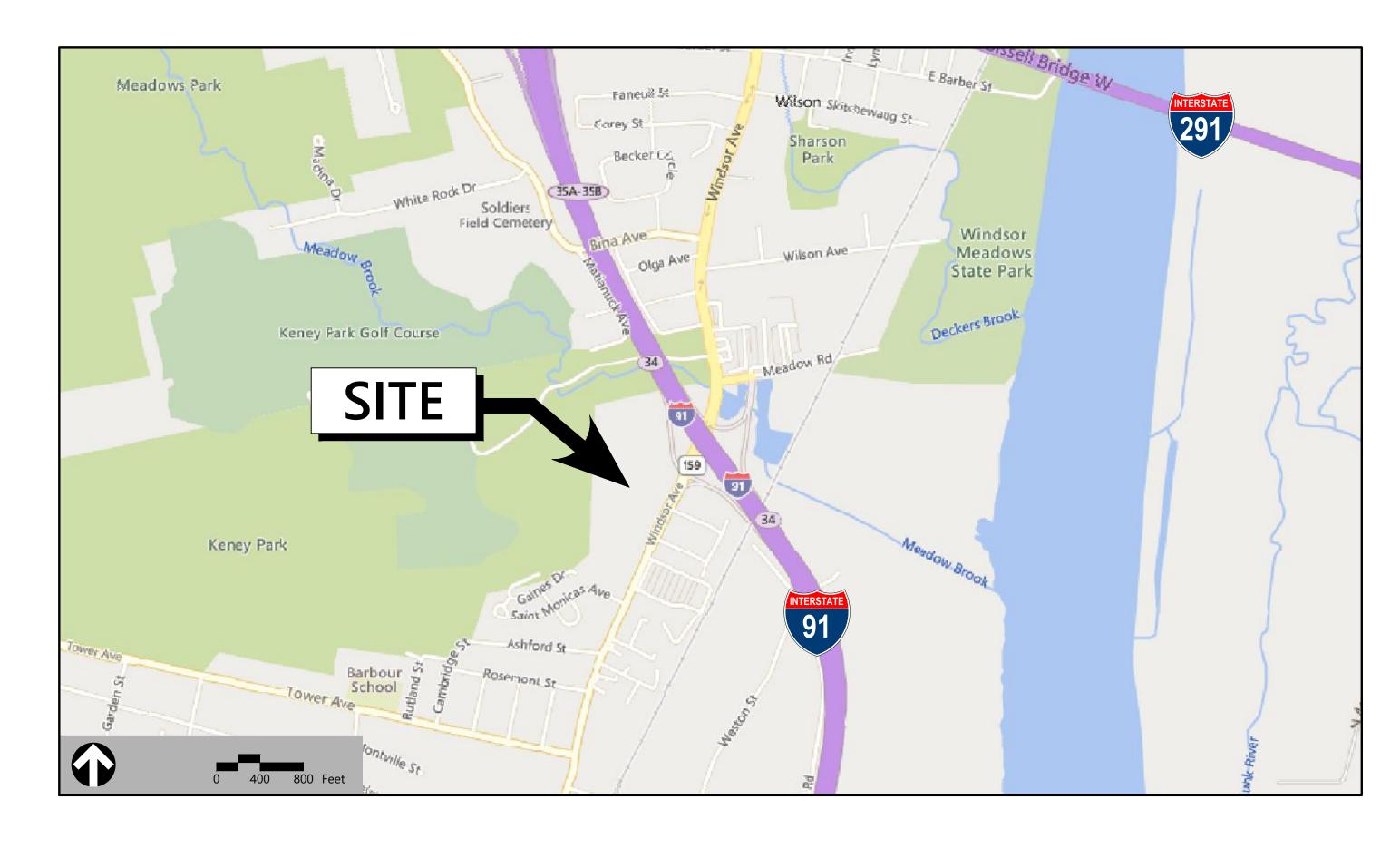
Dijon, LLC 1500 Main Street Suite 255 Springfield, MA 01103

Applicant

Dijon, LLC 1500 Main Street Suite 255 Springfield, MA 01103

Accessor's Map: 81

Block: 9 Lot: 12



Sheet Index			
No.	Drawing Title	Latest Issue	
C-1	Legend, Abbreviations, and General Notes	July 28, 2023	
C-2	Demolition Plan	July 28, 2023	
C-3	Overall Site Plan	July 28, 2023	
C-4	Layout and Materials Plan	July 28, 2023	
C-5	Grading and Drainage Plan	July 28, 2023	
C-6	Utility Plan	July 28, 2023	
C-7	Erosion and Sediment Control Plan	July 28, 2023	
C-8	Details	July 28, 2023	
C-9	Details	July 28, 2023	
C-10	Details	July 28, 2023	
C-11	Details	July 28, 2023	
C-12	Details	July 28, 2023	
C-13	Fire Truck Route	July 28, 2023	

Reference Drawings			
No.	Drawing Title	Latest Issu	
VB101	ALTA/NSPS Land Title Survey	December 6, 20	
VB102	ALTA/NSPS Land Title Survey	December 6, 20	
SL-2	Site Lighting Photometric Calculation	June 5, 20	
L-401	Planting Plan	July 28, 20	
L-402	Planting Details	July 28, 20	
A-1	Proposed Convenience Store	July 6, 20	
A-1	Proposed Car Wash	July 11, 20	





Legend	
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Exist.	Prop.		Exist.	Prop.	
		PROPERTY LINE	4 - 4 - 4 - 4 - 4 - 4 - 4 - 4 - 4 - 4 -		CONCRETE
			(4.5 W. 5 W. 1 V.)	*,5_M_4******	HEAVY DUTY PAVEMENT
		PROJECT LIMIT LINE			BUILDINGS
		RIGHT-OF-WAY/PROPERTY LINE			
		EASEMENT			RIPRAP
		BUILDING SETBACK		1/2/2/2 2/2/2/2	CONSTRUCTION EXIT
10+00	10 + 00	PARKING SETBACK	27.35 TC×	27.35 TC×	TOP OF CURB ELEVATION
10+00	10+00	BASELINE	26.85 BC×	26.85 BC×	
		CONSTRUCTION LAYOUT	20.03 BCX		BOTTOM OF CURB ELEVATION
		ZONING LINE	132.75 ×	132.75 ×	SPOT ELEVATION
		TOWN LINE	45.0 TW × 38.5 BW	45.0 TW 38.5 BW	TOP & BOTTOM OF WALL ELEVATIO
			-	◆	BORING LOCATION
		LIMIT OF DISTURBANCE			TEST PIT LOCATION
<u> </u>	-	WETLAND LINE WITH FLAG	○ MW	→ ^{MW}	MONITORING WELL
		FLOODPLAIN			
		BORDERING LAND SUBJECT	——UD——	——UD——	UNDERDRAIN
BLSF-		TO FLOODING	12"D	12"D»	DRAIN
BZ		WETLAND BUFFER ZONE	6"RD	6"RD»	ROOF DRAIN
NDZ-		NO DISTURB ZONE	1 <u>2</u> "S	1 <u>2"</u> S	SEWER
200'RA		2001 DIVEREDONIT AREA	FM	<u>FM</u>	FORCE MAIN
200 KA		200' RIVERFRONT AREA	- OHW	—— OHW ——	OVERHEAD WIRE
		GRAVEL ROAD	6"W	6"W	
EOP	EOP	EDGE OF PAVEMENT			WATER
BB	BB	BITUMINOUS BERM	4"FP	4"FP	FIRE PROTECTION
BC	BC			——2"DW——	DOMESTIC WATER
		BITUMINOUS CURB	3"G	——G——	GAS
CC	<u>CC</u>	CONCRETE CURB	——Е——	——Е——	ELECTRIC
	CG	CURB AND GUTTER	STM	STM	STEAM
CC	ECC	EXTRUDED CONCRETE CURB	——т—	—_т—	TELEPHONE
CC	MCC	MONOLITHIC CONCRETE CURB	——FA——	——FA——	FIRE ALARM
CC	PCC_	PRECAST CONC. CURB		—— CATV——	CABLE TV
SGE	SGE	SLOPED GRAN. EDGING			CABLLIA
VGC	VGC				CATCH BASIN CONCENTRIC
	->	VERT. GRAN. CURB			CATCH BASIN ECCENTRIC
		LIMIT OF CURB TYPE			DOUBLE CATCH BASIN CONCENTRI
		SAWCUT			DOUBLE CATCH BASIN ECCENTRIC
<i>V.</i>					
(1/////		BUILDING		=	GUTTER INLET
](] ⊲EN	BUILDING ENTRANCE	0	•	DRAIN MANHOLE CONCENTRIC
		LOADING DOCK	(D)		DRAIN MANHOLE ECCENTRIC
•		BOLLARD	=TD=		TRENCH DRAIN
D	D	DUMPSTER PAD	Ľ	r	PLUG OR CAP
_	[b]	SIGN	CO	co •	CLEANOUT
-	•		>	>	FLARED END SECTION
<u>-0</u>		DOUBLE SIGN		\checkmark	HEADWALL
		CTEEL CHARDRAII	-		
		STEEL GUARDRAIL	(\$)	lefton	SEWER MANHOLE CONCENTRIC
		WOOD GUARDRAIL	(\$)	lacksquare	SEWER MANHOLE ECCENTRIC
			 CS	CS ●	
	====	PATH	W∨ specific black;		

General	
ABAN	ABANDON
ACR	ACCESSIBLE CURB RAMP
ADJ	ADJUST
APPROX	APPROXIMATE
BIT	BITUMINOUS
3S	BOTTOM OF SLOPE
BWLL	BROKEN WHITE LANE LINE
CONC	CONCRETE
DYCL	DOUBLE YELLOW CENTER LINE
EL	ELEVATION
ELEV	ELEVATION
ELEV	EXISTING
FDN	FOUNDATION
FFE	FIRST FLOOR ELEVATION
GRAN	GRANITE
GTD	GRADE TO DRAIN
_A	LANDSCAPE AREA
LOD	LIMIT OF DISTURBANCE
MAX	MAXIMUM
MIN	MINIMUM
NIC	NOT IN CONTRACT
NTS	NOT TO SCALE
PERF	PERFORATED
PROP	PROPOSED
REM	REMOVE
RET	RETAIN
R&D	REMOVE AND DISPOSE
R&R	REMOVE AND RESET
SWEL	SOLID WHITE EDGE LINE
SWLL	SOLID WHITE LANE LINE
TS	TOP OF SLOPE
TYP	TYPICAL
Utility	
CB	CATCH BASIN
CMP	CORRUGATED METAL PIPE
CIVII	CONNOGATED WETALT II L
\mathcal{C}	CLEANOLIT
	CLEANOUT
DCB	DOUBLE CATCH BASIN
DCB DMH	DOUBLE CATCH BASIN DRAIN MANHOLE
DCB DMH CIP	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE
DCB DMH CIP COND	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT
DCB DMH CIP COND DIP	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE
DCB DMH CIP COND DIP FES	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION
DCB DMH CIP COND DIP FES FM	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN
DCB DMH CIP COND DIP FES FM	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION
DCB DMH CIP COND DIP ES M =	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN
DCB DMH CIP COND DIP FES FM F&G F&C	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE
DCB DMH CIP COND DIP FES FM F&G F&C	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER
DCB DMH CIP COND DIP FES FM F&G F&C GI	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET
DCB DMH CIP COND DIP FES FM F&G GI GT HDPE	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP
DCB DMH CIP COND DIP FES FM F&G GI GT HDPE	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE
DCB DMH CIP COND DIP FES FM F&G GI GT HDPE HH	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE
DCB DMH CIP COND DIP FES FM F&G GI GT HDPE HH HW HYD	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE HEADWALL
DCB DMH CIP COND DIP FES FM F&G GI GT HDPE HH HW HYD	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE HEADWALL HYDRANT
DCB DMH CIP COND DIP FES FM F&G GI GT HDPE HH HW HYD INV	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE HEADWALL HYDRANT INVERT ELEVATION
DCB DMH CIP COND DIP FES FM F&G GI GT HDPE HH HW HYD INV I= LP	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE HEADWALL HYDRANT INVERT ELEVATION
DCB DMH CIP COND DIP FES FM F&G GI GT HDPE HH HW HYD INV I= LP MES	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE HEADWALL HYDRANT INVERT ELEVATION LIGHT POLE
DCB DMH CIP COND DIP FES FM F&G GI GT HDPE HH HW HYD INV I= LP MES PIV	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE HEADWALL HYDRANT INVERT ELEVATION LIGHT POLE METAL END SECTION
DCB DMH CIP COND DIP FES FM F&G GI GT HDPE HH HW HYD INV I= LP MES PIV PWW	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE HEADWALL HYDRANT INVERT ELEVATION LIGHT POLE METAL END SECTION POST INDICATOR VALVE
DCB DMH CIP COND DIP FES FM F&G F&C GI GT HDPE HH HW HYD INV I= LP MES PIV PWW PVC	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE HEADWALL HYDRANT INVERT ELEVATION LIGHT POLE METAL END SECTION POST INDICATOR VALVE PAVED WATER WAY POLYVINYLCHLORIDE PIPE
DCB DMH CIP COND DIP FES FM F&G F&C GI GT HDPE HH HW HYD INV I= LP MES PIV PWW PVC RCP	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE HEADWALL HYDRANT INVERT ELEVATION LIGHT POLE METAL END SECTION POST INDICATOR VALVE PAVED WATER WAY POLYVINYLCHLORIDE PIPE REINFORCED CONCRETE PIPE
DCB DMH CIP COND DIP FES FM F&G F&C GI GT HDPE HH HW HYD INV I= LP MES PIV PWW PVC RCP R=	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE HEADWALL HYDRANT INVERT ELEVATION LIGHT POLE METAL END SECTION POST INDICATOR VALVE PAVED WATER WAY POLYVINYLCHLORIDE PIPE REINFORCED CONCRETE PIPE RIM ELEVATION
GT HDPE HH HW HYD INV I= LP MES PIV PWW PVC RCP R= RIM=	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE HEADWALL HYDRANT INVERT ELEVATION LIGHT POLE METAL END SECTION POST INDICATOR VALVE PAVED WATER WAY POLYVINYLCHLORIDE PIPE RIM ELEVATION RIM ELEVATION RIM ELEVATION RIM ELEVATION
DCB DMH CIP COND DIP FES FM F&G GT HDPE HH HW HYD INV I= LP MES PIV PWW PVC RCP R= RIM= SMH	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE HEADWALL HYDRANT INVERT ELEVATION INVERT ELEVATION LIGHT POLE METAL END SECTION POST INDICATOR VALVE PAVED WATER WAY POLYVINYLCHLORIDE PIPE REINFORCED CONCRETE PIPE RIM ELEVATION RIM ELEVATION SEWER MANHOLE
DCB DMH CIP COND DIP FES FM F&G GT HDPE HH HW HYD INV I= LP MES PIV PWW PVC RCP R= RIM= SMH SWQU	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE HEADWALL HYDRANT INVERT ELEVATION INVERT ELEVATION LIGHT POLE METAL END SECTION POST INDICATOR VALVE PAVED WATER WAY POLYVINYLCHLORIDE PIPE REINFORCED CONCRETE PIPE RIM ELEVATION RIM ELEVATION SEWER MANHOLE STORMCEPTOR WATER QUALITY UNIT
DCB DMH CIP COND DIP ESS EM ESC GI GT HDPE HH HW HYD NV = LP MES PIV PVC RCP RS EMM ESMQU TSV	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE HEADWALL HYDRANT INVERT ELEVATION INVERT ELEVATION LIGHT POLE METAL END SECTION POST INDICATOR VALVE PAVED WATER WAY POLYVINYLCHLORIDE PIPE REINFORCED CONCRETE PIPE RIM ELEVATION SEWER MANHOLE STORMCEPTOR WATER QUALITY UNIT TAPPING SLEEVE, VALVE AND BOX
DCB DMH CIP COND DIP ES M &G &C GI GT HDPE HH HW HYD NV = P MES PIV PWW PVC &CP &= RIM= SMH SWQU TSV	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE HEADWALL HYDRANT INVERT ELEVATION INVERT ELEVATION LIGHT POLE METAL END SECTION POST INDICATOR VALVE PAVED WATER WAY POLYVINYLCHLORIDE PIPE REINFORCED CONCRETE PIPE RIM ELEVATION SEWER MANHOLE STORMCEPTOR WATER QUALITY UNIT TAPPING SLEEVE, VALVE AND BOX UNDERGROUND
DCB DMH CIP COND DIP FES FM F&G GT HDPE HH HW HYD INV I= LP MES PIV PWW PVC RCP R= RIM= SMH	DOUBLE CATCH BASIN DRAIN MANHOLE CAST IRON PIPE CONDUIT DUCTILE IRON PIPE FLARED END SECTION FORCE MAIN FRAME AND GRATE FRAME AND COVER GUTTER INLET GREASE TRAP HIGH DENSITY POLYETHYLENE PIPE HANDHOLE HEADWALL HYDRANT INVERT ELEVATION INVERT ELEVATION LIGHT POLE METAL END SECTION POST INDICATOR VALVE PAVED WATER WAY POLYVINYLCHLORIDE PIPE REINFORCED CONCRETE PIPE RIM ELEVATION RIM ELEVATION SEWER MANHOLE STORMCEPTOR WATER QUALITY UNIT TAPPING SLEEVE, VALVE AND BOX

Notes

General

- CONTRACTOR SHALL NOTIFY "CALL BEFORE YOU DIG" (1-800-922-4455) AT LEAST 72 HOURS BEFORE EXCAVATING.
- 2. CONTRACTOR SHALL BE RESPONSIBLE FOR SITE SECURITY AND JOB SAFETY. CONSTRUCTION ACTIVITIES
- SHALL BE IN ACCORDANCE WITH OSHA STANDARDS AND LOCAL REQUIREMENTS.
- 3. ACCESSIBLE ROUTES, PARKING SPACES, RAMPS, SIDEWALKS AND WALKWAYS SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE FEDERAL AMERICANS WITH DISABILITIES ACT AND WITH STATE AND LOCAL LAWS AND REGULATIONS (WHICHEVER ARE MORE STRINGENT).
- 4. AREAS DISTURBED DURING CONSTRUCTION AND NOT RESTORED WITH IMPERVIOUS SURFACES (BUILDINGS, PAVEMENTS, WALKS, ETC.) SHALL RECEIVE 4 INCHES LOAM AND SEED.
- 5. WITHIN THE LIMITS OF THE BUILDING FOOTPRINT, THE SITE CONTRACTOR SHALL PERFORM EARTHWORK OPERATIONS REQUIRED UP TO SUBGRADE ELEVATIONS.
- 6. WORK WITHIN THE LOCAL RIGHTS-OF-WAY SHALL CONFORM TO LOCAL MUNICIPAL STANDARDS. WORK WITHIN STATE RIGHTS-OF-WAY SHALL CONFORM TO THE LATEST EDITION OF THE STATE HIGHWAY DEPARTMENTS STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES.
- 7. UPON AWARD OF CONTRACT, CONTRACTOR SHALL MAKE NECESSARY CONSTRUCTION NOTIFICATIONS AND APPLY FOR AND OBTAIN NECESSARY PERMITS, PAY FEES, AND POST BONDS ASSOCIATED WITH THE WORK INDICATED ON THE DRAWINGS, IN THE SPECIFICATIONS, AND IN THE CONTRACT DOCUMENTS. DO NOT CLOSE OR OBSTRUCT ROADWAYS, SIDEWALKS, AND FIRE HYDRANTS, WITHOUT APPROPRIATE PERMITS.
- TRAFFIC SIGNAGE AND PAVEMENT MARKINGS SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- AREAS OUTSIDE THE LIMITS OF PROPOSED WORK DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY THE CONTRACTOR TO THEIR ORIGINAL CONDITION AT THE CONTRACTOR'S
- 10. IN THE EVENT THAT SUSPECTED CONTAMINATED SOIL, GROUNDWATER, AND OTHER MEDIA ARE ENCOUNTERED DURING EXCAVATION AND CONSTRUCTION ACTIVITIES BASED ON VISUAL, OLFACTORY, OR OTHER EVIDENCE, THE CONTRACTOR SHALL STOP WORK IN THE VICINITY OF THE SUSPECT MATERIAL TO AVOID FURTHER SPREADING OF THE MATERIAL, AND SHALL NOTIFY THE OWNER IMMEDIATELY SO THAT THE APPROPRIATE TESTING AND SUBSEQUENT ACTION CAN BE TAKEN.
- 11. CONTRACTOR SHALL PREVENT DUST, SEDIMENT, AND DEBRIS FROM EXITING THE SITE AND SHALL BE RESPONSIBLE FOR CLEANUP, REPAIRS AND CORRECTIVE ACTION IF SUCH OCCURS.
- 12. DAMAGE RESULTING FROM CONSTRUCTION LOADS SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO OWNER.
- 13. CONTRACTOR SHALL CONTROL STORMWATER RUNOFF DURING CONSTRUCTION TO PREVENT ADVERSE IMPACTS TO OFF SITE AREAS, AND SHALL BE RESPONSIBLE TO REPAIR RESULTING DAMAGES, IF ANY, AT NO COST TO OWNER.

Utilities

- 1. THE LOCATIONS, SIZES, AND TYPES OF EXISTING UTILITIES ARE SHOWN AS AN APPROXIMATE REPRESENTATION ONLY. THE OWNER OR ITS REPRESENTATIVE(S) HAVE NOT INDEPENDENTLY VERIFIED THIS INFORMATION AS SHOWN ON THE PLANS. THE UTILITY INFORMATION SHOWN DOES NOT GUARANTEE THE ACTUAL EXISTENCE, SERVICEABILITY, OR OTHER DATA CONCERNING THE UTILITIES, NOR DOES IT GUARANTEE AGAINST THE POSSIBILITY THAT ADDITIONAL UTILITIES MAY BE PRESENT THAT ARE NOT SHOWN ON THE PLANS. PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL VERIFY AND DETERMINE THE EXACT LOCATIONS, SIZES, AND ELEVATIONS OF THE POINTS OF CONNECTIONS TO EXISTING UTILITIES AND, SHALL CONFIRM THAT THERE ARE NO INTERFERENCES WITH EXISTING UTILITIES AND THE PROPOSED UTILITY ROUTES, INCLUDING ROUTES WITHIN THE PUBLIC RIGHTS OF WAY.
- 2. WHERE AN EXISTING UTILITY IS FOUND TO CONFLICT WITH THE PROPOSED WORK, OR EXISTING CONDITIONS DIFFER FROM THOSE SHOWN SUCH THAT THE WORK CANNOT BE COMPLETED AS INTENDED, THE LOCATION, ELEVATION, AND SIZE OF THE UTILITY SHALL BE ACCURATELY DETERMINED WITHOUT DELAY BY THE CONTRACTOR, AND THE INFORMATION FURNISHED IN WRITING TO THE OWNER'S REPRESENTATIVE FOR THE RESOLUTION OF THE CONFLICT AND CONTRACTOR'S FAILURE TO NOTIFY PRIOR TO PERFORMING ADDITIONAL WORK RELEASES OWNER FROM OBLIGATIONS FOR ADDITIONAL PAYMENTS WHICH OTHERWISE MAY BE WARRANTED TO RESOLVE THE CONFLICT.
- 3. SET CATCH BASIN RIMS, AND INVERTS OF SEWERS, DRAINS, AND DITCHES IN ACCORDANCE WITH ELEVATIONS ON THE GRADING AND UTILITY PLANS.
- 4. RIM ELEVATIONS FOR DRAIN AND SEWER MANHOLES, WATER VALVE COVERS, GAS GATES, ELECTRIC AND TELEPHONE PULL BOXES, AND MANHOLES, AND OTHER SUCH ITEMS, ARE APPROXIMATE AND SHALL BE SET/RESET AS FOLLOWS:
 - A. PAVEMENTS AND CONCRETE SURFACES: FLUSH
 - B. ALL SURFACES ALONG ACCESSIBLE ROUTES: FLUSH
 - C. LANDSCAPE, LOAM AND SEED, AND OTHER EARTH SURFACE AREAS: ONE INCH ABOVE SURROUNDING AREA AND TAPER EARTH TO THE RIM ELEVATION.
- 5. THE LOCATION, SIZE, DEPTH, AND SPECIFICATIONS FOR CONSTRUCTION OF PROPOSED PRIVATE UTILITY SERVICES SHALL BE INSTALLED ACCORDING TO THE REQUIREMENTS PROVIDED BY, AND APPROVED BY, THE RESPECTIVE UTILITY COMPANY (GAS, TELEPHONE, ELECTRIC, FIRE ALARM, ETC.). FINAL DESIGN LOADS AND LOCATIONS TO BE COORDINATED WITH OWNER AND ARCHITECT.
- 6. CONTRACTOR SHALL MAKE ARRANGEMENTS FOR AND SHALL BE RESPONSIBLE FOR PAYING FEES FOR POLE RELOCATION AND FOR THE ALTERATION AND ADJUSTMENT OF GAS, ELECTRIC, TELEPHONE, FIRE ALARM, AND ANY OTHER PRIVATE UTILITIES, WHETHER WORK IS PERFORMED BY CONTRACTOR OR BY THE UTILITIES COMPANY.
- 7. UTILITY PIPE MATERIALS SHALL BE AS FOLLOWS, UNLESS OTHERWISE NOTED ON THE PLAN:
 - A. WATER PIPES SHALL BE CEMENT LINED DUCTILE IRON (CLDI) FOR FIRE PROTECTION AND TYPE 'K' COPPER FOR DOMESTIC SERVICES
 - B. SANITARY SEWER PIPES SHALL BE POLYVINYL CHLORIDE (PVC) SEWER PIPE
- C. STORM DRAINAGE PIPES SHALL BE HIGH DENSITY POLYETHYLENE (HDPE)
- 8. CONTRACTOR SHALL COORDINATE WITH ELECTRICAL CONTRACTOR AND SHALL FURNISH EXCAVATION, INSTALLATION, AND BACKFILL OF ELECTRICAL FURNISHED SITEWORK RELATED ITEMS SUCH AS PULL BOXES, CONDUITS, DUCT BANKS, LIGHT POLE BASES, AND CONCRETE PADS. SITE CONTRACTOR SHALL FURNISH CONCRETE ENCASEMENT OF DUCT BANKS IF REQUIRED BY THE UTILITY COMPANY AND AS INDICATED ON THE DRAWINGS.
- CONTRACTOR SHALL EXCAVATE AND BACKFILL TRENCHES FOR GAS IN ACCORDANCE WITH GAS COMPANY'S REQUIREMENTS.
- 10. ALL DRAINAGE AND SANITARY STRUCTURE INTERIOR DIAMETERS (4' MIN.) SHALL BE DETERMINED BY THE MANUFACTURER BASED ON THE PIPE CONFIGURATIONS SHOWN ON THESE PLANS AND LOCAL MUNICIPAL STANDARDS. FOR MANHOLES THAT ARE 20 FEET IN DEPTH AND GREATER, THE MINIMUM DIAMETER SHALL BE 5 FEET.

Layout and Materials

- 1. DIMENSIONS ARE FROM THE FACE OF CURB, FACE OF BUILDING, FACE OF WALL, AND CENTER LINE OF PAVEMENT MARKINGS, UNLESS OTHERWISE NOTED.
- 2. CURB RADII ARE 3 FEET UNLESS OTHERWISE NOTED.
- 3. CURBING SHALL BE PRECAST CONCRETE CURB (PCC) WITHIN THE SITE UNLESS OTHERWISE INDICATED
- 4. SEE ARCHITECTURAL DRAWINGS FOR EXACT BUILDING DIMENSIONS AND DETAILS CONTIGUOUS TO THE BUILDING, INCLUDING SIDEWALKS, RAMPS, BUILDING ENTRANCES, STAIRWAYS, UTILITY PENETRATIONS, CONCRETE DOOR PADS, COMPACTOR PAD, LOADING DOCKS, BOLLARDS, ETC.
- 5. PROPOSED BOUNDS AND ANY EXISTING PROPERTY LINE MONUMENTATION DISTURBED DURING CONSTRUCTION SHALL BE SET OR RESET BY A PROFESSIONAL LAND SURVEYOR.
- 6. PRIOR TO START OF CONSTRUCTION, CONTRACTOR SHALL VERIFY EXISTING PAVEMENT ELEVATIONS AT INTERFACE WITH PROPOSED PAVEMENTS, AND EXISTING GROUND ELEVATIONS ADJACENT TO DRAINAGE OUTLETS TO ASSURE PROPER TRANSITIONS BETWEEN EXISTING AND PROPOSED FACILITIES.

Demolition

- 1. CONTRACTOR SHALL REMOVE AND DISPOSE OF EXISTING MANMADE SURFACE FEATURES WITHIN THE LIMIT OF WORK INCLUDING BUILDINGS, STRUCTURES, PAVEMENTS, SLABS, CURBING, FENCES, UTILITY POLES, SIGNS, ETC. UNLESS INDICATED OTHERWISE ON THE DRAWINGS. REMOVE AND DISPOSE OF EXISTING UTILITIES, FOUNDATIONS AND UNSUITABLE MATERIAL BENEATH AND FOR A DISTANCE OF 10 FEET BEYOND THE PROPOSED BUILDING FOOTPRINT INCLUDING EXTERIOR COLUMNS.
- 2. EXISTING UTILITIES SHALL BE TERMINATED, UNLESS OTHERWISE NOTED, IN CONFORMANCE WITH LOCAL, STATE AND INDIVIDUAL UTILITY COMPANY STANDARD SPECIFICATIONS AND DETAILS. THE CONTRACTOR SHALL COORDINATE UTILITY SERVICE DISCONNECTS WITH THE UTILITY DEDDES ON TATIVES.
- 3. CONTRACTOR SHALL DISPOSE OF DEMOLITION DEBRIS IN ACCORDANCE WITH APPLICABLE FEDERAL, STATE AND LOCAL REGULATIONS, ORDINANCES AND STATUTES.
- THE DEMOLITION LIMITS DEPICTED IN THE PLANS IS INTENDED TO AID THE CONTRACTOR DURING THE BIDDING AND CONSTRUCTION PROCESS AND IS NOT INTENDED TO DEPICT EACH AND EVERY ELEMENT OF DEMOLITION. THE CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING THE DETAILED SCOPE OF DEMOLITION BEFORE SUBMITTING ITS BID/PROPOSAL TO PERFORM THE WORK AND SHALL MAKE NO CLAIMS AND SEEK NO ADDITIONAL COMPENSATION FOR CHANGED CONDITIONS OR UNFORESEEN OR LATENT SITE CONDITIONS RELATED TO ANY CONDITIONS DISCOVERED DURING EXECUTION OF THE WORK
- 5. UNLESS OTHERWISE SPECIFICALLY PROVIDED ON THE PLANS OR IN THE SPECIFICATIONS, THE ENGINEER HAS NOT PREPARED DESIGNS FOR AND SHALL HAVE NO RESPONSIBILITY FOR THE PRESENCE, DISCOVERY, REMOVAL, ABATEMENT OR DISPOSAL OF HAZARDOUS MATERIALS, TOXIC WASTES OR POLLUTANTS AT THE PROJECT SITE. THE ENGINEER SHALL NOT BE RESPONSIBLE FOR ANY CLAIMS OF LOSS, DAMAGE, EXPENSE, DELAY, INJURY OR DEATH ARISING FROM THE PRESENCE OF HAZARDOUS MATERIAL AND CONTRACTOR SHALL INDEMNIFY AND HOLD HARMLESS THE ENGINEER FROM ANY CLAIMS MADE IN CONNECTION THEREWITH. MOREOVER, THE ENGINEER SHALL HAVE NO ADMINISTRATIVE OBLIGATIONS OF ANY TYPE WITH REGARD TO ANY CONTRACTOR AMENDMENT INVOLVING THE ISSUES OF PRESENCE, DISCOVERY, REMOVAL, ABATEMENT OR DISPOSAL OF ASBESTOS OR OTHER HAZARDOUS MATERIALS.

Erosion Control

- PRIOR TO STARTING ANY OTHER WORK ON THE SITE, THE CONTRACTOR SHALL NOTIFY APPROPRIATE AGENCIES AND SHALL INSTALL EROSION CONTROL MEASURES AS SHOWN ON THE PLANS AND AS IDENTIFIED IN FEDERAL, STATE, AND LOCAL APPROVAL DOCUMENTS PERTAINING TO THIS PROJECT.
- 2. CONTRACTOR SHALL INSPECT AND MAINTAIN EROSION CONTROL MEASURES ON A WEEKLY BASIS (MINIMUM) OR AS REQUIRED PER THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP). THE CONTRACTOR SHALL ADDRESS DEFICIENCIES AND MAINTENANCE ITEMS WITHIN TWENTY-FOUR HOURS OF INSPECTION. CONTRACTOR SHALL PROPERLY DISPOSE OF SEDIMENT SUCH THAT IT DOES NOT ENCUMBER OTHER DRAINAGE STRUCTURES AND PROTECTED AREAS.
- 3. CONTRACTOR SHALL BE FULLY RESPONSIBLE TO CONTROL CONSTRUCTION SUCH THAT SEDIMENTATION SHALL NOT AFFECT REGULATORY PROTECTED AREAS, WHETHER SUCH SEDIMENTATION IS CAUSED BY WATER, WIND, OR DIRECT DEPOSIT.
- 4. CONTRACTOR SHALL PERFORM CONSTRUCTION SEQUENCING SUCH THAT EARTH MATERIALS ARE EXPOSED FOR A MINIMUM OF TIME BEFORE THEY ARE COVERED, SEEDED, OR OTHERWISE STABILIZED TO PREVENT EROSION.
- 5. UPON COMPLETION OF CONSTRUCTION AND ESTABLISHMENT OF PERMANENT GROUND COVER, CONTRACTOR SHALL REMOVE AND DISPOSE OF EROSION CONTROL MEASURES AND CLEAN SEDIMENT AND DEBRIS FROM ENTIRE DRAINAGE AND SEWER SYSTEMS.

Existing Conditions Information

- 1. ALTA/NSPS LAND TITLE SURVEY WAS PREPARED BY LANGAN DATED DECEMBER 6, 2017. A PORTION OF THE OFF-SITE INFORMATION IS NOT SURVEYED.
- 2. A GEOTECHNICAL REPORT WAS PREPARED BY WHITESTONE ASSOCIATES, INC., DATED OCTOBER 17, 2017.

Document Use

- . THESE PLANS AND CORRESPONDING CADD DOCUMENTS ARE INSTRUMENTS OF PROFESSIONAL SERVICE, AND SHALL NOT BE USED, IN WHOLE OR IN PART, FOR ANY PURPOSE OTHER THAN FOR WHICH IT WAS CREATED WITHOUT THE EXPRESSED, WRITTEN CONSENT OF VHB. ANY UNAUTHORIZED USE, REUSE, MODIFICATION OR ALTERATION, INCLUDING AUTOMATED CONVERSION OF THIS DOCUMENT SHALL BE AT THE USER'S SOLE RISK WITHOUT LIABILITY OR LEGAL EXPOSURE TO VHB.
- 2. CONTRACTOR SHALL NOT RELY SOLELY ON ELECTRONIC VERSIONS OF PLANS, SPECIFICATIONS, AND DATA FILES THAT ARE OBTAINED FROM THE DESIGNERS, BUT SHALL VERIFY LOCATION OF PROJECT FEATURES IN ACCORDANCE WITH THE PAPER COPIES OF THE PLANS AND SPECIFICATIONS THAT ARE SUPPLIED AS PART OF THE CONTRACT DOCUMENTS.
- SYMBOLS AND LEGENDS OF PROJECT FEATURES ARE GRAPHIC REPRESENTATIONS AND ARE NOT NECESSARILY SCALED TO THEIR ACTUAL DIMENSIONS OR LOCATIONS ON THE DRAWINGS. THE CONTRACTOR SHALL REFER TO THE DETAIL SHEET DIMENSIONS, MANUFACTURERS' LITERATURE, SHOP DRAWINGS AND FIELD MEASUREMENTS OF SUPPLIED PRODUCTS FOR LAYOUT OF THE PROJECT FEATURES.



Building 103-3N Springfield, MA 01105 413.747.7113

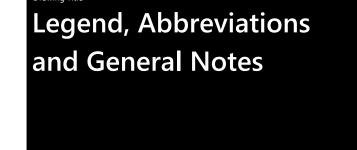
Wilson Center Mixed-Use Development

29 Windsor Avenue Windsor, Connecticut

No.	Revision	Date	Appvd.

Site Plan Approval	July 28, 20
Issued for	Date
Designed by	Checked by

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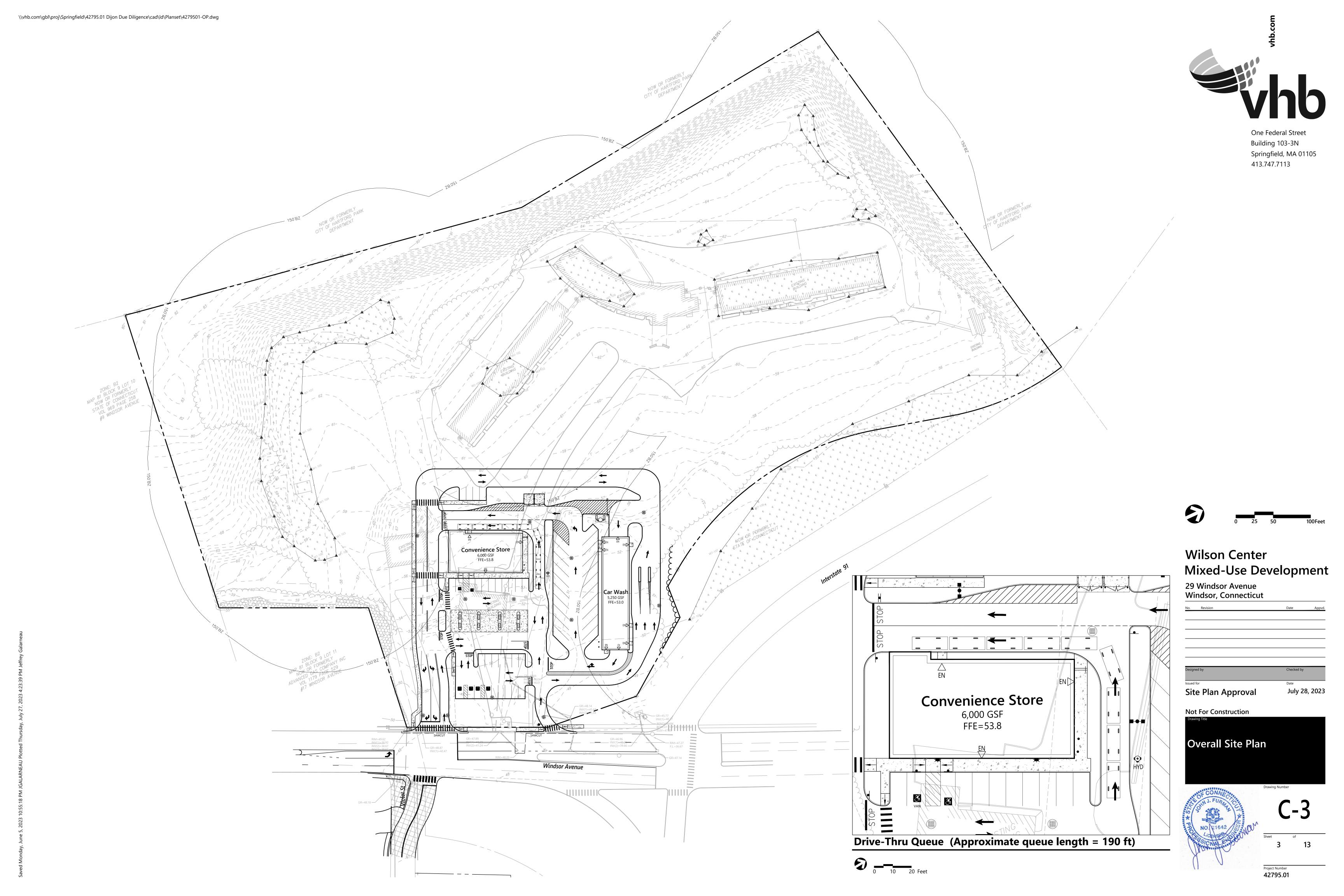


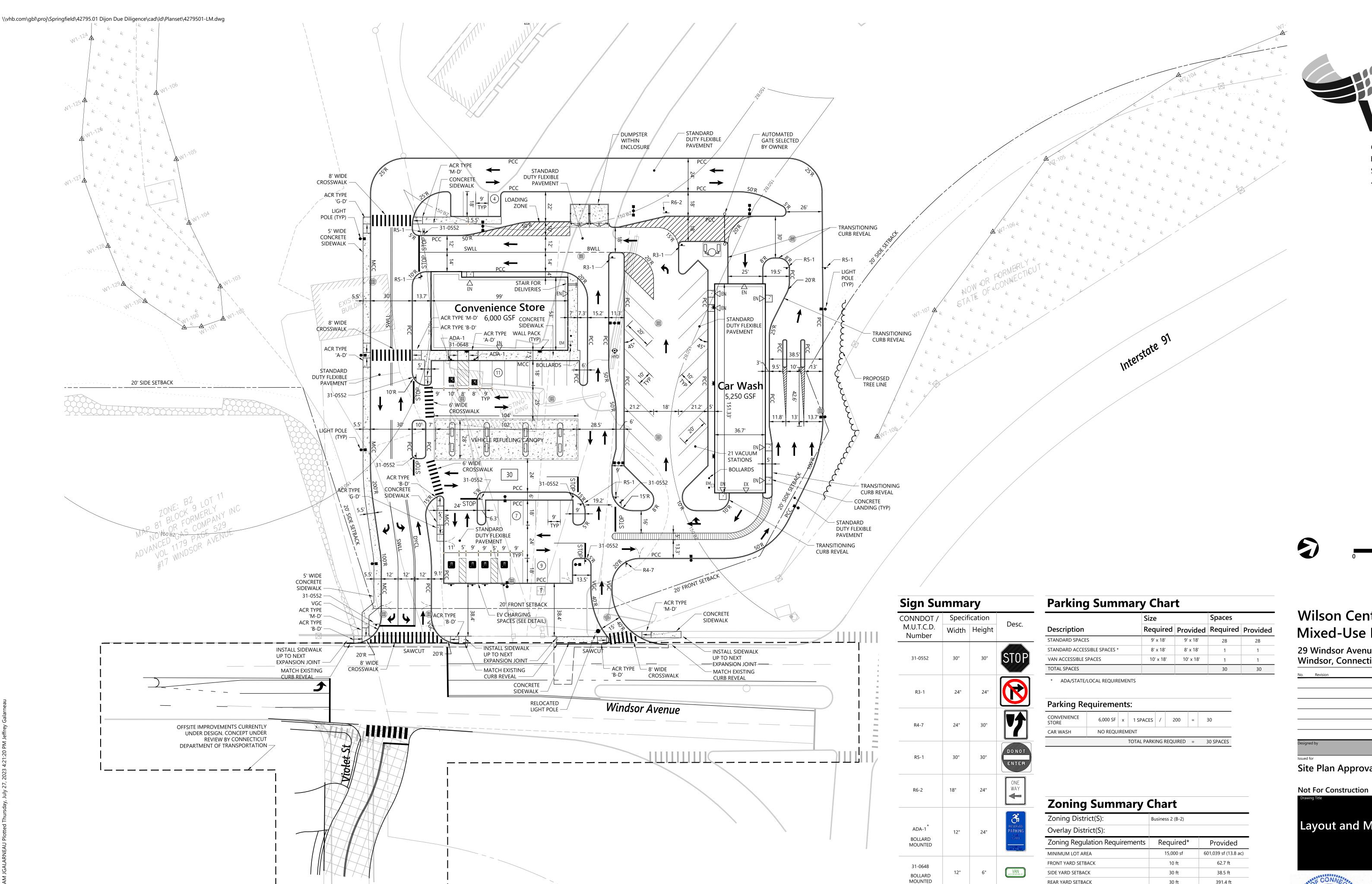
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eet of 1 13







One Federal Street

Springfield, MA 01105

Building 103-3N

413.747.7113

Zoning District(S):	Business 2 (B-2)	
Overlay District(S):		
Zoning Regulation Requirements	Required*	Provided
MINIMUM LOT AREA	15,000 sf	601,039 sf (13.8 ac)
FRONT YARD SETBACK	10 ft	62.7 ft
SIDE YARD SETBACK	30 ft	38.5 ft
REAR YARD SETBACK	30 ft	391.4 ft
MINIMUM LOT WIDTH	75 ft	189 ft
BUILDING AREA	1,500 sf	6,000 sf and 5,250 sf
MAXIMUM BUILDING HEIGHT	3 Stories / 45 ft	1 Story / <45 ft
MAXIMUM BUILDING COVERAGE	33.3%	1.9%

* NEW SIGN NUMBER IS UNKNOWN. THE PREVIOUS NUMBER WAS 31-0629.

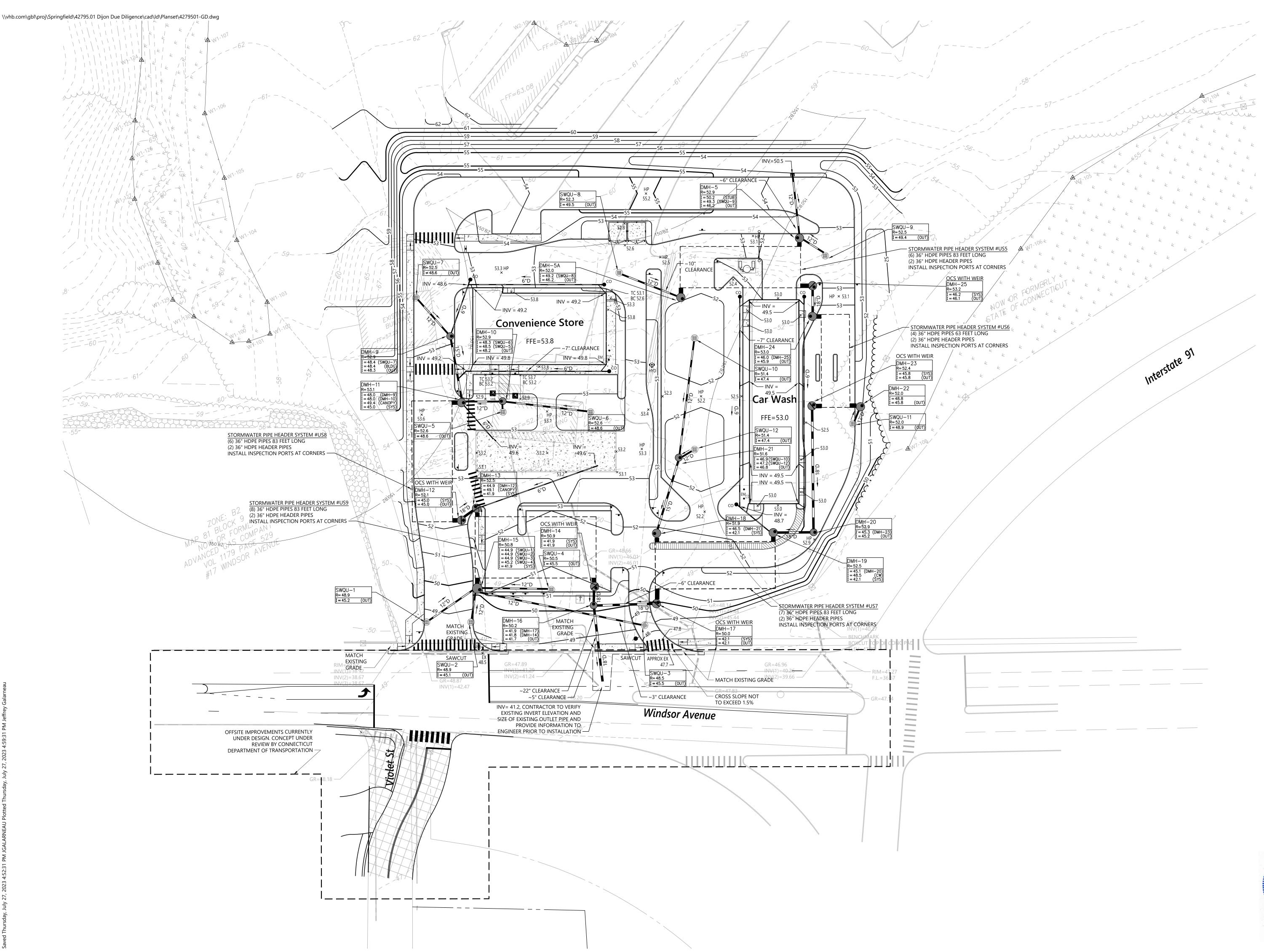
Wilson Center Mixed-Use Development

29 Windsor Avenue Windsor, Connecticut

Site Plan Approval	July 28, 2023
ssued for	Date
	,
Designed by	Checked by

Layout and Materials Plan







One Federal Street Building 103-3N Springfield, MA 01105 413.747.7113

NOTES:

- 1) ALL CATCH BASINS ON SITE ARE TO BE CDS1515-3-C WATER QUALITY UNITS.
- 2) ALL ROOF DRAIN DOWNSPOUT CONNECTIONS SHALL BE 4" PVC. COORDINATE WITH ARCHITECTURAL AND MEP PLANS FOR FINAL LOCATIONS OF DOWNSPOUTS.





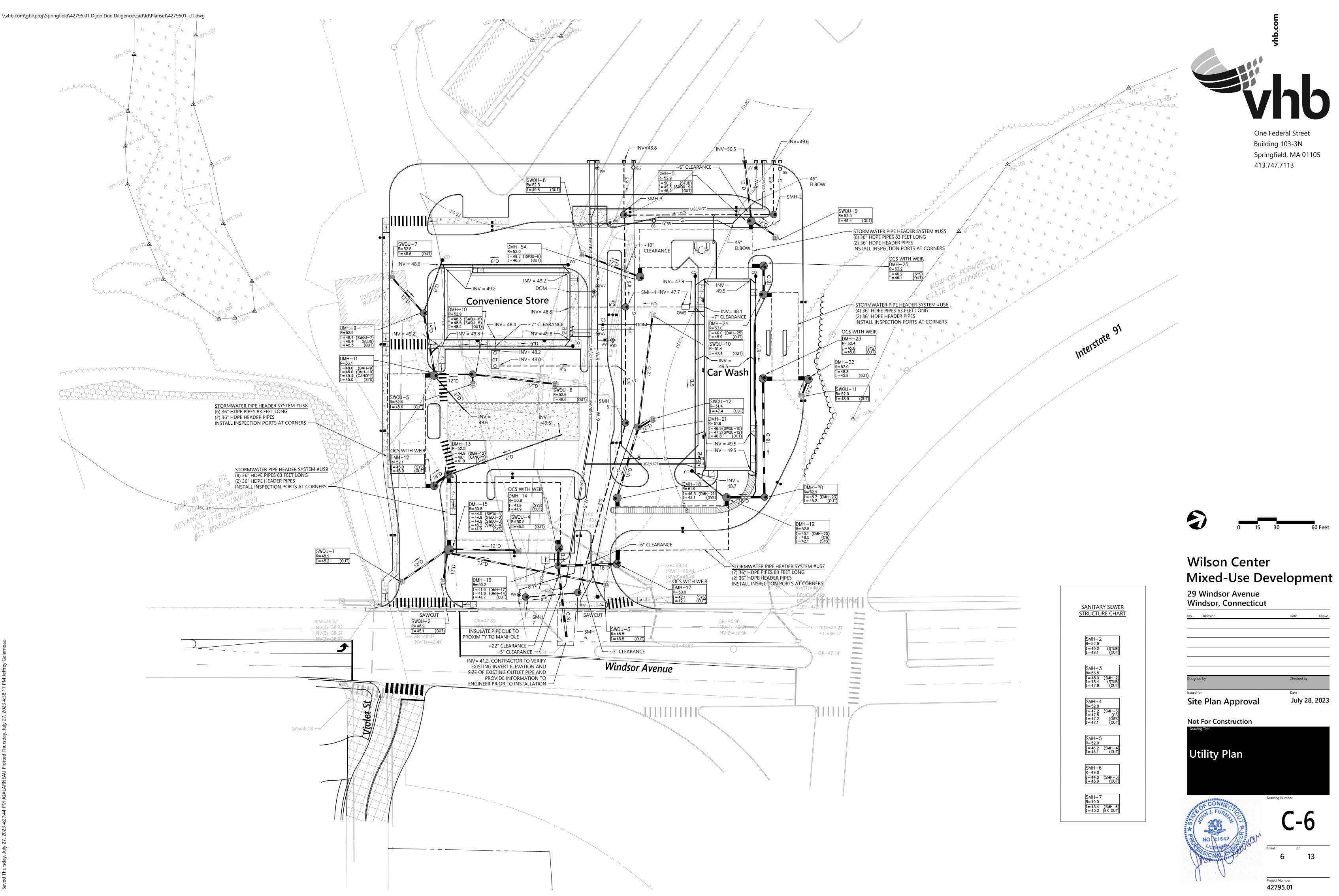
Mixed-Use Development 29 Windsor Avenue Windsor, Connecticut

Designed by	Checked by
Issued for	Date
Site Plan Approval	July 28, 2023

Not For Construction

Grading and Drainage Plan







Springfield, MA 01105

413.747.7113

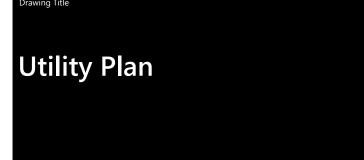




29 Windsor Avenue Windsor, Connecticut

July 28, 2023 Site Plan Approval

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One Federal Street Building 103-3N Springfield, MA 01105 413.747.7113

NOTES:

1) CONTRACTOR TO PERFORM DAILY ENSURE EASTERN BOX TURTLE DOES NOT ENTER WORKING ZONE. RELOCATE ANY TURTLES AWAY FROM SILT FENCE BARRIER FURTHER INTO UNDISTURBED SITE. SEE WINDSOR INLAND WETLAND PERMIT 22-143 ISSUED FOR PROJECT.

Site S&E Narrative:

THE PROPOSED PROJECT CONSISTS OF A CONVENIENCE STORE, GAS STATION, RETAIL, CAR WASH AND DAYCARE AND ASSOCIATED PARKING AND AMENITIES. THE APPROXIMATELY 13.8 ACRE SITE WILL BE DEVELOPED WITH A DISTURBANCE OF APPROXIMATELY 3.8 ACRES.

THE CONTRACTOR SHALL EMPLOY EROSION AND SEDIMENTATION CONTROL TECHNIQUES, INCLUDING BUT NOT LIMITED TO THOSE LISTED BELOW, TO ENSURE THAT EROSION DOES NOT OCCUR AND THAT SEDIMENT IS NOT TRANSPORTED OFF-SITE TO REGULATED RESOURCE AREA OR OTHER SENSITIVE AREAS ON-SITE. THE CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN THE EROSION AND SEDIMENTATION CONTROLS THROUGHOUT THE DURATION OF CONSTRUCTION.

Temporary Erosion and Sedimentation Control Maintenance (Throughout Construction)

THE SITE CONTRACTOR WILL BE RESPONSIBLE FOR IMPLEMENTING EACH CONTROL SHOWN ON THE SOIL EROSION

THE EROSION AND SEDIMENTATION CONTROLS SHOWN HEREON ARE PERIMETER MEASURES ONLY AND ARE PROVIDED AS A STARTING POINT FOR CONTRACTOR'S STORMWATER POLLUTION CONTROL PLAN (SWPCP). THE CONTRACTOR IS REQUIRED TO PROVIDE ADDITIONAL INTERIM CONTROLS TO MANAGE EROSION AND SEDIMENTATION DURING CONSTRUCTION TO PREVENT IMPACTS TO RESOURCE AREAS, ROADWAYS, AND

THE SITE CONTRACTOR SHALL INSPECT AND MAINTAIN EROSION CONTROL MEASURES ON A WEEKLY BASIS (MINIMUM) OR AS REQUIRED PER THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP). THE CONTRACTOR SHALL ADDRESS DEFICIENCIES AND MAINTENANCE ITEMS WITHIN TWENTY-FOUR HOURS OF INSPECTION. CONTRACTOR SHALL PROPERLY DISPOSE OF SEDIMENT SUCH THAT IT DOES NOT ENCUMBER OTHER DRAINAGE STRUCTURES AND PROTECTED AREAS. RECORDS OF THE INSPECTIONS WILL BE PREPARED AND MAINTAINED ON-SITE BY THE CONTRACTOR.

SILT SHALL BE REMOVED FROM BEHIND BARRIERS IF GREATER THAN 6-INCHES DEEP OR AS NEEDED.

DAMAGED OR DETERIORATED ITEMS WILL BE REPAIRED IMMEDIATELY AFTER IDENTIFICATION. SEDIMENT THAT IS COLLECTED IN STRUCTURES SHALL BE DISPOSED OF PROPERLY AND COVERED IF STORED

EROSION CONTROL STRUCTURES SHALL REMAIN IN PLACE UNTIL ALL DISTURBED EARTH HAS BEEN SECURELY

MAINTAIN THE CONSTRUCTION ENTRANCE IN A CONDITION WHICH WILL PREVENT TRACKING AND WASHING OF SEDIMENTS ONTO PAVED SURFACES.

ALL E&S CONTROL MEASURES WILL BE INSPECTED WEEKLY AND AFTER RAINFALL GENERATING A DISCHARGE.

Construction Sequence

- SEDIMENTATION SHALL NOT AFFECT ROADS/HIGHWAYS AND THEIR DRAINAGE SYSTEM, NEIGHBORING PROPERTIES, AND REGULATORY PROTECTED AREAS, WHETHER SUCH SEDIMENTATION IS CAUSED BY WATER, WIND. OR DIRECT DEPOSIT. PRIOR TO CONSTRUCTION, THE APPLICANT SHALL PROVIDE THE TOWN OF WINDSOR WITH THE NAME OF CONTACT AND 24 HOUR CONTACT INFORMATION.
- AT LEAST 5 DAYS PRIOR TO INITIAL SITE WORK, A PRE-CONSTRUCTION CONFERENCE SHALL BE HELD WITH THE APPLICANT, APPLICANTS CONTRACTOR, A REPRESENTATIVE OF THE BOARD, ITS CONSULTING ENGINEER, AND THE REPRESENTATIVES OF THE TOWN DEPARTMENTS HAVING AN INTEREST IN THE PLAN. (REMEMBER TO CALL BEFORE YOU DIG AT 811). INSTALL STABILIZED VEHICLE CONSTRUCTION ENTRANCE/EXIT.
- PRIOR TO INSTALLING SURFACE WATER CONTROLS SUCH AS TEMPORARY DIVERSION SWALES, INSPECT EXISTING CONDITIONS TO ENSURE DISCHARGE LOCATIONS ARE STABLE. IF NOT STABLE, REVIEW DISCHARGE CONDITIONS WITH THE DESIGN ENGINEER AND IMPLEMENT ADDITIONAL STABILIZATION MEASURES PRIOR TO INSTALLING SURFACE WATER CONTROLS.
- 6. INSTALL EROSION AND SEDIMENT CONTROLS IN ACCORDANCE WITH THE E&S PLAN FOR THE SITE INCLUDING SILT FENCE/HAY BALE BARRIERS, SILT SACKS, SLOPE STABILIZATION MATTING, RIPRAP CHECK DAMS AND STABILIZED CONSTRUCTION EXITS.
- ESTABLISH ROUGH GRADE ON THE SITE.
- APPLY SLOPE STABILIZATION MATTING IN AREAS EXCEEDING 3:1 SLOPES. CONSTRUCT BUILDING AND UNDERGROUND UTILITIES. INSTALL SILT SACK SEDIMENT TRAPS IN ALL NEW
- CATCH BASINS. INSTALL PAVEMENT BASE & FIRST COURSE OF BITUMINOUS CONCRETE.
- INSTALL LANDSCAPING & LOAM AND SEED ALL DISTURBED AREAS. 13. AFTER SITE IS STABILIZED REMOVE TEMPORARY EROSION AND SEDIMENT CONTROLS.
- WHEN ALL OTHER WORK HAS BEEN COMPLETED, REPAIR AND SWEEP ALL PAVED AREAS FOR THE FINAL COURSE OF PAVING. INSPECT THE DRAINAGE SYSTEM AND CLEAN AS NEEDED.
- 16. INSTALL FINAL COURSE OF PAVEMENT.

Erosion and Sedimentation Control Techniques

THE FOLLOWING EROSION AND SEDIMENTATION CONTROLS SHALL BE EMPLOYED BY THE CONTRACTOR DURING THE FARTHWORK AND CONSTRUCTION PHASES OF THE PROJECT:

CATCH BASIN/ WATER QUALITY UNIT PROTECTION
NEWLY CONSTRUCTED AND EXISTING CATCH BASINS/WATER QUALITY UNITS WILL BE PROTECTED WITH SILT SACKS

THROUGHOUT CONSTRUCTION.

A TEMPORARY CRUSHED-STONE CONSTRUCTION ENTRANCE/EXIT WILL BE CONSTRUCTED. A CROSS SLOPE WILL BE PLACED IN THE ENTRANCE TO DIRECT RUNOFF TO THE SEDIMENT TRAP.

STABILIZATION OF OPEN SOIL SURFACES WILL BE IMPLEMENTED WITHIN 14 DAYS AFTER GRADING OR CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, UNLESS THERE IS SUFFICIENT SNOW COVER TO PROHIBIT IMPLEMENTATION. VEGETATIVE SLOPE STABILIZATION WILL BE USED TO MINIMIZE EROSION ON SLOPES OF 3:1 OR FLATTER. ANNUAL GRASSES, SUCH AS ANNUAL RYE, WILL BE USED TO ENSURE RAPID GERMINATION AND PRODUCTION OF ROOTMASS. PERMANENT STABILIZATION WILL BE COMPLETED WITH THE PLANTING OF PERENNIAL GRASSES OR LEGUMES. ESTABLISHMENT OF TEMPORARY AND PERMANENT VEGETATIVE COVER MAY BE ESTABLISHED BY HYDRO-SEEDING OR SODDING. A SUITABLE TOPSOIL, GOOD SEEDBED PREPARATION, AND ADEQUATE LIME, FERTILIZER AND WATER WILL BE

TEMPORARY SEDIMENT BASINS WILL BE DESIGNED EITHER AS EXCAVATIONS OR BERMED STORMWATER DETENTION STRUCTURES (DEPENDING ON GRADING) THAT WILL RETAIN RUNOFF FOR A SUFFICIENT PERIOD OF TIME TO ALLOW SUSPENDED SOIL PARTICLES TO SETTLE OUT PRIOR TO DISCHARGE. THESE TEMPORARY BASINS WILL BE LOCATED BASED ON CONSTRUCTION NEEDS AS DETERMINED BY THE CONTRACTOR AND OUTLET DEVICES WILL BE DESIGNED TO CONTROL

VELOCITY AND SEDIMENT. POINTS OF DISCHARGE FROM SEDIMENT BASINS WILL BE STABILIZED TO MINIMIZE EROSION.

PROVIDED FOR EFFECTIVE ESTABLISHMENT OF THESE VEGETATIVE STABILIZATION METHODS. MULCH WILL ALSO BE USED

AFTER PERMANENT SEEDING TO PROTECT SOIL FROM THE IMPACT OF FALLING RAIN AND TO INCREASE THE CAPACITY OF

SIDESLOPES OF STOCKPILED MATERIAL SHALL BE NO STEEPER THAN 2:1. STOCKPILES NOT USED WITHIN 30 DAYS NEED TO BE SEEDED AND MULCHED IMMEDIATELY AFTER FORMATION OF THE STOCKPILE. HAYBALES AND SILT FENCE ARE TO BE PLACED AROUND THE STOCKPILE AREA APPROXIMATELY 10 FEET FROM THE TOW OF SLOPE

PERIODICALLY MOISTEN EXPOSED SURFACES ON UNPAVED TRAVELWAYS TO KEEP THE TRAVELWAY DAMP AND REDUCE

Seeding

- MULCH TO COVER HYDROSEEDED AREAS SHALL BE FIBER-PROCESSED FROM WHOLE WOOD CHIPS MANUFACTURED SPECIFICALLY FOR STANDARD HYDRAULIC MULCHING EQUIPMENT. FIBER SHALL NOT BE PRODUCED FROM RECYCLED MATERIAL SUCH AS SAWDUST, PAPER, OR CARDBOARD.
- INDUSTRY STANDARDS. FIBER SHALL HAVE A WATER HOLDING CAPACITY OF NOT LESS THAN 900 GRAMS OF WATER PER 100 GRAMS FIBER. 3. MULCH SHALL DISPERSE INTO A UNIFORM SLURRY WHEN MIXED WITH WATER. MULCH SHALL BE NONTOXIC TO PLANT LIFE OR ANIMAL LIFE. 4. MULCH SHALL CONTAIN A NON-PETROLEUM BASED TACKIFIER AND A GREEN DYE FOR VISUAL MONITORING

MOISTURE CONTENT SHALL NOT EXCEED 10%, PLUS OR MINUS 3%, AS DEFINED BY THE PULP AND PAPER

DURING APPLICATION, BUT NON-INJURIOUS TO PLANT GROWTH.

SOW LAWN SEED UNIFORMLY WITH AN APPROVED MECHANICAL SEEDER AT THE RATE OF 5 LBS. PER 1,000 SQUARE FEET. CULTI-PACKER OR APPROVED SIMILAR EQUIPMENT MAY BE USED TO COVER THE SEED AND TO FORM THE SEED BED IN ONE OPERATION. IN AREAS INACCESSIBLE TO THE CULTI-PACKER, THE SEEDED GROUND SHALL BE LIGHTLY RAKED WITH FLEXIBLE RAKES AND ROLLED WITH A WATER BALLAST ROLLER. SEEDING SHALL BE DONE IN TWO DIRECTIONS AT RIGHT ANGLES TO EACH OTHER.

IN AREAS HAVING SLOPES 3:1 OR STEEPER, AND IN DRAINAGE SWALES, THE CONTRACTOR SHALL CARRY OUT A SEPARATE OVERSEEDING OPERATION IMMEDIATELY AFTER SOWING THE SPECIFIED SEED MIX. THE OVERSEEDING SHALL BE SOWN AT THE RATE OF 3 LBS. PER 1,000 SQUARE FEET. SEEDED AREAS REQUIRING ADDITIONAL EROSION CONTROL, SHALL BE COVERED WITH AN APPROVED, BIODEGRADABLE EROSION CONTROL FABRIC AND THE FABRIC FIRMLY ANCHORED IN PLACE.

- DESIGNATED AREAS SHALL BE HYDROSEEDED ONLY AFTER WRITTEN APPROVAL OF THE FINISHED GRADING BY THE ENGINEER.
- FERTILIZER SHALL BE ADDED TO THE HYDROSEEDING SLURRY AT THE RATE OF 5 LBS. PER 1,000 SQUARE FEET. SEED SHALL BE ADDED TO THE HYDROSEEDING SLURRY AT THE RATE OF 220 LBS. PER ACRE. WOOD CELLULOSE FIBER MULCH SHALL BE ADDED TO THE HYDROSEEDING SLURRY AT THE RATE OF 2 TONS
- 5. A MOBILE TANK WITH A CAPACITY OF AT LEAST 500 GALLONS SHALL BE FILLED WITH WATER, AND THE REQUIRED AMOUNTS OF SEED, WOOD CELLULOSE MULCH, AND FERTILIZER. THE SLURRY SHALL BE THOROUGHLY MIXED BY MEANS OF POSITIVE AGITATION IN THE TANK. THE SLURRY SHALL BE APPLIED BY MEANS OF A CENTRIFUGAL PUMP USING THE TURRET OR HOSE APPLICATION TECHNIQUE FROM THE MOBILE TANK. THE HOSE OR TURRET SHALL BE EQUIPPED WITH A SEEDING NOZZLE OF A PROPER DESIGN TO ENSURE EVEN DISTRIBUTION OF THE SOLUTION OVER THE AREA TO BE SEEDED AND SHALL BE OPERATED BY A PERSON THOROUGHLY FAMILIAR WITH THIS TYPE OF SEEDING OPERATION.





Wilson Center Mixed-Use Development

29 Windsor Avenue Windsor, Connecticut

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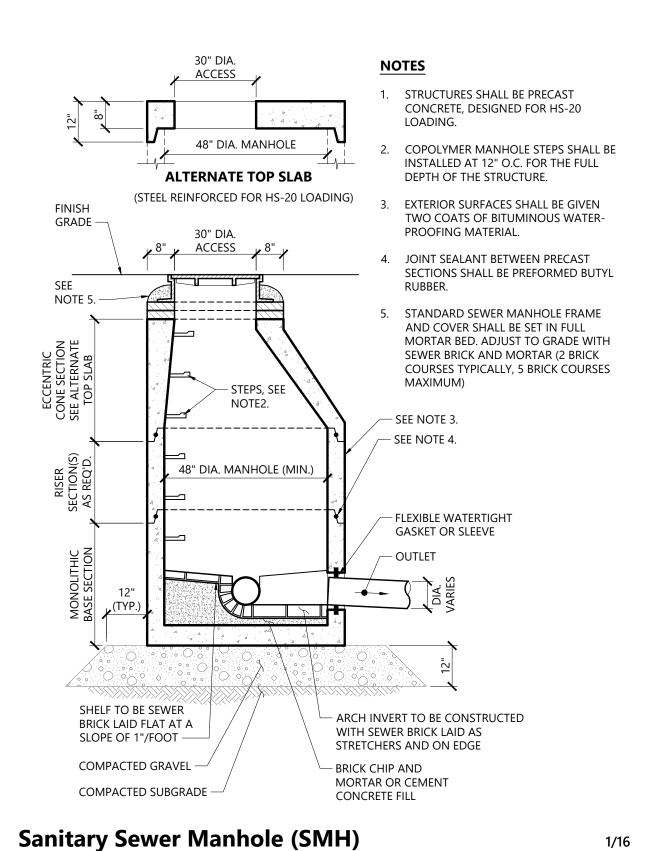




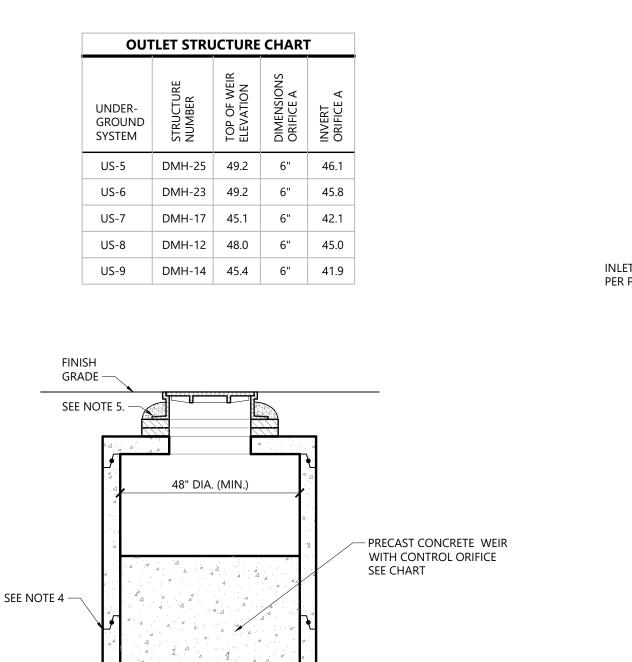
Source: VHB

Drain Manhole (DMH)

N.T.S.



Source: VHB



COMPACTED GRAVEL

— COMPACTED SUBGRADE

SECTION A-A

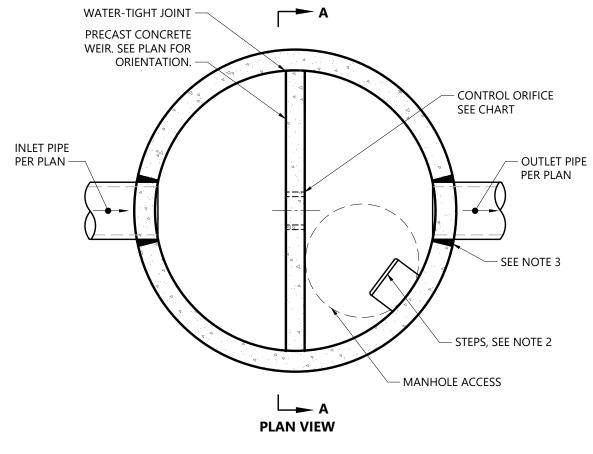
Outlet Control Structure with Weir (OCS)

WATER-TIGHT JOINT —

N.T.S.

1/16

LD_200



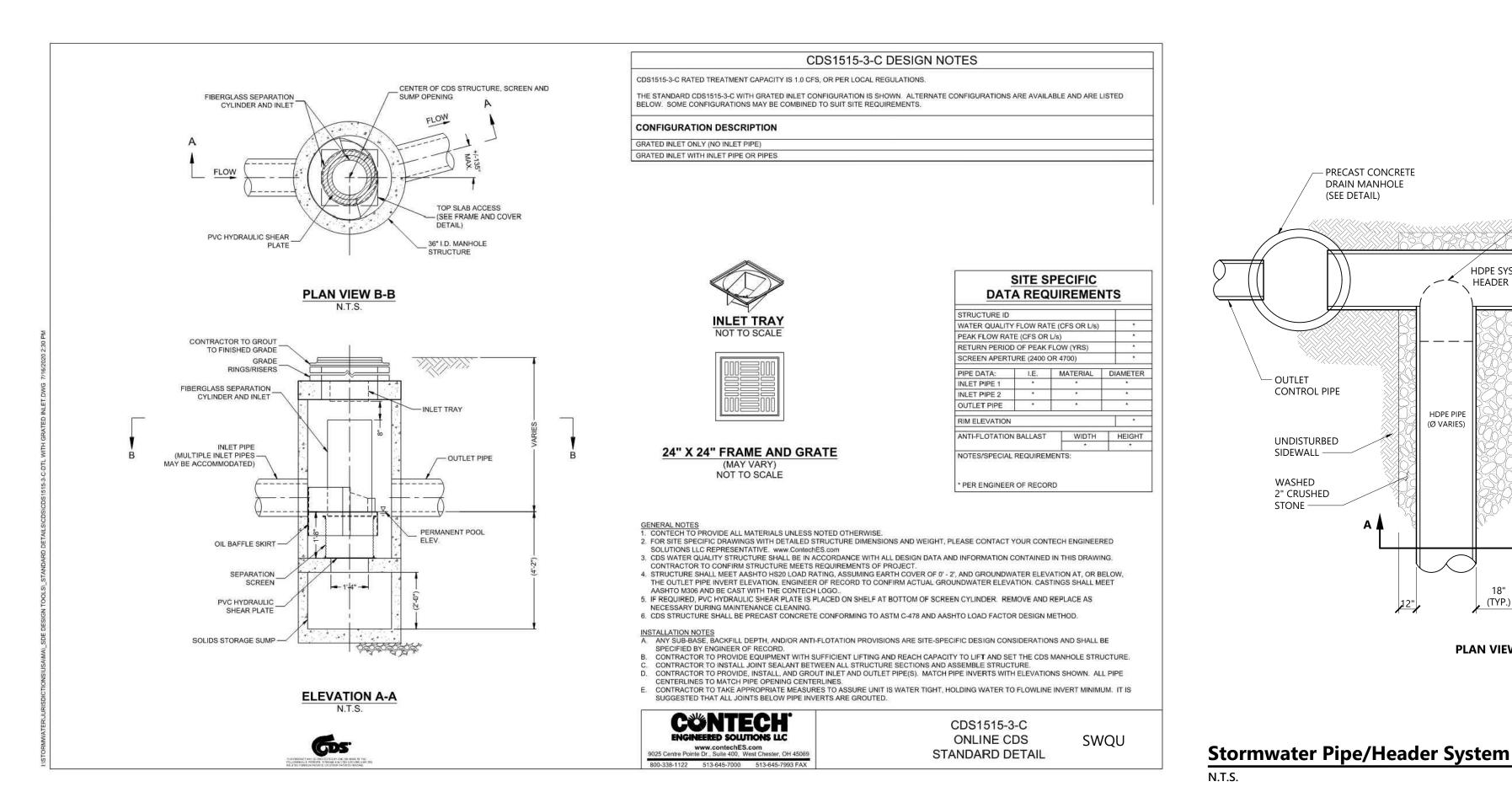
NOTES

- 1. ALL SECTIONS SHALL BE DESIGNED FOR HS-20 LOADING. DIAMETER OF STRUCTURES SHALL BE COORDINATED WITH PIPE CONFIGURATIONS.
- 2. COPOLYMER MANHOLE STEPS SHALL BE INSTALLED AT 12" O.C. FOR THE FULL DEPTH OF THE STRUCTURE.
- 3. FOR HDPE, PVC, AND DI PIPE, PROVIDE FLEXIBLE BOOT CONNECTION INSTALLED PER MANUFACTURER'S RECOMMENDATIONS. FOR RCP, PROVIDE OPENINGS FOR PIPES WITH 2" MAX. CLEARANCE TO OUTSIDE OF PIPE AND MORTAR CONNECTIONS.
- 4. JOINT SEALANT BETWEEN PRECAST SECTIONS SHALL BE PREFORMED BUTYL RUBBER.
- 5. DRAIN MANHOLE FRAME AND COVER SHALL BE SET IN FULL MORTAR BED. ADJUST TO GRADE WITH CLAY BRICK AND MORTAR (2 BRICK COURSES TYPICALLY, 5 BRICK COURSES MAXIMUM)

3/20 LD_162A Source: VHB REV

REV

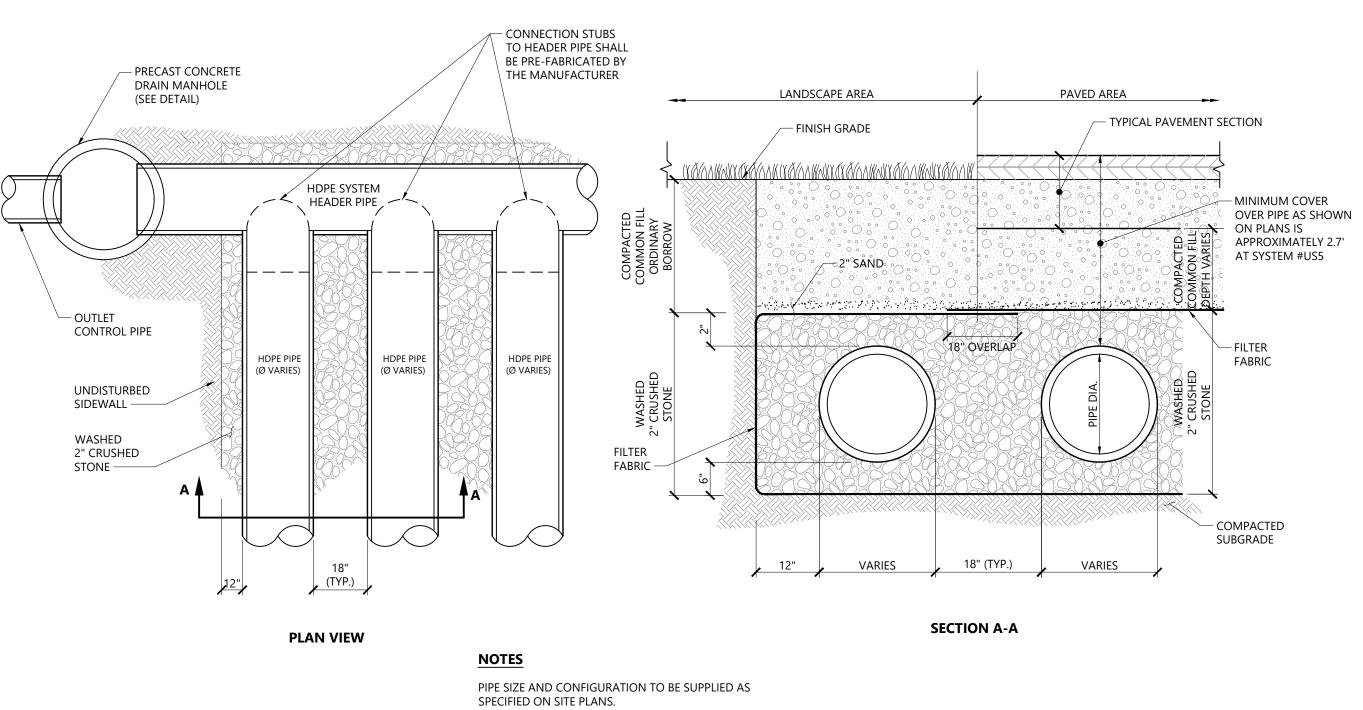
LD_183



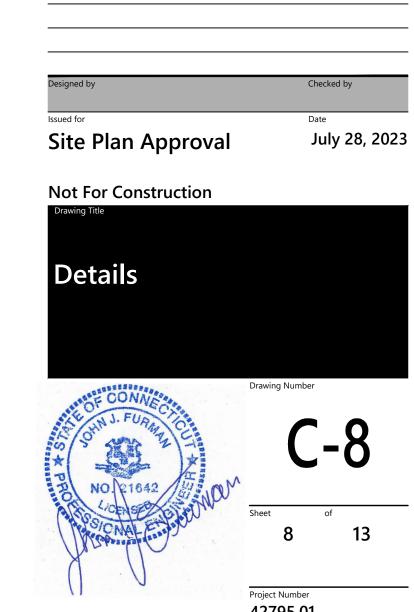
1/16

N.T.S.

LD_115



Source: VHB



Wilson Center

29 Windsor Avenue

Windsor, Connecticut

Mixed-Use Development

One Federal Street

Springfield, MA 01105

Building 103-3N

413.747.7113

NOTES

Source: VHB

- OIL/WATER SEPARATOR SHALL BE A STANDARD PRECAST SEPTIC TANK
 WITH PRECAST BAFFLES AS SHOWN.
- 2. STRUCTURE SHALL BE DESIGNED FOR HS-20 LOADING.
- STRUCTURE SHALL BE DESIGNED FOR HS-20 LOADING.
 SEE PLANS FOR ACTUAL DISPOSITION OF PIPING LAYOUT FOR

COORDINATION OF MANHOLE ACCESS AND BAFFLE.

PIPE AND MORTAR CONNECTIONS.

- 4. FOR HDPE, PVC, AND DI PIPE, PROVIDE FLEXIBLE BOOT CONNECTION INSTALLED PER MANUFACTURER'S RECOMMENDATIONS. FOR RCP, PROVIDE OPENINGS FOR PIPES WITH 2" MAX. CLEARANCE TO OUTSIDE OF
- 5. JOINT SEALANT BETWEEN ALL SECTIONS SHALL BE PREFORMED BUTYL
- 6. STANDARD DRAIN MANHOLE FRAME AND COVER SHALL BE SET IN FULL MORTAR BED. ADJUST TO GRADE WITH CLAY BRICK AND MORTAR (2 BRICK COURSES TYPICALLY, 5 BRICK COURSES MAXIMUM).
- 7. STANDARD BARREL BLOCK AND MORTAR SHALL BE USED TO BRING MANHOLE FRAME AND COVER TO FINISHED GRADE WHEN DEPTH TO TOP OF STRUCTURE EXCEEDS 18 INCHES.
- 8. WHEN CONNECTING TO DISCHARGE PIPE THE PVC OUTLET TEE AND PIPE SHALL BE SIZED TO FIT SNUGGLY INSIDE THE DISCHARGE PIPE AND THE JOINT SHALL BE SEALED WITH NON-SHRINK CEMENT GROUT.
- 9. GEOMETRY OF PIPING AND LOCATION OF BAFFLES AND MANHOLE ACCESS TO BE CONSISTENT WITH UTILITY PLAN.

12/19

LD_144

INLET -— OUTLET - 30" DIA. OPENING TO BE LOCATED OVER TEES (TYP.) **PLAN VIEW** MANHOLE RISER W/FRAME AND – 4' DIA. MANHOLE COVER (TYP.) RISER TO BE SET SEE NOTE 4. ON GROUT BEDDING — FOR VENT LOCATION SEE PLANS INLET — — OUTLET ALTERNATE SEE NOTE 3. — OUTLET — SEE NOTE 2. STAINLESS STEEL - CROSS WITH CLEANOUT BRACKET (TYP.) —— ADAPTER AND THREADED PLUG — COMPACTED SUBGRADE — — COMPACTED GRAVEL SECTION

13' - 0"

SIZE (GAL.) LIQUID DEPTH

2,000 4'-4"

2,500 5'-4"

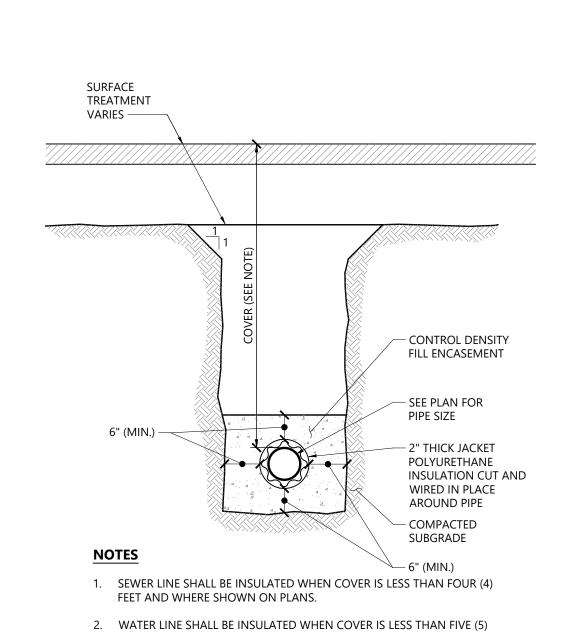
3,000 6'-5"

3,500 7'-6"

NOTES

- 1. STRUCTURE SHALL BE DESIGNED FOR HS-20 LOADING.
- EXTERIOR SURFACES SHALL BE GIVEN TWO COATS OF BITUMINOUS WATER-PROOFING MATERIAL.
- 3. JOINT SEALANT BETWEEN PRECAST SECTIONS SHALL BE PREFORMED BUTYL RUBBER.
- 4. STANDARD 30-INCH SEWER MANHOLE FRAME AND COVER SHALL BE LOCATED OVER CROSSES AND SET IN FULL MORTAR BED. ADJUST TO GRADE WITH SEWER BRICK AND MORTAR (2 BRICK COURSES TYPICALLY, 5 BRICK COURSES MAXIMUM)
- 5. PIPING SHALL BE SCH 40 PVC WITH SOLVENT WELDED JOINTS. INTERNAL PIPE DIAMETER SHALL BE SAME SIZE AS OUTLET PIPE.
- FINAL DESIGN OF GREASE TRAP TO BE BY PLUMBING ENGINEER.
- 7. THE INSTALLATION OF GREASE TRAP, THE PIPING TO AND 10 FEET BEYOND IS BY PLUMBER.

Precast Concrete Grease Trap (GT)N.T.S.Source: VHBREVLD_210



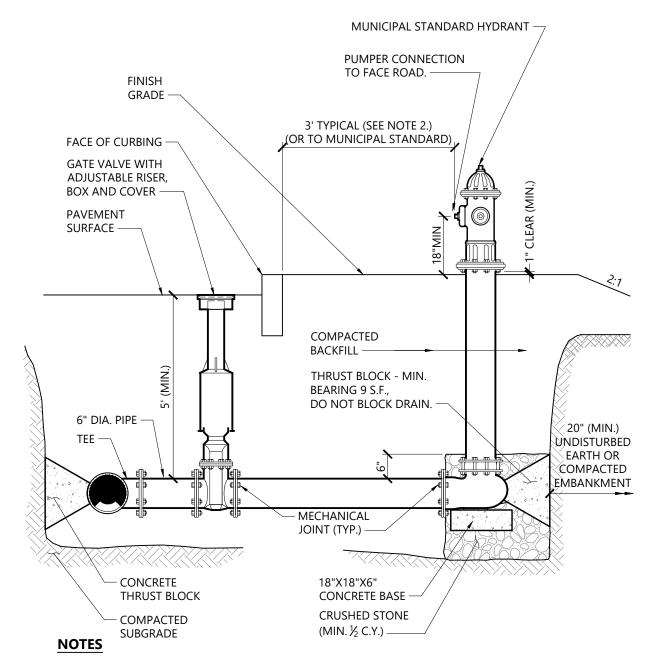
Utility - Pipe Insulation 12/19				
Othity - Pipe	ilisulation	12/19		
N.T.S.	Source: VHB	LD_304		

3. BACKFILL PLACED IN UTILITY TRENCHES INCLUDING DISTURBED AREAS

SURROUNDING UTILITY TRENCHES SHALL BE PLACED AND COMPACTED

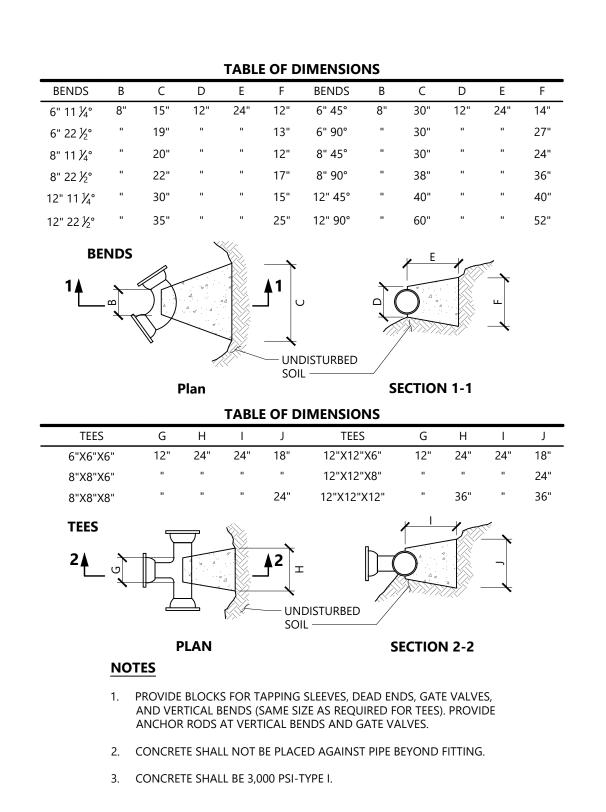
FEET AND WHERE SHOWN ON PLANS.

IN 12" (MAX.) VERTICAL LIFTS.

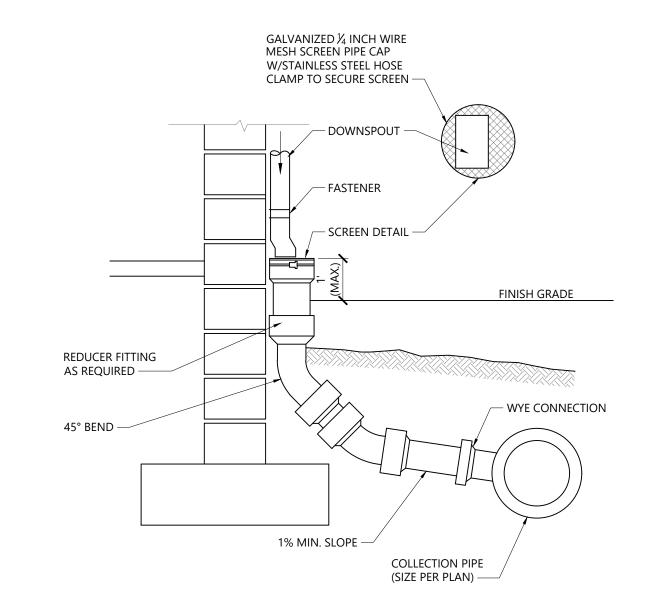


- 1. CONCRETE THRUST BLOCKS TO BE USED ONLY WHERE THEY CAN BEAR ON UNDISTURBED EARTH AS SHOWN. USE CLAMPS AND TIE RODS OR OTHER ACCEPTABLE METHOD OF JOINT RESTRAINT WHERE SOIL CONDITIONS PROHIBIT THE USE OF THRUST BLOCKS.
- HYDRANT IN SIDEWALK AREAS TO BE LOCATED TO PROVIDE MINIMUM CLEAR SIDEWALK PASSAGE WIDTH OF 3 FEET AT HYDRANT.
- 3. A 36-INCH CLEAR SPACE SHALL BE MAINTAINED AROUND THE CIRCUMFERENCE OF THE HYDRANT UNLESS OTHERWISE APPROVED BY AUTHORITY HAVING JURISDICTION.

Hydrant Construction		12/18
N.T.S.	Source: VHB	LD_250



Concrete Thrust Block		1/16
N.T.S.	Source: VHB	LD_260



Downspout Rain Leader		1/16
N.T.S.	Source: VHB	LD 195

Wilson Center Mixed-Use Development

One Federal Street

Springfield, MA 01105

Building 103-3N

413.747.7113

29 Windsor Avenue Windsor, Connecticut

No. Revision Date Appvo

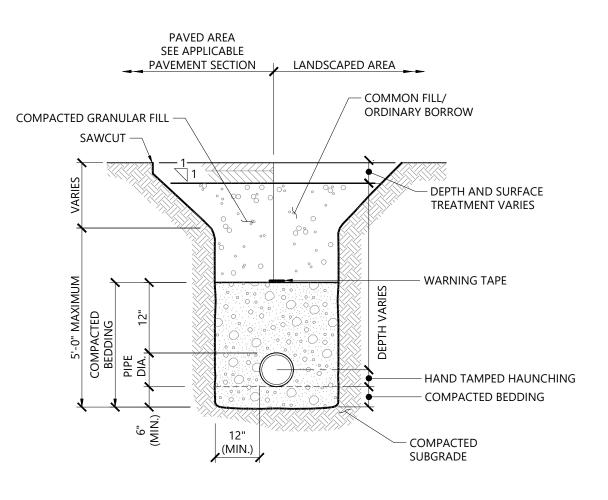
Site Plan Approval	July 28, 202
sued for	Date
esigned by	Checked by
esigned by	Checked by

Not For Construction



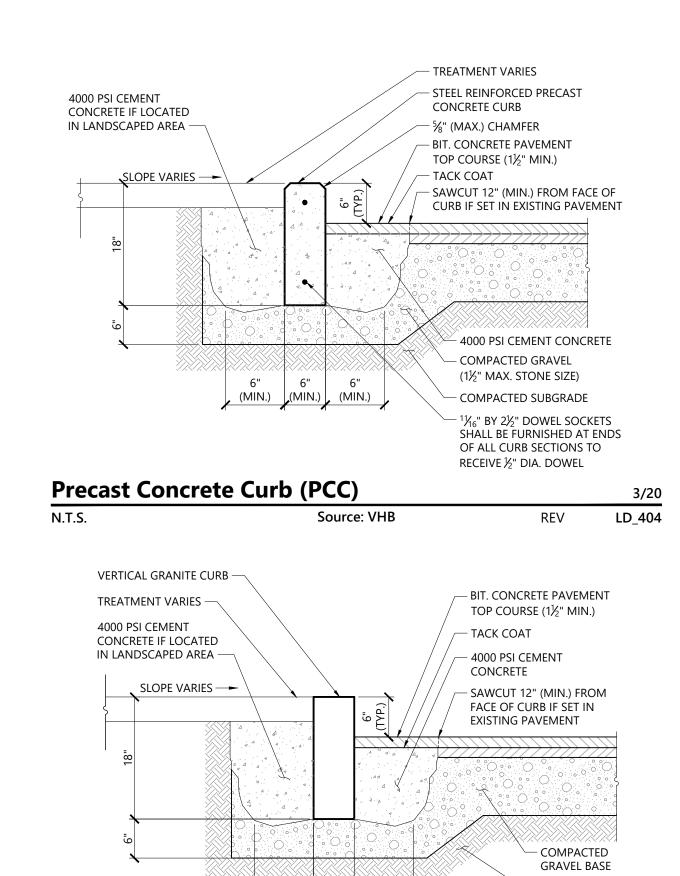






- 1. WHERE UTILITY TRENCHES ARE CONSTRUCTED THROUGH DETENTION BASIN BERMS OR OTHER SUCH SPECIAL SECTIONS, PLACE TRENCH BACKFILL WITH MATERIALS SIMILAR TO THE SPECIAL SECTION REQUIREMENTS.
- 2. USE METALLIC TRACING/WARNING TAPE OVER ALL PIPES.

Utility Trench		1/16
N.T.S.	Source: VHB	LD_300



Vertical Granite Curb (VGC)

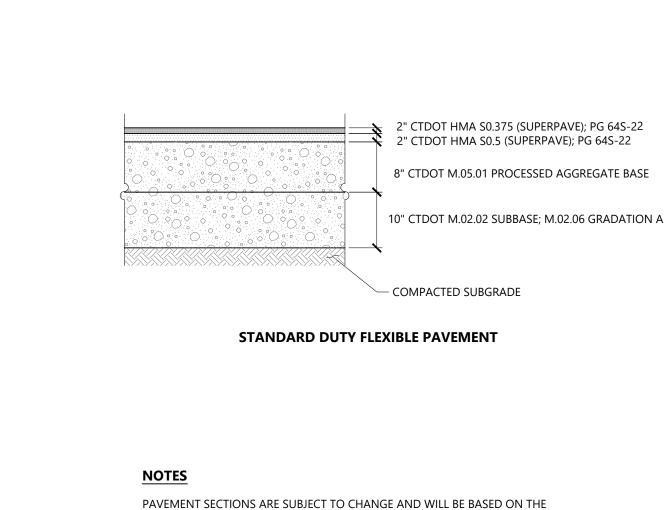
N.T.S.

- COMPACTED SUBGRADE

REV

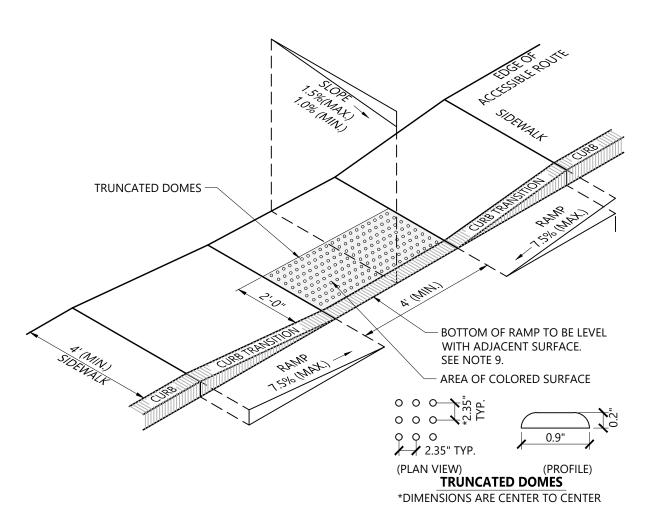
3/20

LD_402



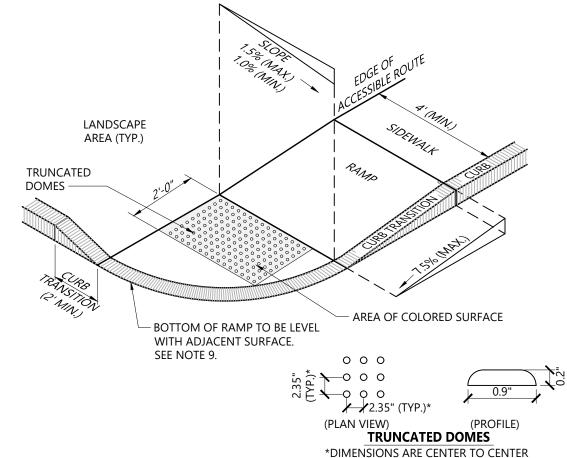
RESULTS OF FURTHER GEOTECHNICAL INVESTIGATIONS.

Bituminous Concrete Pavement Sections 11/19 N.T.S. Source: VHB REV LD_430



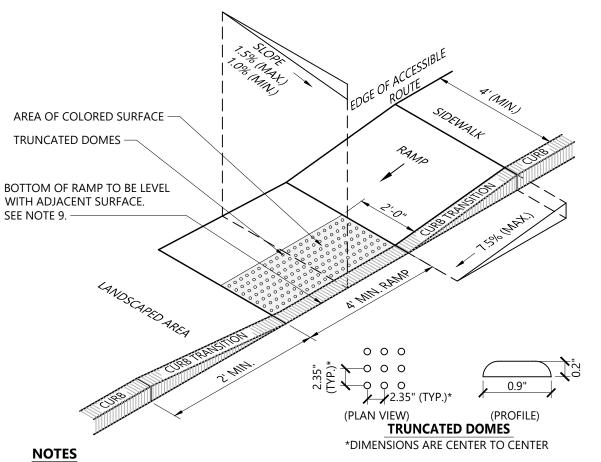
- 1. THE MAXIMUM ALLOWABLE SIDEWALK AND CURB RAMP CROSS SLOPES SHALL BE 1.5 (1% MIN.).
- 2. THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE EXCLUDING CURB RAMPS SHALL BE 5%. 3. THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE AT CURB RAMPS SHALL BE 7.5%.
- 4. A MINIMUM OF 3 FEET CLEAR SHALL BE MAINTAINED AT ANY PERMANENT OBSTACLE IN ACCESSIBLE ROUTE (I.E., HYDRANTS, UTILITY POLES, TREE WELLS, SIGNS, ETC.).
- 5. CURB TREATMENT VARIES, SEE PLANS FOR CURB TYPE.
- 6. RAMP, CURB, AND ADJACENT PAVEMENTS SHALL BE GRADED TO PREVENT PONDING.
- 7. SEE TYPICAL SIDEWALK SECTION FOR RAMP CONSTRUCTION.
- 8. WHERE ACCESSIBLE ROUTES ARE LESS THAN 5' IN WIDTH (EXCLUDING CURBING) A 5' x 5' PASSING AREA SHALL BE PROVIDED AT INTERVALS NOT TO EXCEED 200 FEET.
- 9. ELIMINATE CURBING AT RAMP (OTHER THAN VERTICAL CURBING, WHICH SHALL BE SET FLUSH) WHERE IT ABUTS ROADWAY.
- 10. DETECTABLE WARNINGS SHALL CONTRAST VISUALLY WITH ADJOINING SURFACES.
- 11. DETECTABLE WARNINGS SHALL BE INSTALLED PERPENDICULAR TO ACCESSIBLE ROUTE.

Accessible Curb Ramp (ACR) Type 'A-D' 12/20 LD_500



- 1. THE MAXIMUM ALLOWABLE SIDEWALK AND CURB RAMP CROSS SLOPES SHALL BE 1.5 (1% MIN.).
- 2. THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE EXCLUDING CURB RAMPS SHALL BE 5%. 3. THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE AT CURB RAMPS SHALL BE 7.5%.
- 4. A MINIMUM OF 3 FEET CLEAR SHALL BE MAINTAINED AT ANY PERMANENT OBSTACLE IN ACCESSIBLE ROUTE (I.E., HYDRANTS, UTILITY POLES, TREE WELLS, SIGNS, ETC.).
- 5. CURB TREATMENT VARIES, SEE PLANS FOR CURB TYPE.
- 6. RAMP, CURB AND ADJACENT PAVEMENTS SHALL BE GRADED TO PREVENT PONDING.
- 7. SEE TYPICAL SIDEWALK SECTION FOR RAMP CONSTRUCTION.
- 9. ELIMINATE CURBING AT RAMP WHERE IT ABUTS ROADWAY, EXCEPT WHERE VERTICAL CURBING IS INDICATED ON THE DRAWINGS TO BE INSTALLED AND SET FLUSH.
- 10. DETECTABLE WARNINGS SHALL CONTRAST VISUALLY WITH ADJOINING SURFACES.
- 11. DETECTABLE WARNINGS SHALL BE INSTALLED PERPENDICULAR TO THE ACCESSIBLE ROUTE. 12. CONTRACTOR TO SUBMIT R.F.I. FOR THIS TYPE OF ACCESSIBLE CURB RAMP FOR APEX ROADWAY CROSSINGS.

Accessible Curb Ramp (ACR) - Type 'B-D' 12/20 LD_501



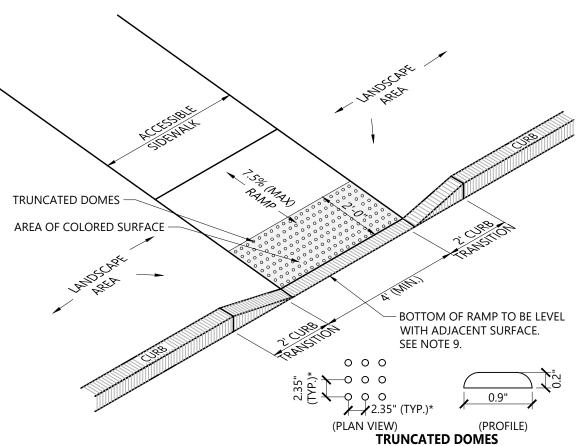
Source: VHB

1. THE MAXIMUM ALLOWABLE SIDEWALK AND CURB RAMP CROSS SLOPES SHALL BE 1.5 (1% MIN.).

- 2. THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE EXCLUDING CURB RAMPS SHALL BE 5%.
- 3. THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE AT CURB RAMPS SHALL BE 7.5%.
- 4. A MINIMUM OF 3 FEET CLEAR SHALL BE MAINTAINED AT ANY PERMANENT OBSTACLE IN ACCESSIBLE ROUTE (I.E., HYDRANTS, UTILITY POLES, TREE WELLS, SIGNS, ETC.). 5. CURB TREATMENT VARIES, SEE PLANS FOR CURB TYPE
- 6. RAMP, CURB AND ADJACENT PAVEMENTS SHALL BE GRADED TO PREVENT PONDING.
- 7. SEE TYPICAL SIDEWALK SECTION FOR RAMP CONSTRUCTION.
- 9. ELIMINATE CURBING AT RAMP WHERE IT ABUTS ROADWAY, EXCEPT WHERE VERTICAL CURBING IS INDICATED ON THE DRAWINGS TO BE INSTALLED AND SET FLUSH.
- 10. DETECTABLE WARNINGS SHALL CONTRAST VISUALLY WITH ADJOINING SURFACES.
- 11. DETECTABLE WARNINGS SHALL BE INSTALLED PERPENDICULAR TO THE ACCESSIBLE ROUTE.

12. CONTRACTOR TO SUBMIT R.F.I. FOR THIS TYPE OF ACCESSIBLE CURB RAMP FOR APEX ROADWAY CROSSINGS.

Accessible Curb Ramp (ACR) Type 'G-D' 12/20 LD_506



1. THE MAXIMUM ALLOWABLE SIDEWALK AND CURB RAMP CROSS SLOPES SHALL BE 1.5 (1% MIN.).

*DIMENSIONS ARE CENTER TO CENTER

- THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE EXCLUDING CURB RAMPS SHALL BE 5%.
- 3. THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE AT CURB RAMPS SHALL BE 7.5%.
- A MINIMUM OF 3 FEET CLEAR SHALL BE MAINTAINED AT ANY PERMANENT OBSTACLE IN ACCESSIBLE ROUTE (I.E., HYDRANTS, UTILITY POLES, TREE WELLS, SIGNS, ETC.).
- CURB TREATMENT VARIES, SEE PLANS FOR CURB TYPE.
- 6. RAMP, CURB AND ADJACENT PAVEMENTS SHALL BE GRADED TO PREVENT PONDING.
- SEE TYPICAL SIDEWALK SECTION FOR RAMP CONSTRUCTION.
- ELIMINATE CURBING (OTHER THAN VERTICAL CURBING, WHICH SHALL BE SET FLUSH) WHERE IT ABUTS ROADWAYS.
- 10. DETECTABLE WARNINGS SHALL CONTRAST VISUALLY WITH ADJOINING SURFACES.

11. DETECTABLE WARNINGS SHALL BE INSTALLED PERPENDICULAR TO THE ACCESSIBLE

Accessible Curb Ramp (ACR) Type 'M-D'

12/20 Source: VHB LD_512

One Federal Street Building 103-3N Springfield, MA 01105 413.747.7113

July 28, 2023

Wilson Center

29 Windsor Avenue

Site Plan Approval

Not For Construction

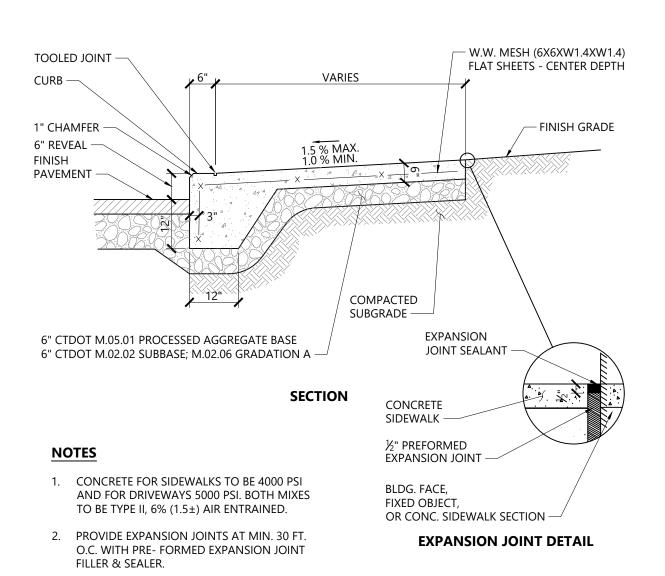
Details

Windsor, Connecticut

Mixed-Use Development

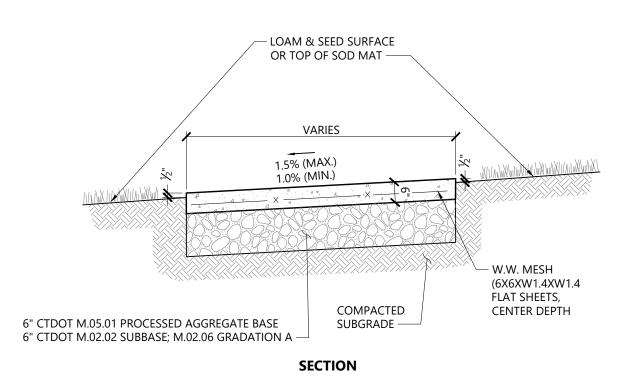






- 3. PROVIDE SAWCUT CONTROL JOINTS AT 6' O.C. OR AS NOTED ON PLANS.
- 4. PROVIDE BROOM FINISH IN DIRECTION PERPENDICULAR TO CURB.
- 5. ALL EXPOSED CONCRETE SURFACES SHALL BE SEALED WITH A SILANE-SILOXANE PRODUCT.

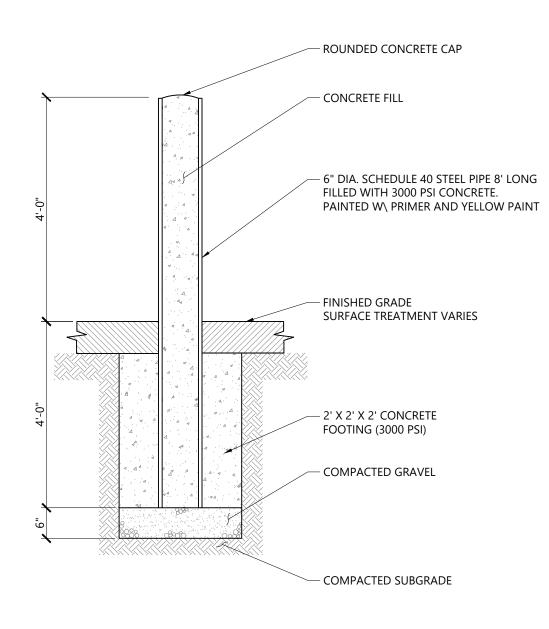
Monolithic Concrete Curb (MCC) & Sidewalk			3/20
N.T.S.	Source: VHB	REV	LD_421



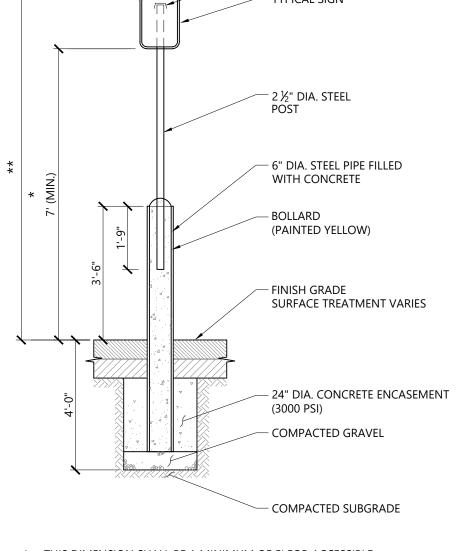
NOTES

- 1. CONCRETE FOR SIDEWALKS TO BE 4000 PSI AND FOR DRIVEWAYS 5000 PSI. BOTH MIXES TO BE TYPE II, 6% (1.5±) AIR ENTRAINED.
- 2. PROVIDE EXPANSION JOINTS AT MIN. 30 FT. O.C. WITH PRE- FORMED EXPANSION JOINT FILLER & SEALER.
- 3. PROVIDE SAWCUT CONTROL JOINTS AT 6' O.C. OR AS NOTED ON
- 4. PROVIDE MEDIUM BROOM FINISH IN DIRECTION PERPENDICULAR TO
- 5. ALL EXPOSED CONCRETE SURFACES SHALL BE SEALED WITH A SILANE-SILOXANE PRODUCT.

Concrete Si		3/21	
N.T.S.	Source: VHB	REV	LD_426



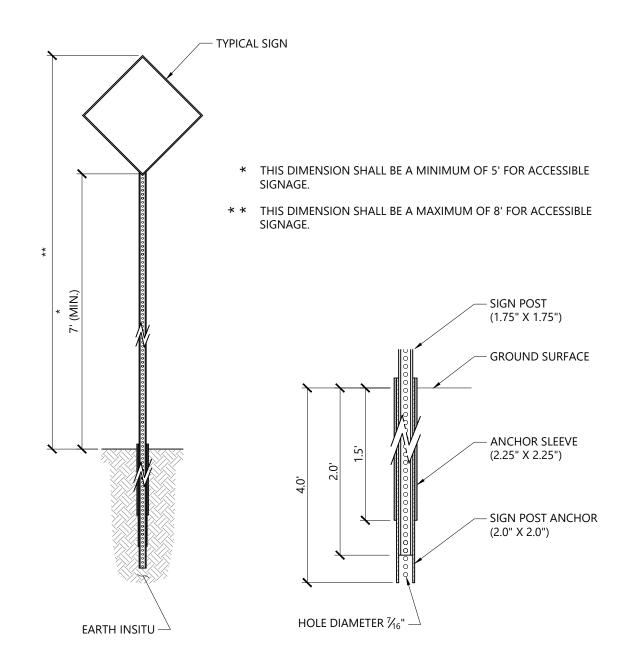
Bollard		12/18
N.T.S.	Source: VHB	LD_700S



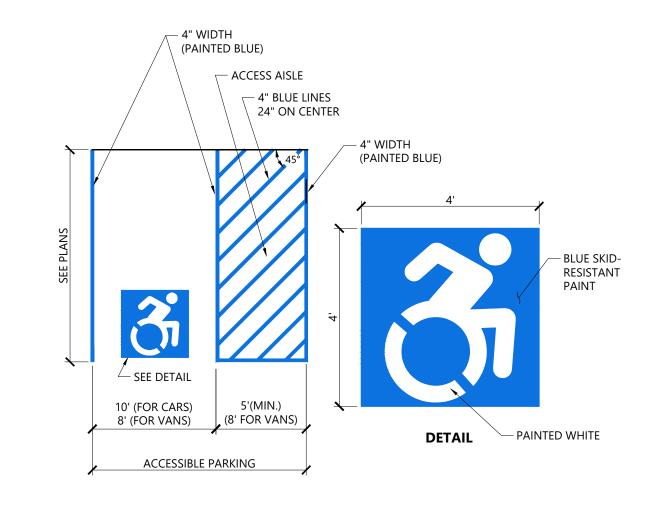
— CAP STEEL PIPE POST

- * THIS DIMENSION SHALL BE A MINIMUM OF 5' FOR ACCESSIBLE * * THIS DIMENSION SHALL BE A MAXIMUM OF 8' FOR ACCESSIBLE

Source: VHB

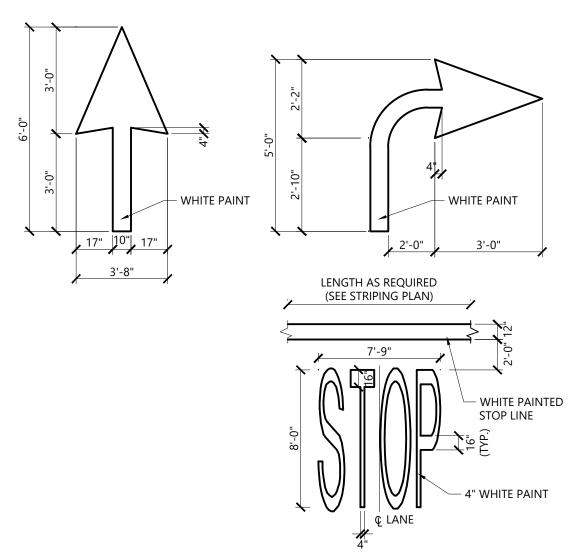


Sign Post - Type 'B' LD_702 Source: VHB



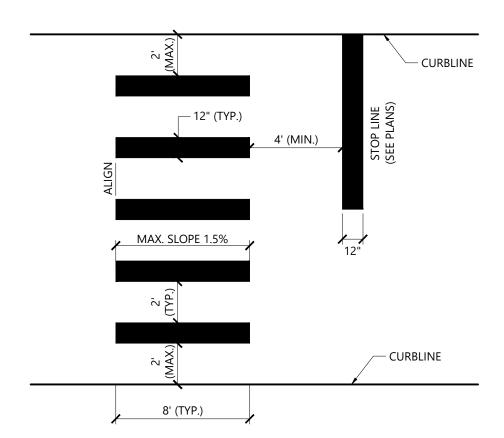
- 1. ALL DIMENSIONS TO CENTER OF 4" PAVEMENT STRIPING.
- 2. ALL SLOPES THROUGHOUT THE ACCESSIBLE PARKING AND AISLE AREAS SHALL NOT EXCEED 1.5%.
- 3. THE ACCESSIBLE SYMBOL DEPICTED ABOVE DOES NOT COMPLY WITH THE AMERICANS WITH DISABILITIES ACT (ADA) AND IS SHOWN FOR COMPLIANCE WITH STATE AND LOCAL REGULATIONS ONLY.

Accessible Parking Space (CT ONLY)			12/19
N.T.S.	Source: VHB	REV	LD 552D



PAVEMENT MARKINGS TO BE INSTALLED FOR ON SITE WORK IN LOCATIONS SHOWN.

Painted Pavem	avement Markings - On Site		1/16
N.T.S.	Source: VHB	REV	LD 554



Bollard Mounted Sign

- 1. TWELVE INCH (12") LINES SHALL BE APPLIED IN ONE APPLICATION, NO COMBINATION OF LINES (TWO - 6 INCH LINES) WILL BE ACCEPTED.
- 2. LONGITUDINAL CROSSWALK LINES TO BE PARALLEL TO CURBLINE.
- 3. ALL LONGITUDINAL CROSSWALK LINES TO BE THE SAME LENGTH AND PROPERLY ALIGNED.
- 4. CROSS WALK SIDESLOPE SHALL NOT EXCEED 1.5%.

Crosswalk		1/16
N.T.S.	Source: VHB	LD 553



29 Windsor Avenue Windsor, Connecticut

1/16

LD_703S

		_	
No.	Revision	Date	App
Designe	ed by	Checked by	

July 28, 2023 Site Plan Approval

Not For Construction

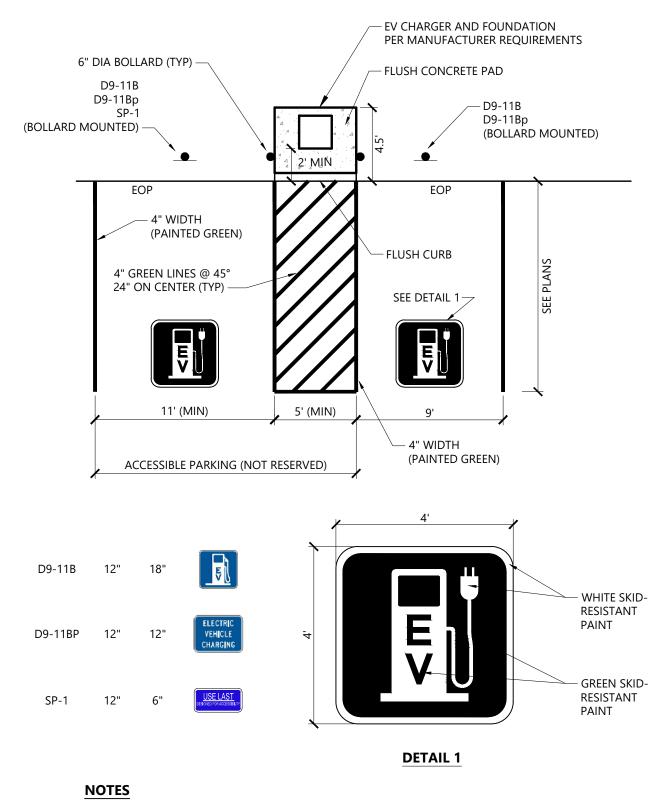




NOTES

 DUMPSTER PAD DIMENSIONS SHOWN AS MINIMUM. REFER TO PLAN FOR ACTUAL DIMENSION.

Dumpster Pad w/ Enclosure1/16N.T.S.Source: VHBLD_713

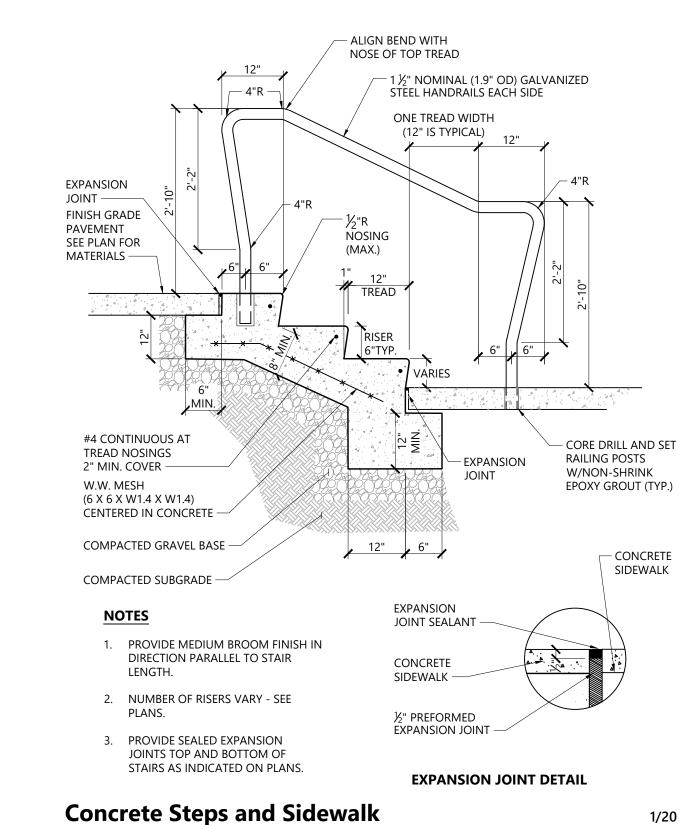


1. ALL DIMENSION TO CENTER OF STRIPING.

2. ALL SLOPES THROUGH OUT THE ACCESSIBLE PARKING AND AISLE SHALL NOT EXCEED 1.5%

Electric Vehicle Charging Spaces

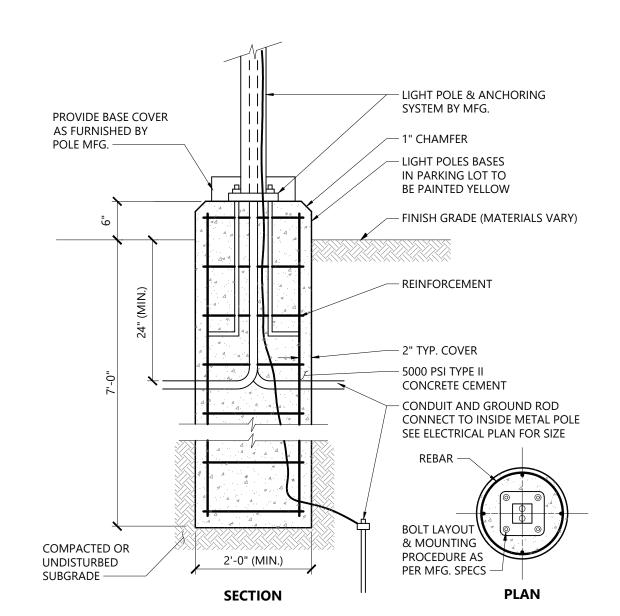
N.T.S. Source: VHB



Source: VHB





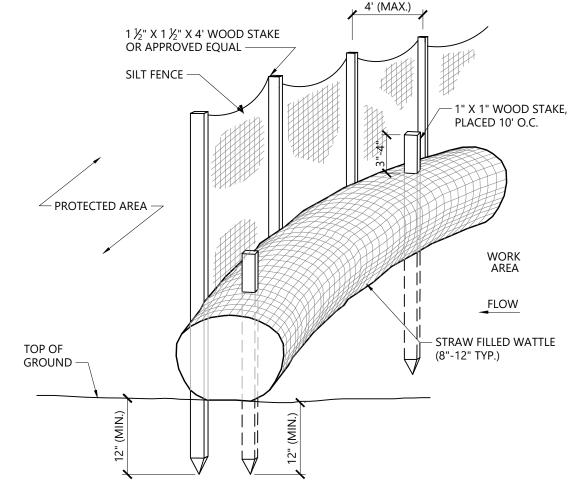


NOTES

DETAIL PROVIDED FOR GENERAL INFORMATION ONLY. CONTRACTOR TO PROVIDE STAMPED FINAL DESIGN OF LIGHT POLE FOUNDATION BASED ON RECOMMENDATIONS FROM THE GEOTECHNICAL ENGINEER.

Light Pole Foundation Detail (Up to 40' Pole) 12/19

N.T.S. Source: VHB REV LD_310



NOTES

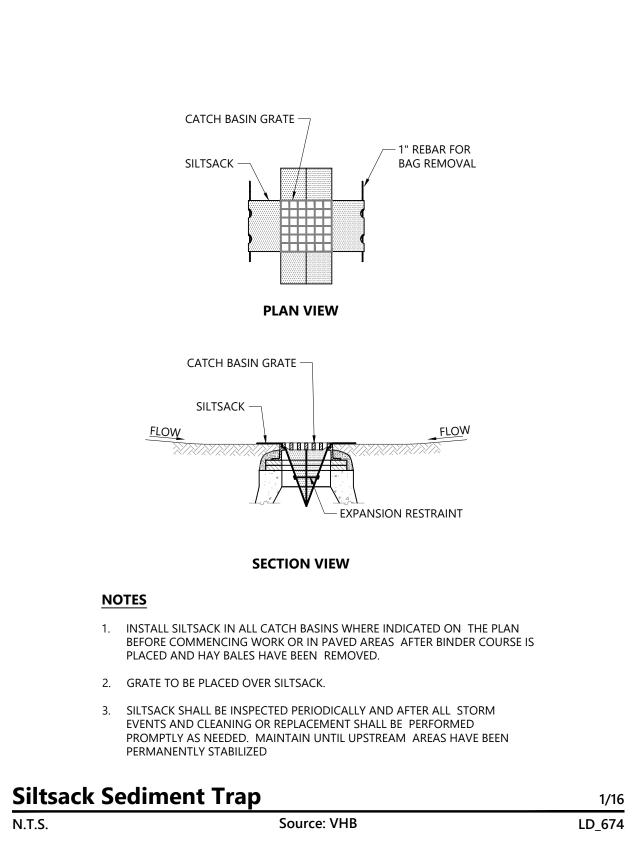
- STRAW WATTLE SHALL BE AS MANUFACTURED BY EARTHSAVER OR
- STRAW WATTLES SHALL OVERLAP A MINIMUM OF 12 INCHES.
- 3. STRAW WATTLE SHALL BE INSPECTED PERIODICALLY AND AFTER ALL STORM EVENTS, AND REPAIR OR REPLACEMENT SHALL BE PERFORMED PROMPTLY
- 4. TEMPORARY STRAW WATTLES TO BE REMOVED BY CONTRACTOR. ALL
- OTHERS TO REMAIN IN PLACE UNLESS DIRECTED OTHERWISE BY ENGINEER.

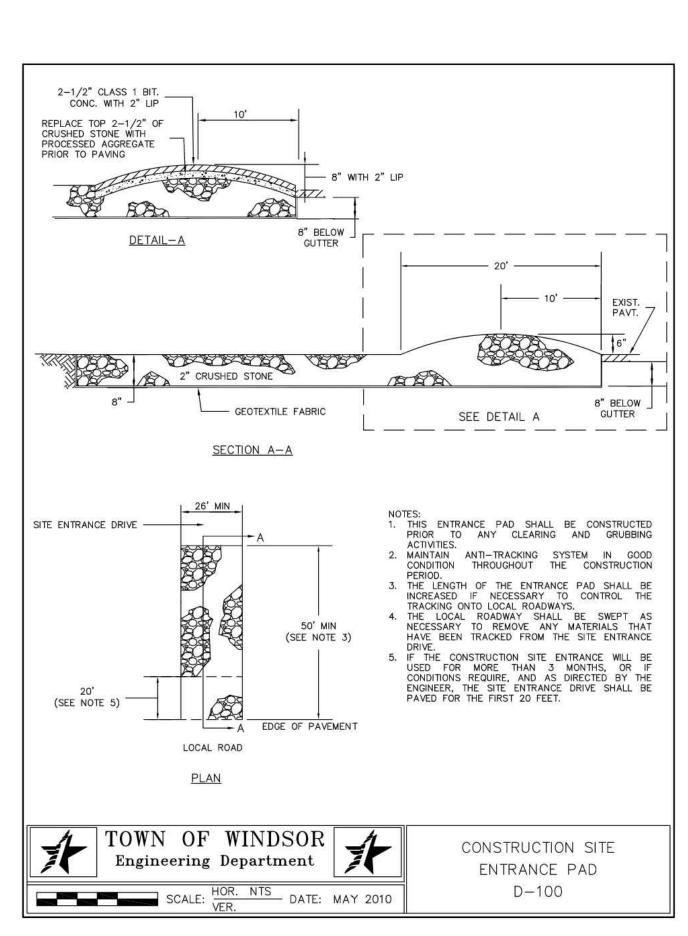
5. IF NON BIODEGRADABLE NETTING IS USED THE NETTING SHALL BE

- COLLECTED AND DISPOSED OF OFFSITE.

 6. SILT FENCE TO BE A MINIMUM OF 30" HIGH.
- Straw Wattle Erosion Control Barrier 10/21

 N.T.S. Source: VHB REV LD_659-A



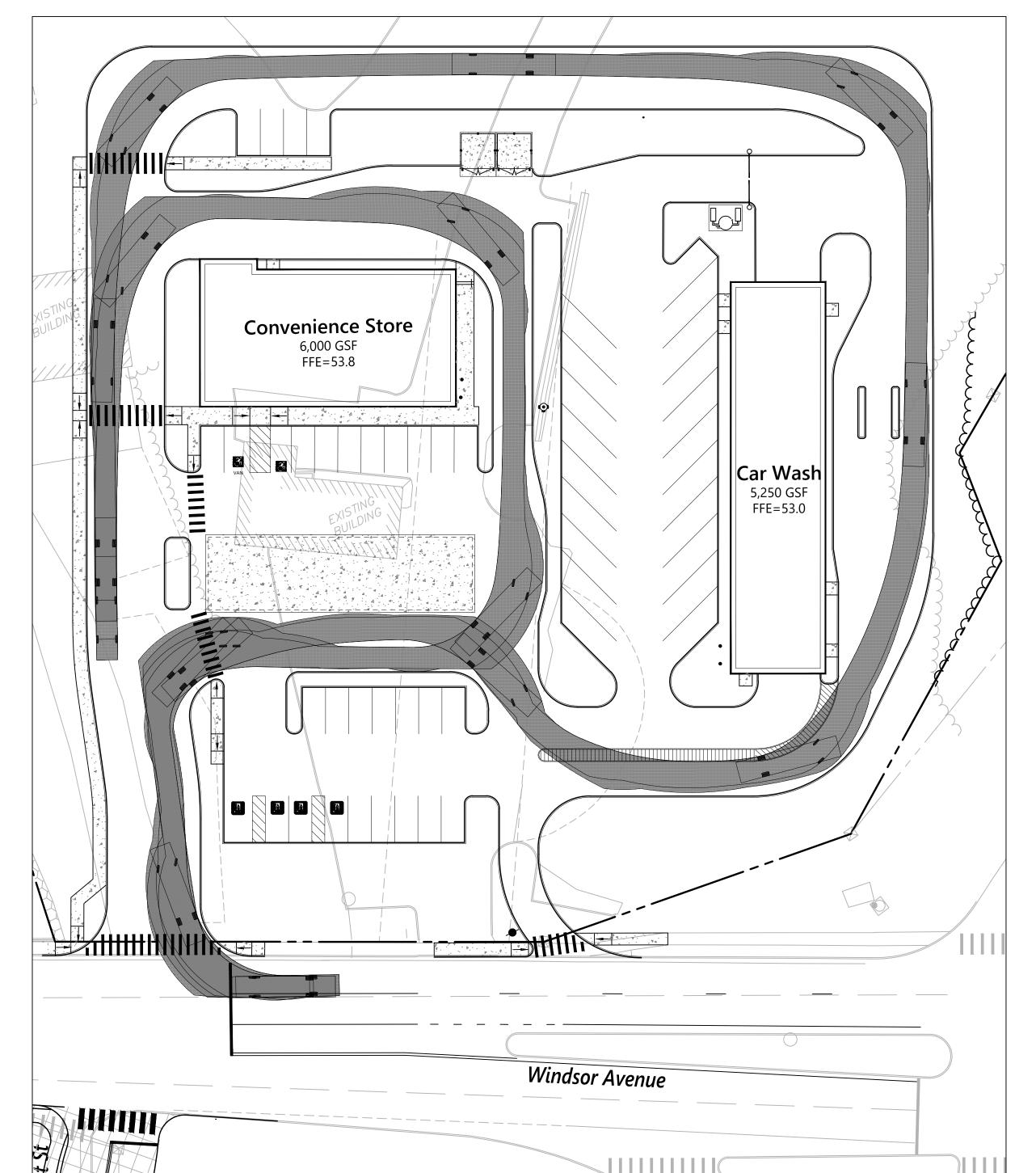


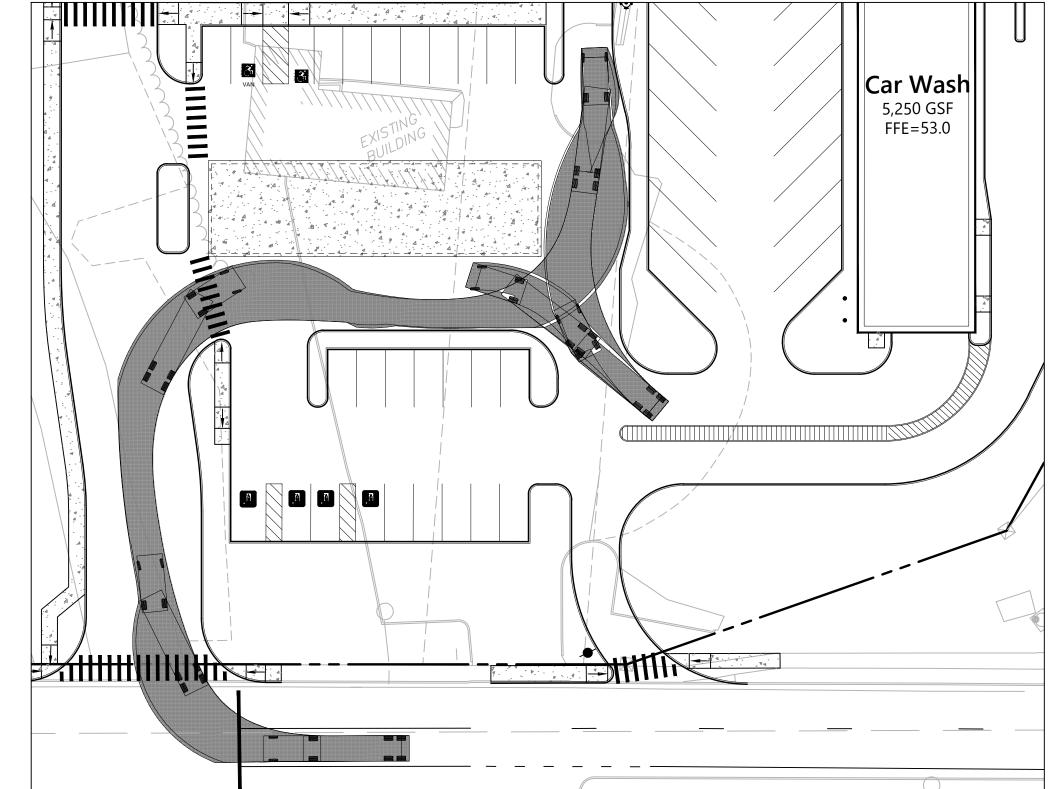
LD_766

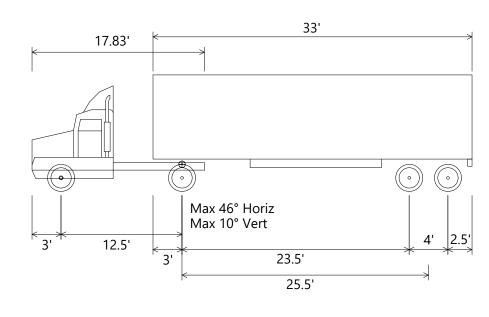


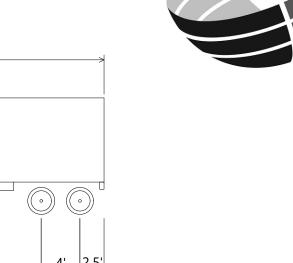
NO C1642

Details









One Federal Street

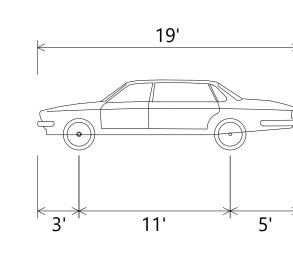
Springfield, MA 01105

Building 103-3N

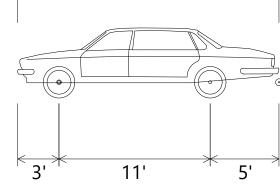
413.747.7113

Fuel Tanker Truck (Similar to a WB-40)

Overall Length	45.5 ft
Overall Width	8 ft
Overall Body Height	12.1 ft
Min Body Ground Clearance	1.3 ft
Гrack Width	8 ft
ock-to-lock time	4 s
Max Steering Angle (Virtual)	20.4°



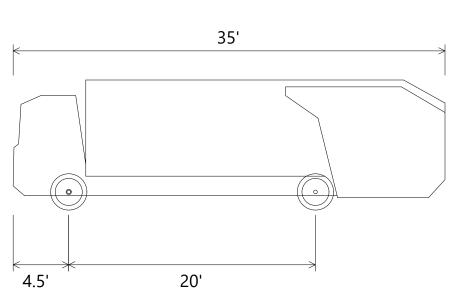
Overall Length	19 ft
Overall Width	7 ft
Overall Body Height	5.1 ft
Min Body Ground Clearance	1.1 ft
Track Width	6 ft
Lock-to-lock time	4 s
Max Steering Angle (Virtual)	31.6°



Passenger Car

Overall Length	19 ft
Overall Width	7 ft
Overall Body Height	5.1 ft
Min Body Ground Clearance	1.1 ft
Track Width	6 ft
Lock-to-lock time	4 s
Max Steering Angle (Virtual)	31.6°





Garbage Truck

Overall Length	35 ft
Overall Width	8.4 ft
Overall Body Height	10.5 ft
Min Body Ground Clearance	1 ft
Track Width	8.4 ft
Lock-to-lock time	6 s
Curb to Curb Turning Radius	29.3 ft





29 Windsor Avenue Windsor, Connecticut

Designed by	Checked by

Issued for	Date
Site Plan Approval	July 28, 20

Not For Construction

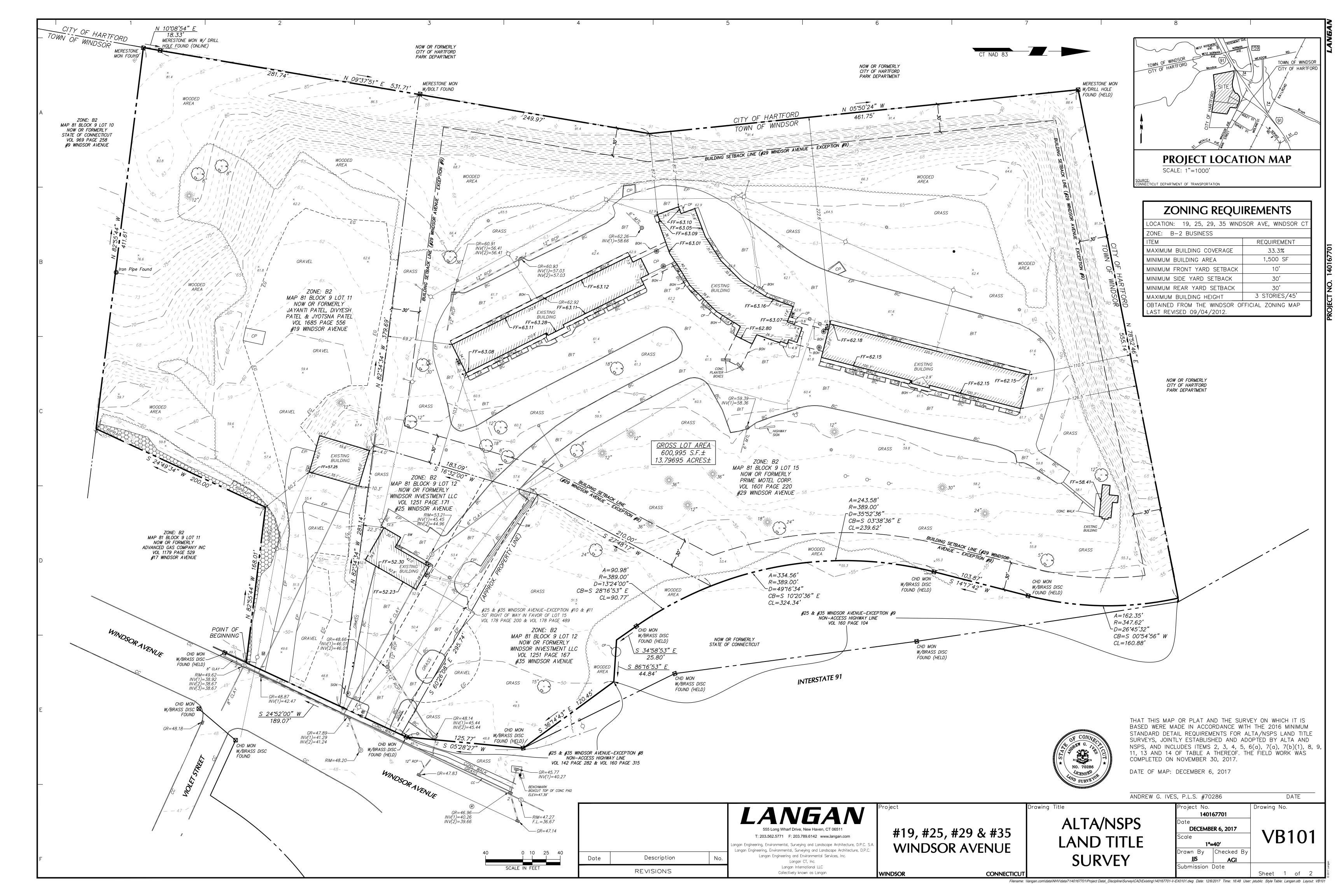




Project Number 42795.01

Pumper Fire Truck

Overall Length	47 ft
Overall Width	8.17 ft
Overall Body Height	7.7 ft
Min Body Ground Clearance	0.66 ft
Track Width	8.17 ft
Lock-to-lock time	5 s
Max Wheel Angle	45°



a. THIS SURVEY IS A PROPERTY SURVEY CONFORMING TO A HORIZONTAL ACCURACY OF A-2 AND A TOPOGRAPHIC SURVEY CONFORMING TO A T-2 ACCURACY. THE BOUNDARY DETERMINATION IS A RESURVEY. THE PURPOSE OF THIS SURVEY IS TO PROVIDE A BOUNDARY OPINION AND DEPICT SITE FEATURES FOR FUTURE SITE DEVELOPMENT.

- 2. THIS SURVEY IS BASED UPON EXISTING PHYSICAL CONDITIONS FOUND AT THE SUBJECT SITE, DEED INFORMATION AND THE FOLLOWING REFERENCES:
 - A.MAP TITLED "TOWN OF WINDSOR MAP SHOWING LAND & RIGHTS OF ACCESS ACQUIRED FROM MARY L. CARVILLE BY THE STATE OF CONNECTICUT RELOCATION OF ROUTE U.S. 5-A", SCALE: 1"-40', DATED: MAY 1953, TOWN NO. 164, PROJECT NO. 58, SERIAL NO. 95, SHEET 1 OF 1
 - B. MAP TITLED "TOWN OF WINDSOR MAP SHOWING LAND RELEASED TO MARY L. CARVILLE ET AL BY THE STATE OF CONNECTICUT HARTFORD-SPRINGFIELD EXPRESSWAY", SCALE: 1"=40', DATED: OCTOBER 1955, TOWN NO. 164, PROJECT NO. 58, SERIAL NO. 1-B, SHEET 1 OF 1
 - C. MAP TITLED "PLOT PLAN SHOWING THE PROPERTY OF HENRY P. & MARY L. CARVILLE, WINDSOR AVE., WINDSOR, CONN.", SCALE: 1"=20', DATED: DEC. 1959, LAST REVISED: NOV. 1960, MAP VOLUME 5 PAGE 453
 - D. MAP TITLED "CONNECTICUT STATE HIGHWAY DEPARTMENT, RIGHT OF WAY MAP. TOWN OF WINDSOR HARTFORD-SPRINGFIELD EXPRESSWAY FROM HARTFORD TOWN LINE NORTHERLY TO BINA AVENUE, NUMBER 164-06. SHEET NO. 1 OF 1", SCALE: 1"=100', DATED: JANUARY 29,
 - E. MAP TITLED "RIGHT OF WAY SURVEY, STATE OF CONNECTICUT, DEPARTMENT OF TRANSPORTATION, RIGHT OF WAY MAP, TOWN OF WINDSOR, INTERSTATE ROUTE 91 IN THE VICINITY OF WINDSOR AVENUE, NUMBER 164-14, SHEET NO. 1A OF 1", SCALE: 1"=80', DATED: MARCH 30, 2001, LAST REVISED: 10/11/01
- 3. THE MERIDIAN OF THIS SURVEY IS REFERENCED TO CONNECTICUT STATE PLANE COORDINATE SYSTEM NAD 83 AS ESTABLISHED THROUGH GPS METHODS.
- 4. ELEVATIONS SHOWN ARE REFERENCED TO NAVD 88 ESTABLISHED THROUGH GPS METHODS.
- 5. PLANIMETRIC AND TOPOGRAPHIC INFORMATION SHOWN HEREON HAS BEEN OBTAINED FROM GROUND SURVEYS BY LANGAN CT, INC. FIELD WORK COMPLETED DURING THE MONTH OF NOVEMBER 2017.
- 6.AS PER THE NATIONAL FLOOD INSURANCE PROGRAM FIRM MAP ENTITLED "HARTFORD COUNTY, CONNECTICUT PANEL 366 OF 675. MAP NUMBER 09003C0366G, EFFECTIVE DATE SEPTEMBER 16, 2011" THE PROJECT AREA IS IN ZONE X (UNSHADED).
- 7. UNLESS SPECIFICALLY NOTED HEREON, STORM AND SANITARY SEWER INFORMATION (INCLUDING PIPE INVERT, PIPE MATERIAL, AND PIPE SIZE) WAS OBSERVED AND MEASURED AT FIELD LOCATED STRUCTURES (MANHOLES/CATCH BASINS, ETC). CONDITIONS CAN VARY FROM THOSE ENCOUNTERED AT THE TIMES WHEN AND LOCATIONS WHERE DATA IS OBTAINED. DESPITE MEETING THE REQUIRED STANDARD OF CARE, THE SURVEYOR CANNOT, AND DOES NOT WARRANT THAT PIPE MATERIAL AND/OR PIPE SIZE THROUGHOUT THE PIPE RUN ARE THE SAME AS THOSE OBSERVED AT EACH STRUCTURE, OR THAT THE PIPE RUN IS STRAIGHT BETWEEN THE LOCATED STRUCTURES.
- 8. ADDITIONAL UTILITY (WATER, GAS, ELECTRIC ETC.) DATA MAY BE SHOWN FROM FIELD LOCATED SURFACE MARKINGS (BY OTHERS), EXISTING STRUCTURES, AND/OR FROM EXISTING DRAWINGS.
- 9. UNLESS SPECIFICALLY NOTED HEREON, THE SURVEYOR HAS NOT EXCAVATED TO PHYSICALLY LOCATE THE UNDERGROUND UTILITIES. THE SURVEYOR MAKES NO GUARANTEES THAT THE SHOWN UNDERGROUND UTILITIES ARE EITHER IN SERVICE, ABANDONED OR SUITABLE FOR USE, NOR ARE IN THE EXACT LOCATION OR CONFIGURATION INDICATED HEREON.
- 10. ALL BUILDINGS AND STRUCTURES WERE LOCATED AND MEASURED AT GROUND LEVEL. THE SURVEYOR MAKES NO DETERMINATIONS OR GUARANTEES AS TO THE ABSENCE, EXISTENCE OR LOCATION OF UNDERGROUND STRUCTURES, FOUNDATIONS, FOOTINGS, PROJECTIONS, WALLS, TANKS, SEPTIC SYSTEMS, ETC. NO TEST PITS, EXCAVATIONS OR GROUND PENETRATING RADAR WERE PERFORMED AS PART OF THIS SURVEY.
- 11. PRIOR TO ANY DESIGN OR CONSTRUCTION, THE PROPER UTILITY AGENCIES MUST BE CONTACTED FOR VERIFICATION OF UTILITY TYPE AND FOR FIELD LOCATIONS.
- 12. THIS SURVEY IS NOT VALID WITHOUT THE EMBOSSED OR INKED SEAL OF THE PROFESSIONAL

TITLE REPORT

#19 WINDSOR AVENUE COMMITMENT FOR TITLE INSURANCE ISSUED BY FIRST AMERICAN TITLE INSURANCE COMPANY. COMMITMENT NUMBER: HART2474357, EFFECTIVE DATE SEPTEMBER 28, 2017. SCHEDULE B SECTION II:

- NOT SURVEY RELATED.
- 2. SURVEY PROVIDED
- 3-7. NOT SURVEY RELATED

#25 WINDSOR AVENUE COMMITMENT FOR TITLE INSURANCE ISSUED BY FIRST AMERICAN TITLE INSURANCE COMPANY. COMMITMENT NUMBER: HART2474292, EFFECTIVE DATE SEPTEMBER 28, 2017. SCHEDULE B SECTION II:

- 1. NOT SURVEY RELATED.
- 2. SURVEY PROVIDE
- 3-7. NOT SURVEY RELATED
- 8. WAIVER AND RELINQUISHMENT OF ACCESS TO ROUTE U.S. 5A AS DEFINED IN VOL 142 PAGE 282 AND VOL 160 PAGE 315. DEPICTED ON SURVEY.
- 9. DENIAL OF RIGHTS OF INGRESS AND EGRESS AS DEFINED IN VOL 160 PAGE 104. DEPICTED ON SURVEY.
- 10. RIGHT OF WAY AS DEFINED IN VOL 178 PAGE 200. DEPICTED
- 11. RIGHT OF WAY AS DEFINED IN VOL 178 PAGE 489.
- 12. NOT SURVEY RELATED.
- 13. ENCROACHMENT AGREEMENT AS DEFINED IN VOL 1626 PAGE 211. THE BITUMINOUS CURB ENCROACHMENT AS REFERENCED IN SAID DEED HAS BEEN REMOVED AND REPLACED WITH GRASS AREA. THIS SITE CHANGE DOES NOT AFFECT THE 50 FOOT RIGHT OF WAY EASEMENT, WHICH IS STILL ACCESSIBLE

COMMITMENT FOR TITLE INSURANCE ISSUED BY FIRST AMERICAN TITLE INSURANCE COMPANY. COMMITMENT NUMBER: HART2474308, EFFECTIVE DATE SEPTEMBER 28, 2017. SCHEDULE B SECTION II:

- NOT SURVEY RELATED
- 2. SURVEY RELATED.
- 3-8. NOT SURVEY RELATED
- 9. 30' BUILDING LINES AS SHOWN IN MAP #5467. DEPICTED ON SURVEY.
- 10. ENCROACHMENT AGREEMENT AS DEFINED IN VOL 1626 PAGE 211. THE BITUMINOUS CURB ENCROACHMENT AS REFERENCED IN SAID DEED HAS BEEN REMOVED AND REPLACED WITH GRASS AREA. THIS SITE CHANGE DOES NOT AFFECT THE 50 FOOT RIGHT OF WAY EASEMENT, WHICH IS STILL ACCESSIBLE

COMMITMENT FOR TITLE INSURANCE ISSUED BY FIRST AMERICAN TITLE INSURANCE COMPANY. COMMITMENT NUMBER: HART2474303, EFFECTIVE DATE SEPTEMBER 28. 2017. SCHEDULE B SECTION II:

- 1. NOT SURVEY RELATED.
- 2. SURVEY PROVIDE.
- 3-7. NOT SURVEY RELATED.
- 8. WAIVER AND RELINQUISHMENT OF ACCESS TO ROUTE U.S. 5A AS DEFINED IN VOL 142 PAGE 282 AND VOL 160 PAGE 315. DEPICTED ON SURVEY.
- 9. DENIAL OF RIGHTS OF INGRESS AND EGRESS AS DEFINED IN VOL 160 PAGE 104. DEPICTED ON SURVEY.
- 10. RIGHT OF WAY AS DEFINED IN VOL 178 PAGE 200. DEPICTED
- 11. RIGHT OF WAY AS DEFINED IN VOL 178 PAGE 489.

---- BOLLARD

---- COLUMN

---- MAILBOX

----- FLAG POLE

SIGN

12. NOT SURVEY RELATED.

LEGEND (NOT SHOWN TO SCALE)

SURVEYED DESCRIPTION

BEGINNING AT A POINT IN THE WESTERLY SIDE OF WINDSOR AVENUE, SAID POINT BEING A SOUTHEAST CORNER OF HEREIN DESCRIBED PARCEL AND THE NORTHEAST CORNER OF LAND NOW OR FORMERLY OF ADVANCED GAS COMPANY INC;

THENCE N 82° 55' 44" W BOUNDED SOUTHERLY BY LAND NOW OR FORMERLY OF ADVANCED GAS COMPANY INC A DISTANCE OF 168.01' TO A POINT;

THENCE S 24° 49' 34" W BOUNDED EASTERLY BY LAND NOW OR FORMERLY OF ADVANCED GAS COMPANY INC A DISTANCE OF 200.00' TO A POINT:

THENCE N 82° 55' 44" W BOUNDED SOUTHERLY BY LAND NOW OR FORMERLY OF STATE OF CONNECTICUT A DISTANCE OF 411.61' TO

THENCE N 10° 08' 54" E BOUNDED WESTERLY BY LAND NOW OR

FORMERLY OF CITY OF HARTFORD PARK DEPARTMENT A DISTANCE OF 18.33' TO A POINT; THENCE N 09° 37' 51" E BOUNDED WESTERLY BY LAND NOW OR

OF 531.71' TO A POINT; THENCE N 05° 50' 24" W BOUNDED WESTERLY BY LAND NOW OR FORMERLY OF CITY OF HARTFORD PARK DEPARTMENT A DISTANCE

FORMERLY OF CITY OF HARTFORD PARK DEPARTMENT A DISTANCE

THENCE N 78° 52' 46" E BOUNDED NORTHERLY BY LAND NOW OR FORMERLY OF CITY OF HARTFORD PARK DEPARTMENT A DISTANCE

OF 461.75' TO A POINT;

OF 555.14' TO A POINT;

THENCE CURVING TO THE RIGHT BOUNDED EASTERLY BY LAND NOW OR FORMERLY OF STATE OF CONNECTICUT, WITH AN ARC LENGTH OF 162.35', A RADIUS OF 347.62', AND INCLUDED ANGLE OF 26' 45' 32", AND A CHORD OF 160.88' WITH A BEARING OF S 00° 54' 56" W TO A POINT;

THENCE S 14° 17' 42" W BOUNDED EASTERLY BY LAND NOW OR FORMERLY OF STATE OF CONNECTICUT A DISTANCE OF 103.87' TO A POINT OF CURVATURE;

THENCE CURVING TO THE LEFT BOUNDED EASTERLY BY LAND NOW OR FORMERLY OF STATE OF CONNECTICUT, WITH AN ARC LENGTH OF 334.56', A RADIUS OF 389.00', AND INCLUDED ANGLE OF 49° 16' 34", AND A CHORD OF 324.34' WITH A BEARING OF S 10° 20' 36" E TO A POINT;

THENCE S 34° 58' 53" E BOUNDED EASTERLY BY LAND NOW OR FORMERLY OF STATE OF CONNECTICUT A DISTANCE OF 25.80' TO A POINT:

THENCE S 86° 16' 53" E BOUNDED NORTHERLY BY LAND NOW OR FORMERLY OF STATE OF CONNECTICUT A DISTANCE OF 44.84' TO A POINT:

THENCE S 36° 14' 43" E BOUNDED EASTERLY BY LAND NOW OR FORMERLY OF STATE OF CONNECTICUT A DISTANCE OF 120.45' TO A POINT IN THE WESTERLY SIDE OF WINDSOR AVENUE;

THENCE S 05° 28' 27" W ALONG THE WESTERLY SIDE OF

THENCE S 24° 52' 00" W ALONG THE WESTERLY SIDE OF WINDSOR AVENUE A DISTANCE OF 189.07' TO THE POINT OF BEGINNING;

CONTAINING APPROXIMATELY 600,995 SQUARE FEET (13.79695 ACRES).

RECORD DESCRIPTION

#19 WINDSOR AVENUE A CERTAIN PIECE OR PARCEL OF LAND, WITH ALL THE BUILDINGS THEREON STANDING, SITUATED ON THE WESTERLY SIDE OF WINDSOR AVENUE, IN THE TOWN OF WINDSOR, COUNTY OF HARTFORD AND STATE OF CONNECTICUT, AND MORE PARTICULARLY BOUNDED AND DESCRIBED AS FOLLOWS:

COMMENCING AT A POINT SITUATED IN THE WESTERLY LINE OF WINDSOR AVENUE, AT THE SOUTHEAST CORNER OF LAND NOW OR FORMERLY OF CARVILLE'S RANCH HOUSE, INC., BEING THE NORTHEAST CORNER OF LAND HEREIN DESCRIBED; THENCE S 24 DEGREES 55' 07" W ALONG WINDSOR AVENUE, A DISTANCE OF 110.48 FEET TO A POINT; THENCE N 82 DEGREES 50' 11" W ALONG OTHER LAND OF CARMELLA SPONZO, X DISTANCE OF 168.00 FEET TO A POINT; THENCE S 24 DEGREES 55' 07" W ALONG OTHER LAND OF CARMELLA SPONZO, A DISTANCE OF 200.00 FEET TO A POINT; THENCE N 82 DEGREES 50' 11" W ALONG LAND NOW OR FORMERLY OF UNIVERSAL WOODWORK & SUPPLY, INC. A DISTANCE OF 4L1.38 FEET TO A POINT: THENCE N 10 DEGREES 11' 49" E ALONG LAND OF THE CITY OF HARTFORD, A DISTANCE OF 18.32 FEET TO A MONUMENT; THENCE N 9 DEGREES 40' 46" E ALONG LAND OF THE CITY OF HARTFORD, A DISTANCE OF 281.74 FEET TO A POINT; THENCE S 82 DEGREES 29' 01" E ALONG LAND NOW OR FORMERLY OF WINDSOR MOTEL, INC. AND LAND OF CARVILLE'S RANCH HOUSE, INC., PARTLY BY EACH, A DISTANCE OF 660.74 FEET TO THE POINT AND PLACE OF BEGINNING.

#29 WINDSOR AVENUE

A CERTAIN PIECE OR PARCEL OF LAND, WITH ALL THE IMPROVEMENTS THEREON, KNOWN AS 29 WINDSOR AVENUE AND SITUATED WESTERLY OF U.S. ROUTE 5-A AND WINDSOR AVENUE IN THE TOWN OF WINDSOR, COUNTY OF HARTFORD, AND STATE OF CONNECTICUT AS SHOWN ON A CERTAIN MAP ENTITLED "BOUNDARY SURVEY PREPARED FOR PRIME HOTEL CORP., HABIB AMERICA BANK AND CHICAGO TITLE INSURANCE COMPANY, #29 WINDSOR AVENUE. WINDSOR, CONNECTICUT SCALE L " = 40', FEBRUARY 6, 2008, SCALE L" = 40 FEET, WHICH MAP WILL BE FILED IN THE OFFICE OF THE WINDSOR TOWN CLERK,

BEGINNING AT THE SOUTHWESTERLY MOST POINT ON SAID MAP, WHICH MARKS THE INTERSECTION OF THE SUBJECT PREMISES WITH LAND NOW OR FORMERLY OF CITY OF HARTFORD (KEENY PARK) AND LAND NOW OR FORMERLY OF JAYANTI PATEL, MAHENDRA PATEL AND DIVYESH PATEL, THENCE PROCEEDING

NORTHEASTERLY, N 09° 37' 41" E, BY LAND NOW OR FORMERLY OF CITY OF HARTFORD (KEENY PARK), A DISTANCE OF 249.97

NORTHWESTERLY, N 05° 50' 34" W, BY LAND NOW OR FORMERLY OF CITY OF HARTFORD (KEENY PARK), A DISTANCE OF 461.75 EAST; THENCE

NORTHEASTERLY, N 78° 52' 36" E BY LAND NOW OR FORMERLY OF CITY OF HARTFORD (KEENY PARK), A DISTANCE OF 555.14 FEET; THENCE

DISTANCE OF 162.35 FEET TO A POINT, THENCE

SOUTHWESTERLY, IN A CURVE WITH A RADIUS OF 347.62 FEET, A

SOUTHWESTERLY, S 14° 17' 32" W, A DISTANCE OF 103.87 FEET,

DISTANCE OF 243.58 FEET TO A POINT; THENCE SOUTHWESTERLY, S 27° 48' 07" W. BY LAND NOW OR FORMERLY

SOUTHERLY AGAIN, IN A CURVE WITH A RADIUS OF 389 FEET, A

SOUTHWESTERLY AGAIN, S 16° 31' 50" W, BY LAND NOW OR FORMERLY OF WINDSOR INVESTMENT, LLC, A DISTANCE OF 183.09

OF WINDSOR INVESTMENT, LLC, A DISTANCE OF 210.00 FEET;

NORTHWESTERLY, N 82° 34' 44" W BY LAND NOW OR FORMERLY OF JAYANTI PATEL, MAHENDRA PATEL AND DIVYESH PATEL, A DISTANCE OF 379.69 FEET, TO THE POINT AND PLACE OF

TOGETHER WITH, A FIFTY (50.00) FOOT RIGHT OF WAY TO #29 WINDSOR AVENUE FOR INGRESS AND EGRESS BY FOOT AND VEHICLE AS SHOWN ON SAID MAP

BEGINNING.

TOGETHER WITH AN ENCROACHMENT AGREEMENT BETWEEN WINDSOR INVESTMENT LLC AND PRIMA MOTEL CORP. DATED FEBRUARY 2, 2008 AND RECORDED IN VOLUME 1626 AT PAGE 211 OF THE WINDSOR LAND REWARDS.

#25 WINDSOR AVENUE

A certain piece or percel of land, with all the improvements thereon, eltusted on the Westerly side of U.S. Route 5-A and Windsor Avenue in the Town of Windsor, County of Hartford and State of Connecticut and being more particularly bounded and described.

Beginning at a point in the Nesterly line of Windsor Avenua, which point marks the Mortheast corner of land now or formerly of Michael Sponzo and the Southeast corner of the Within described Michael Sponzo and Sponzo and distance of 660.69 feet to or formerly of said Michael Sponzo a distance of 660.69 feet to I and now or formerly of the City of Martford; thence proceeding Morth 704/35° East along land now or formerly of the City of Martford a distance of 249.97 feet to a point, at which point is located a monument; thence proceeding Morth 8021/40° West along located a monument; thence proceeding North 8°21'40" West along land now or formerly of the City of Hartford a distance of 461.75 feet to a point, at which point is located a monument; thence proceeding North 76°19'10" Fast along land now or formerly of the proceeding North 76°19'10" East along late to the Westerly line of City of Hartford a distance of 555.1 feet to the Westerly line of U.S. Route 5-A, Ramp "E", at which point is located a C.H.D. marker; thence proceeding Southerly in a curve with a radius of 147.62 feet a distance of 162.35 feet to a point, at which point is located a C.H.D. marker; thence proceeding South 11°44'26" is located a C.H.D. marker; thence proceeding South 11°44'26" Hest a distance of 103.87 feet to a point, at which point is Nest a distance of 103.87 feet to a point, at which point is located a C.H.D. marker; thence proceeding Southerly in a curve with a radius of 389 feet a distance of 334.56 feet to a point, at which point is located a C.H.D. marker; thence proceeding South 37°32'10" East a distance of 25.8 feet to a point, at which point is located a C.K.D. warker; thence proceeding South 82°50'10" East a distance of 44.84 feet to a point, at which point is located a C.H.D. marker; thence proceeding South 18 148 Tast a distance of 120.45 feet to a point, at which point is located at C.H.D. marker; thence proceeding South 2056 20" West, a distance of 125.79 feet to a point, at which point is located a C.H.D. marker, the last seven courses being along the Wasterly line of said U.S. Route 5-A, Ramp "E"; thence proceeding southerly 22020 West along the Westerly line of Windsor Avenue a distance of 78.55 feet to the point or place of beginning

Excluding therefrom a certain piece or parcel of land, with all the improvements thereon, known as 29 Windsor Avenue and situated Westerly of U.S. Routs 5-A and Windsor Avenue in the Town of Windsor, County of Hartford, and State of Connecticut and being more particularly bounded and described as follows:

> Beginning at a point on the Westerly line of U.S. Route 5-Ramp "F" which point is the Northeasterly corner of the within described premises and at which point is located a C.H.D. marker; thence proceeding Southerly in a curve with a redius of 347.62 feet a distance of 162.35 feet to a point, at which point is located a C.H.D. marker; thence proceeding South 11° 44' 26" Hest a distance of 103.87 feet to a point, at which point is located a C.H.D. marker; thence proceeding Southerly in a curve with a radius of 389 feet a distance of 243.58 feet to a point; thence proceeding South 250 15' West a distance of 210 feet to a point; thence proceeding South 58' 43" Hest a distance of 183.09 feet, the last two courses being along other land of the Present Owner; thence proceeding North 85° 07" 50" Hest along land now or formerly f Michael Sponzo a distance of 379.69 feet to land now of ormerly of the city of Hertford; thence proceeding North 7 04' 35" East along land now or formerly of the City of Hartford a distance of 249.97 feet to a point, at which oint is located a monument; thence proceeding North 80 23' 0" West along land now or formerly of the City of Hartford a distance of 451.75 feat to a point, at which point is located a monument; thence proceeding North 76° 15' 10" East along land now or formerly of the City of Hartford a distance of 555.1 feet to the point or place of beginning.

Excluding therefrom a certain piece or parcel of land, with all the improvements thereon, known as 35 Windsor Avenue and sinuated in the Town of Windsor, County of Hartford, and State of Connecticut and being more particularly described as follows: Map 81, Block 9, Lot 12 and File Code Number 00014.00 in the records of the Windsor Tax Assessor.

#35 WINDSOR AVENUE

A certain piece or percel of land, with all the improvements thereon, situated on the Hosterly side of U.S. Routs 5-A and Mindsor Avenus in the Town of Mindsor, County of Martford and State of County of Martford and Ma State of Connecticut and being more particularly bounded and

Beginning at a point in the Hesterly line of Windsor Avenue, which point marks the Northeast corner of land now or formerly of Michael Sponzo and the Southeast corner of the within described Michael Sponzo and the Southeast corner of the within described Premises; thence proceeding North 85°07'50" Heat along land now or formerly of said Michael Sponzo a distance of 660.69 feet to or formerly of said Michael Sponzo a distance of 660.69 feet to family of the City of Hartford; thence proceeding land now or formerly of the City of Hartford a distance of 149.97 feet to a point, at which point is located a monument; thence proceeding North 8°23'40" Hest along land now or formerly of the City of Hartford a distance of 661.75 feet to a point, at which point is located a monument; thence feet to a point, at which point is located a monument; thence proceeding North 76°19'10" East along land now or formerly of the City of Hartford a distance of 555.1 feet to the wasterly the of U.S. Route 5-A, Ramp "E", at which point is located a C.H.D. marker; thence proceeding Southerly in a curve with a radius of 147.62 feet a distance of 162.75 feet to a point, at which point 147.62 feet a distance of 162.35 feet to a point, at which point is located a C.H.D. marker; thence proceeding South 11°44'26" West a distance of 103.87 feet to a point, at which point is located a C.H.D. marker; thence proceeding Southerly in a curve with a radius of 389 feet a distance of 334.56 feet to a point, at which point is located a C.H.D. marker; thence proceeding south 37°32'10" East a distance of 25.8 feet to a point, at which point is located a C.H.D. marker; thence proceeding south 37°32'10" East a distance of 25.8 feet to a point, at which point is located a C.K.D. marker; thence proceeding south \$2°50'10" East a distance of 44.84 feet to a point, at which point is located a C.H.D. marker; thence proceeding South is located at C.H.D. marker; thence proceeding South 1058'20"
West; a distance of 125.79 feet to a point, at which point is
located a C.H.D. marker, the last seven courses being along the Westerly line of said U.S. Route 5-A, Ramp "E"; thence proceeding Southerly 22°20". West along the Westerly line of Windson Avenue a distance of 78.55 feet to the point or place of beginning.

Excluding therefrom a certain piece or parcel of land, with all the improvements thereon, known as 29 Hindsor Avenue and situated Nesterly of U.S. Route 5-A and Hindsor Avenue in the Town of Windsor, County of Hartford, and State of Connecticut and being

Beginning at a point on the Westerly line of U.S. Route S-A Ramp "E" which point is the Hortheasterly corner of the within described presises and at which point is located a C.H.D. marker; thence proceeding Southerly in a curve with a radius of 347.62 feet a distance of 162.35 feet to a point, at which point is located a C.R.D. marker; thence proceeding South 11° 44' 26" Wart a distance of 103.87 feet to a point, at which point is located a C.S.D. marker; thence proceedings southerly in a curve with a radius of J89 feet a distance of 141.58 feet to a point; thence proceeding South 15° 15' Heet a distance of 210 feet to a point; thence proceeding South 13° 58' 43" West a distance of 183.09 feet, the last two a distance of 461.75 feet to a point, at which point is located a monument; thence proceeding North 760 lp' 30" Fast along land now or formerly of the City of Hartford a distance of 555.1 feet to the point or place of beginning.

Excluding therefrom a certain piece or parcel of land, with all the improvements thereon, known as 25 Windsor Avenue and situated in the Town of Windsor, County of Hartford, and State of Connecticut and being more particularly described as follows: Map 81, Block 9, Lot 12b and File Code Number 00016.00 in the records of the Windsor Tax Assessor.

THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2016 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 2, 3, 4, 5, 6(a), 7(a), 7(b)(1), 8, 9, 11. 13 AND 14 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON NOVEMBER 30, 2017.

DATE OF MAP: DECEMBER 6, 2017



CONC ---- CONCRETE ---- CATCH BASIN ---- CONCRETE PAD ----- LANDSCAPED AREA ----- CLEANOUT ----- ELECTRIC METER ----- BUILDING OVERHANG ----- FIRE HYDRANT ---- BOTTOM OF WALL ---- GAS METER ---- EDGE OF PAVEMENT ----- GAS VALVE ----- EDGE OF GRAVEL ---- BITUMINOUS CURB ---- GUY WIRE ---- LIGHT POLE ---- CONCRETE CURB ----- MANHOLE (TYPE AS LABELED

---- POWER POLE

----- WATER VALVE

---- BITUMINOUS

---- SPOT ELEVATION

----- TRAFFIC SIGNAL POLE

Date

---- STANDPIPE

STOCKADE FENCE ---- CHAINLINK FENCE ----- TREE LINE ----- OVERHEAD WIRE ---- EASEMENT LINE ----- PROPERTY LINE - - - 322 - - - CONTOUR LINE ----E----- ELECTRIC MARK OUT LINE

Description

REVISIONS

LANGAN

T: 203.562.5771 F: 203.789.6142 www.langan.com angan Engineering, Environmental, Surveying and Landscape Architecture, D.P.C. S. Langan Engineering, Environmental, Surveying and Landscape Architecture, D.P.C. Langan Engineering and Environmental Services, Inc. Langan CT, Inc.

Langan International LLC

Collectively known as Langan

#19, #25, #29 & #35 WINDSOR AVENUE

WINDSOR

ALTA/NSPS **LAND TITLE SURVEY**

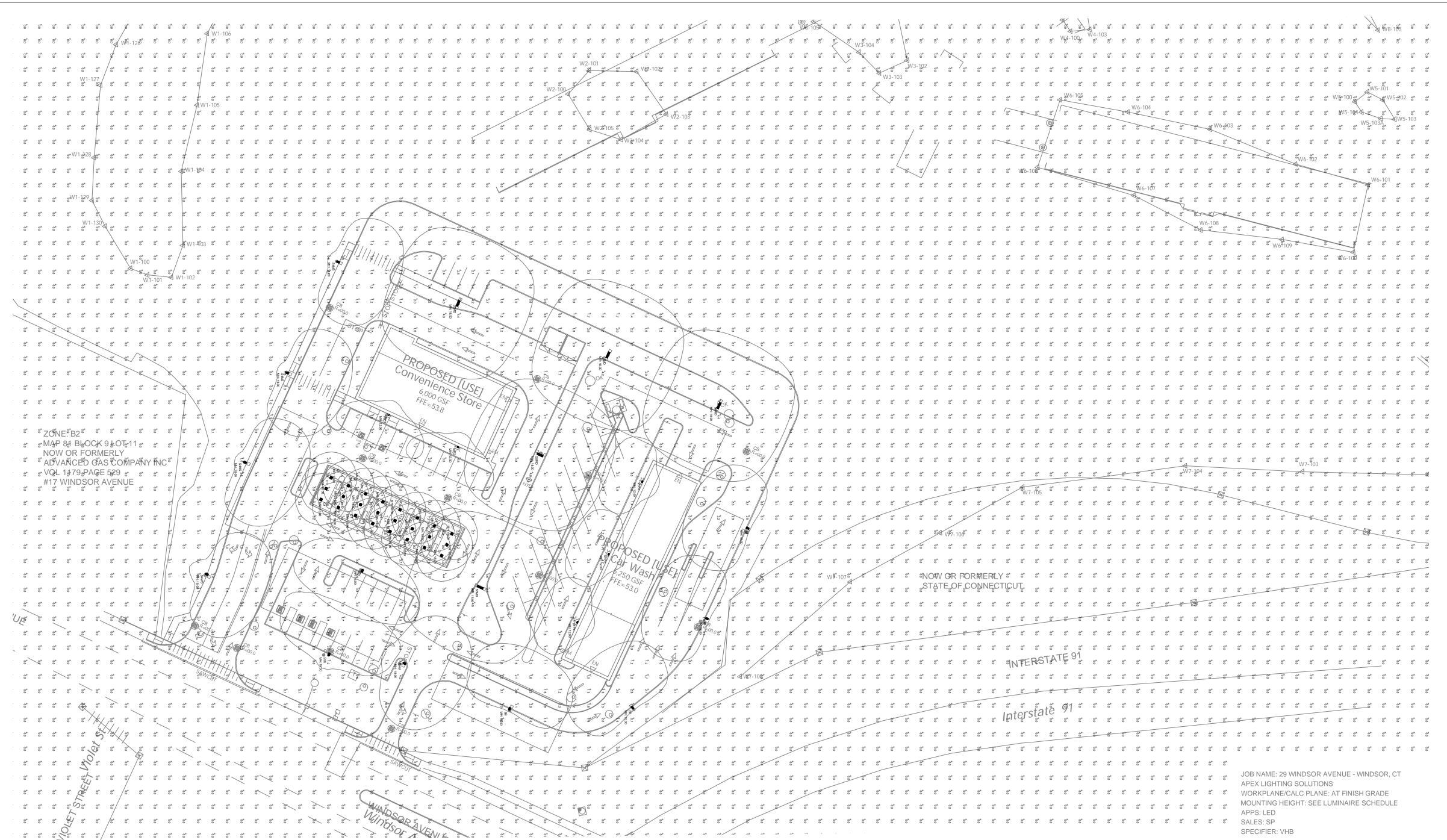
140167701 **DECEMBER 6, 2017** 1"=40' Drawn By | Checked By AGI ubmission Date

VB102

Filename: \|langan.com\|data\|NHV\|data7\|140167701\|Project Data_Discipline\|Survey\|CAD\|Existing\|140167701-V-EX0101.dwg Date: \|12/6/2017 Time: 16:48 User: jstublic Style Table: Langan.stb Layout: VB102

ANDREW G. IVES, P.L.S. #70286

DATE



Lumina	Luminaire Schedule							
Qty	Label	Arrangement	Lumens	Input Watts	LLF	BUG Rating	Description	
24	C1	Single	2352	20.49	0.850	N.A.	TRACELITE SCP-R-20-LG-VS-4K-WH / RECESSED IN CANOPY @ 18FT AFG	
1	S2	Single	8940	54.129	0.850	B2-U0-G2	GARDCO OPF-S-A02-730-T2M-AR1-UNV-FINISH / MOUNTED TO SSA-CA-4-125-18-D1-DTX-xxx-FINISH	
1	S3S	Single	6969	54.129	0.850	B1-U0-G2	GARDCO OPF-S-A02-730-T3M-AR1-UNV-FINISH-HIS / MOUNTED TO SSA-CA-4-125-18-D1-DTX-xxx-FINISH	
1	S4M	Single	8989	54.129	0.850	B1-U0-G2	GARDCO OPF-S-A02-730-T4M-AR1-UNV-FINISH / MOUNTED TO SSA-CA-4-125-18-D1-DTX-xxx-FINISH	
3	S4MS	Single	6993	54.129	0.850	B1-U0-G2	GARDCO OPF-S-A02-730-T4M-AR1-UNV-FINISH-HIS / MOUNTED TO SSA-CA-4-125-18-D1-DTX-xxx-FINISH	
1	S4WS	Single	5733	54.129	0.850	B1-U0-G2	GARDCO OPF-S-A02-730-T4W-AR1-UNV-FINISH-HIS / MOUNTED TO SSA-CA-4-125-18-D1-DTX-xxx-FINISH	
5	S5MD	Back-Back	9265	54.129	0.850	B3-U0-G2	GARDCO OPF-S-A02-730-T5M-AR1-UNV-FINISH / MOUNTED TO SSA-CA-4-125-18-D2-DTX-xxx-FINISH	
4	SB	Single	6234	54.129	0.850	B0-U0-G2	GARDCO OPF-S-A02-730-BLC-AR1-UNV-FINISH / MOUNTED TO SSA-CA-4-125-18-D1-DTX-xxx-FINISH	
2	WP3	Single	3687	34.3	0.850	B1-U0-G1	STONCO LPW16-30-WW-G3-3-UNV-FINISH / WALL MOUNTED @ 12FT AFG TO BOF	
3	WP4	Single	3638	34.3	0.850	B1-U0-G1	STONCO LPW16-30-WW-G3-4-UNV-FINISH / WALL MOUNTED @ 12FT AFG TO BOF	

Calculation Summary								
Label	Grid Height	Avg	Max	Min	Avg/Min	Max/Min		
GAS STATION CANOPY	0	8.80	11.4	5.4	1.63	2.11		
EXCLUSIVE VEHICLE USE	0	1.82	6.2	0.2	9.10	31.00		
UNPAVED	0	0.04	4.7	0.0	N.A.	N.A.		
PARKING, LOADING, PEDESTRIAN	0	1.98	5.8	0.4	4.95	14.50		

GENERAL DISCLAIMER:

Calculations have been performed according to IES standards and good practice Some differences between measured values and calculated results may occur due to tolerances in calculation methods, testing procedures, component performance, measurement techniques and field conditions such as voltage and temperature variations. Input data used to generate the attached calculations such as room dimensions, reflectances, furniture and architectural elements significantly affect the lighting calculations. If the real environment conditions do not match the input data, differences will occur between measured values and calculated values.

* LLF Determined Using Current Published Lamp Data

NOTE TO REVIEWER:

Total Light Loss Factor (LLF) applied at time of design is determined by applying the Lamp Lumen Depreciation (LLD) from current lamp manufacturer's catalog, a Luminaire Dirt Depreciation Factor (LDD) based on IES recommended values and a Ballast Factor (BF) from current ballast specification sheets. Application of an incorrect Light Loss Factor (LLF) will result in forecasts of performance that will not accurately depict actual results. For proper comparison of photometric layouts, it is essential that you insist all designers use correct Light Loss Factors.



PROJECT TITLE:

WILSON CENTER 29 WINDSOR AVE WINDSOR, CT

DRAWING TITLE:

SITE LIGHTING

PHOTOMETRIC CALCULATION

FILE NAME: 2023-06-05 SL-2 WILSON CENTER - 29 WINDSOR AVE - WINDSOR, CT-LED.dwg

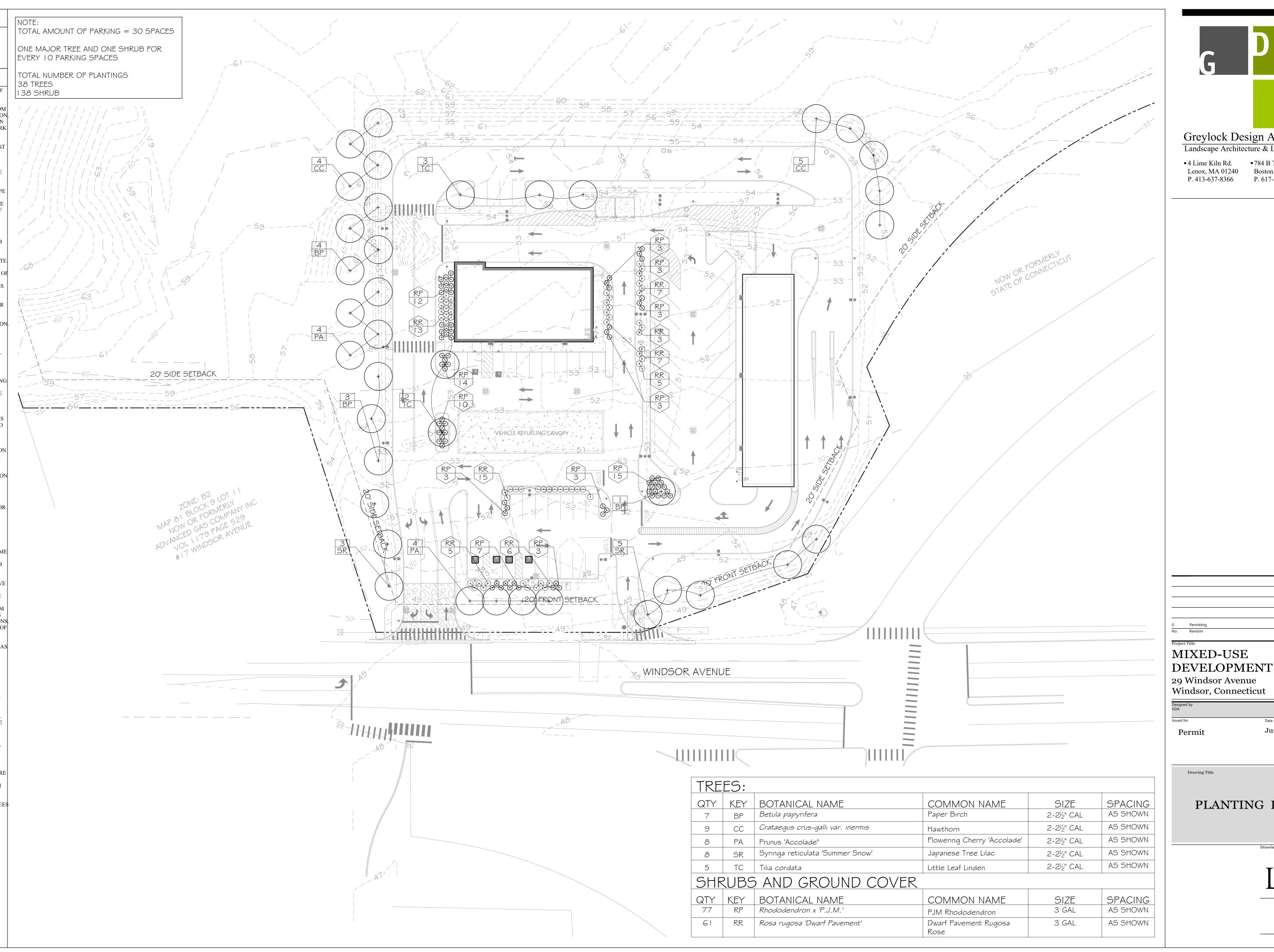
SCALE: 1"=40'-0"

DATE: 6/5/23

DRAWN BY: LED

SHEET:

PLAN NOTES PROPERTY LINE AND EXISTING CONDITIONS INFORMATION OBTAINED FROM DRAWING PREPARED BY VHB PREPARED MARCH 30, 2022. PLANTING NOTES ALL PLANT MATERIAL TO BE HEALTHY NURSERY GROWN STOCK SUBJECT TO A.A.N. STANDARDS AND FREE OF DISEASE & INSECT INFESTATION, AND SHALL BE WARRANTED FOR ONE YEAR FROM THE DATE OF PROJECT SUBSTANTIAL COMPLETION THE CONTRACTOR SHALL SUPPLY ALL PLANTS IN QUANTITIES SUFFICIENT TO COMPLETE THE WORK SHOWN ON THE DRAWINGS AND LISTED IN THE PLANT LIST. IN THE EVENT OF A DISCREPANCY BETWEEN QUANTITIES SHOWN IN THE PLANT LIST AND THOSE REQUIRED BY THE DRAWINGS, THE LARGER NUMBER SHALL APPLY. ALL PLANTS SHALL BE APPROVED PRIOR TO INSTALLATION AND SHALL BE LOCATED ON SITE BY THE CONTRACTOR FOR APPROVAL OF THE LANDSCAPE ARCHITECT. ANY INSTALLATIONS WHICH WERE NOT APPROVED BY THE LANDSCAPE ARCHITECT AND WHICH ARE SUBSEQUENTLY REQUESTED TO BE MOVED WILL BE DONE AT THE CONTRACTORS EXPENSE. NO SUBSTITUTIONS OF PLANT MATERIAL ARE TO BE MADE WITHOUT APPROVAL OF LANDSCAPE ARCHITECT. SUBSTITUTIONS WILL REQUIRE APPROVAL OF PLANNING DEPARTMENT. PRECISE LOCATION OF ITEMS NOT DIMENSIONED ON THE PLAN ARE TO BE FIELD STAKED BY THE CONTRACTOR AND SHALL BE SUBJECT TO THE REQUIREMENTS SPECIFIED IN THE PREVIOUS NOTE ALL SHRUB MASSING AND TREE PITS SHALL BE MULCHED TO A DEPTH OF NO GREATER THAN 2" O PINE BARK MULCH. TREES SHALL NOT BE STAKED OR GUYED UNLESS OTHERWISE INDICATED. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGED VEGETATION AND SHALL REPLACE OR REPAIR ANY DAMAGED MATERIAL, AT HIS OWN EXPENSE. THE CONTRACTOR SHALL CONTACT "CALL BEFORE YOU DIG" PRIOR TO CONSTRUCTION ALL SHRUB AND GROUNDCOVER PLANTINGS AREAS SHALL HAVE CONTINUOUS BEDS OF TOPSOIL 12" DEEP. AREAS DISTURBED BY CONSTRUCTION AND NOT RESTORED WITH IMPERVIOUS SURFACES (BUILDINGS, PAVEMENTS, WALKS, ETC.) SHALL RECEIVE 4" LOAM & SEED THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UTILITIES IN THE FIELD WHERE PLANT MATERIAL MAY INTERFERE WITH UTILITIES, THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT TO COORDINATE THEIR CONTRACTOR SHALL CONDUCT A SOIL ANALYSIS AND CONSULT LANDSCAPE ARCHITECT PRIOR TO INCORPORATING ALL NECESSARY SOIL AMENDMENTS FOR HEALTHY PLANT DEVELOPMENT. ALL EXISTING RILL, GULLY OR CHANNEL EROSION SHALL BE FILLED WITH APPROPRIATE BACKFILL MATERIAL, FINE RAKED, SCARIFIED AND STABILIZED WITH APPROPRIATE VEGETATIVE MATERIAL AND/OR APPROPRIATE SEDIMENTATION AND EROSION CONTROL MEASURES. 3. ADJUSTMENTS IN THE LOCATION OF THE PROPOSED PLANT MATERIAL AS A RESULT OF EXISTING VEGETATION TO REMAIN SHALL BE TO INSTALLATION. . THE CONTRACTOR IS RESPONSIBLE FOR ALL MAINTENANCE REPAIR AND REPLACEMENT OF PLANT MATERIAL, AS REQUIRED, FOR THE DURATION OF THE PROJECT AND SUBSEQUENT WARRANTY PERIOD. PLANTINGS SHOULD BE INSTALLED DURING PRIME PLANTING MONTHS (MARCH - MAY AND SEPTEMBER - OCTOBER). PLANTINGS INSTALLED SEEDED OUT OF SPRING OR FALL PERIODS IF ALLOWED BY OWNER WILL REQUIRE AGGRESSIVE IRRIGATION PROGRAMS AT THE CONTRACTORS EXPENSE UNLESS OTHERWISE DIRECTED BY THE UPON COMPLETION OF PLANTING, REMOVE FROM SITE ALL EXCESS SOIL, MULCH, AND MATERIALS AND DEBRIS RESULTING FROM WORK OPERATIONS CLEAN UP SHOULD BE COMPLETED AT THE END OF EACH WORKING DAY. RESTORE TO ORIGINAL CONDITIONS ALL DAMAGED PAVEMENTS, PLANTING AREAS, STRUCTURES AND LAWN AREAS RESULTING FROM LANDSCAPING OPERATIONS. CONTRACTOR SHALL SURVEY, LOCATE, AND PROTECT ALL TREES WITHIN AREAS SHOWN AS "EXISTING TREES TO REMAIN" WITHIN THE DEVELOPMENT ENVELOPE FOR REVIEW BY LANDSCAPE ARCHITECT PRIOR TO CLEANING OPERATIONS. ALL EXISTING PLANT MATERIAL LISTED TO BE REMOVED SHALL INCLUDE ROOTS SYSTEMS AND BE REMOVED OFF SITE. PERENNIALS ARE TO BE PLACED 2' ON CENTER SPACING AND IN ODD NUMBER GROUPS OF 3,5,7. PLANT SYMBOLOGY SHOWN IS REPRESENTATIVE OF PLANTS THAT ARE OF 2/3 MATURITY OR 5-10 YEARS OF GROWTH.). ALL PROPOSED PLANTS ARE NON-INVASIVE, AS DETERMINED FROM THE LATEST "CONNECTICUT INVASIVE PLANT LIST" DATED OCTOBER, 2018 PUBLISHED BY THE CONNECTICUT INVASIVE PLANTS COUNCIL . THE SIGNIFICANT TREES NOTED ON THE PLAN ARE TO REMAIN AND SHALL BE PRUNED TO THE EXTENT REQUIRED IN ORDER TO PROTECT THEM DURING CONSTRUCTION CONTRACTOR SHALL COORDINATE WITH ADJACENT OWNERS PRIOR TO PRUNING ANY TREES WHOSE CANOPIES OVERHANG THE PROPERTY SCALE IN FEET 1"=30'





Greylock Design Associates Landscape Architecture & Land Planning

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MIXED-USE

29 Windsor Avenue

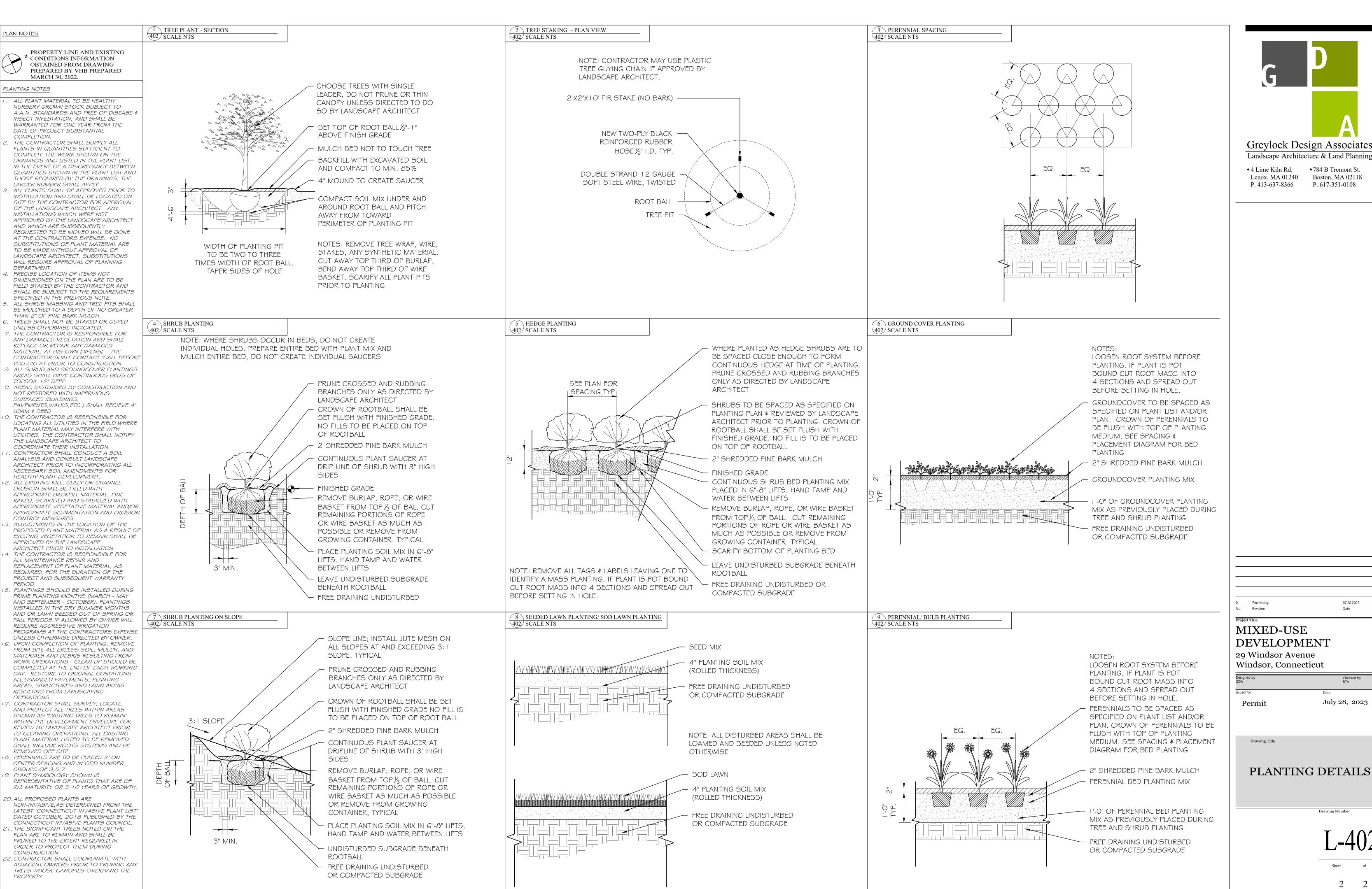
Permit

Drawing Title

June 6, 2023

PLANTING PLAN

• 784 B Tremont St. Boston, MA 02118 P. 617-351-0108





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July 28, 2023

